

A detailed landscape architectural plan for the Evergreen project. The plan features a central circular area with a fountain and surrounding trees. To the right is a large rectangular building with a grid of windows. To the left is a curved building with a checkered pattern. The entire site is bordered by a dense line of trees. The text "JULY 20, 1991" is written in the upper right, and "CITY OF SAN JOSE" is written below it. In the lower right, the title "EVERGREEN" is written in large letters, with "SPECIFIC PLAN" below it.

JULY 20, 1991

CITY OF SAN JOSE

**EVERGREEN**  
SPECIFIC PLAN

# **EVERGREEN SPECIFIC PLAN**

**Approved by the San Jose City Council on July 2, 1991**

Prepared for The City of San Jose  
by Dahlin Group, Inc.

July 2, 1991



# Acknowledgments

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**T**he Evergreen Specific Plan and associated documents represent over a year's worth of effort by a wide range of individuals. Dahlin Group would like to acknowledge the following groups and individuals for their valuable assistance and contributions toward the development of San Jose's first Specific Plan.

## **The Evergreen Specific Plan Task Force**

The Evergreen Specific Plan Task Force, chaired by Councilmember Pat Sausedo, have volunteered their time twice a month over the past year to guide the development of the specific plan. After a year of analyzing traffic impacts, environmental issues, density allocations, land use relationships and infrastructure layouts, the Task Force should be proud of their contributions and commitment they have shown to the project. The Evergreen Specific Plan Task Force members are:

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**Planning Commissioner Dan Vezinaw, Vice-chairperson**  
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Jerry Chen, Business Owner  
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## **The City of San Jose City Staff**

The City of San Jose City staff, representing all of the various City departments involved in the specific plan process, have invested a great deal of time and effort in working with the property owners, consultants, and Task Force. In that this is San Jose's first specific plan, the contribution by City staff has been invaluable in working toward the goal of a unique and distinctive project.

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# Table of Contents

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	Page No.
Evergreen Specific Plan Exhibit List	iii
<b>Chapter 1 - Executive Summary</b>	1-1
<b>Chapter 2 - Introduction</b>	
Background	2-1
Elements of the Evergreen Planned Residential Community	2-1
Detailed Project Description	2-2
<b>Chapter 3 - Overview</b>	
The Purpose of the Specific Plan	3-1
Goals, Policies, and Specific Plan Responses	3-1
Community Development	3-4
<b>Chapter 4 - Environmental Setting</b>	
Site Characteristics	4-1
<b>Chapter 5 - The Plan</b>	
Elements of the Land Use Plan	5-2
Land Use Plan	5-3
Circulation Plan	5-4
<b>Chapter 6 - Community Design Strategy</b>	
Imagery and Identity	6-1
Design Criteria	6-3
Details	6-4
Definitions	6-8
<b>Chapter 7 - Major Parks and Recreational Amenities</b>	
Recreational Amenities	7-1
Public Buildings	7-2
Walls, Monumentation, and Focal Art	7-4
Street Landscaping	7-4
Pocket Parks	7-8

	<b>Page No.</b>
<b>Chapter 7 (Cont'd)</b>	
Trails and Creek Corridors	7-10
Lakes	7-11
Heritage Vineyard	7-12
 <b>Chapter 8 - Private Development</b>	
Evergreen Village Center	8-1
Multi-Family Residential	8-8
Townhouses	8-10
Duplexes	8-12
Carriage Homes	8-14
4,000 and 5,000 Square Foot Lots	8-17
6,000 Square Foot Lots	8-20
7,000 and 8,000 Square Foot Lots	8-22
Hillside Lots	8-24
 <b>Chapter 9 - Infrastructure</b>	
Circulation System	9-1
Internal Circulation	9-2
Village Center and Fowler Creek Park Rotaries	9-4
Water Distribution	9-5
Sanitary Sewer System	9-5
Storm Drainage System	9-6
Use of Reclaimed Water	9-7
 <b>Chapter 10 - Implementation</b>	
Ownership Within the Specific Plan	10-1
Urban Service Area Boundary	10-1
Annexations	10-1
Land Exchanges Within the Specific Plan Area	10-2
Development Allocations Within the Specific Plan Area	10-3
Off-Site Traffic Mitigation	10-4
Enforcement of Design Guidelines and GDP Standards	10-7
Responsibility for Interpretation of the Specific Plan	10-7
Proposed Amendments to the Specific Plan	10-7
Development Review Process	10-7
Landscape and Lighting District	10-8
Architectural Review Committee	10-8
Financing Plan	10-8
Funding Strategy	10-10

# Evergreen Specific Plan Exhibit List

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	Exhibit No.
<b>Chapter 2 - Introduction</b>	
Vicinity Map	1
<b>Chapter 3 - Overview</b>	
City Limits of San Jose	2
<b>Chapter 4 - Environmental Setting</b>	
Existing Site Conditions	3
<b>Chapter 5 - The Plan</b>	
Illustrative Site Plan	4
Land Use Plan	5
Circulation Plan	6
<b>Chapter 6 - Community Development Strategy</b>	
Imagery Story Board	7
Evergreen Vistas	8
Architectural Details	9
Architectural Details	10
<b>Chapter 7 - Major Parks and Recreational Amenities</b>	
Community Amenities Illustrative Plan	11
Major Parks and Recreation Amenities	12
Fowler Creek Park	13
Public Building Locations	14
Community Center	15
Fire Station	16
Northern Elementary School	17
Walls, Monumentation, and Focal Art	18
Walls, Monumentation, and Focal Art	19
Landscape Features	20
"A" Aborn Road Plan and Section/"B" Murillo Avenue Plan and Section	21
"C" Quimby Road Plan and Section	22
"D" Aborn Road at the Northeast Radial Street	23
"E" Murillo Avenue at Quimby Creek	24
"F" Murillo Avenue at the East/West Street Intersection	25
"G" Aborn Road at Heritage Estates Drive	26
"H" Fowler Road/Radial	27

	<b>Exhibit No.</b>
<b>Chapter 7 - (Cont'd)</b>	
"I" Village Center Rotary	28
"J" Fowler Creek Park Rotary	29
Rotary Sections	30
Trails, Creek Corridors, and Pocket Parks	31
"K/L" Typical Radial Sections	32
"M" Pocket Park	33
"N" Pocket Park	34
"O" Pocket Park	35
"P" Pocket Park	36
"Q" Pocket Park	37
"R" Trail at Street Features	38
"S" Typical Trail Section	39
"T" Typical Creek Corridor	40
Lakes Map	41
Lake Plans/Details	42
Heritage Vineyard	43
Heritage Vineyard Plan	44
<b>Chapter 8 - Private Development</b>	
Evergreen Village Center	45
Village Center Concept Plan	46a
Village Center Concept Plan	46b
Village Development Criteria	47
High Density Residential	48
High Density Residential Criteria	49
Townhomes Map	50
Townhomes Development Criteria	51
Duplex/4-Plex Map	52
Duplex/4-Plex Development Criteria	53
Carriage Homes Map	54
Carriage Homes Development Criteria	55
4,000 Square Foot Lots Map	56
5,000 Square Foot Lots Map	57
4,000 to 5,000 Square Foot Lots Development Criteria	58
6,000 Square Foot Lots Map	59
6,000 Square Foot Lots Development Criteria	60
7,000 to 8,000 Square Foot Lots Map	61
7,000 to 8,000 Square Foot Lots Development Criteria	62
Hillside Lots Map	63
Hillside Lots Development Criteria	64

	<b>Exhibit No.</b>
<b>Chapter 9 - Infrastructure</b>	
Street Sections	65
Through-Community Streets	66
Collectors	67
Radial Collectors	68
Neighborhood Collectors	69
Intersection Signalization	70
Water System Map	71
Sewer System Map	72
Storm Drainage System Map	73
Use of Reclaimed Water	74
<b>Chapter 10 - Implementation</b>	
Existing Parcelization	75
Ownership Chart	76
Property Exchanges	77
Phase I Parcelization	78
Development Allocation	79
Off-Site Circulation Map	80
City Road Improvements	81
Santa Clara County Road Improvements	82
CalTrans Road Improvements	83

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## **Executive Summary**



# Chapter 1 - Executive Summary

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**T**he Evergreen Specific Plan provides a detailed planning document which outlines the future development of 865 acres of property located in or immediately adjacent to the City of San Jose. The project is bounded by Quimby Road to the north, Ruby Avenue to the west, Evergreen Creek to the south, and non-urban hillsides to the east.

The specific plan details the following elements:

A Village Center commercial area consisting of approximately 150,000 square feet of retail and service space.

Ongoing wine-making, wine-growing, warehousing and distribution, and visitor events and tours at the Mirassou Vineyards.

Residential development consisting of 2,996 dwelling units of varying types and densities:

- 299 units of High Density Residential
- 279 units of Townhomes
- 185 units of Duplex Units
- 318 units of Carriage Homes
- 425 units of 4000 Square Foot Lots
- 474 units of 5000 Square Foot Lots
- 692 units of 6000 Square Foot Lots
- 224 units of 7000 to 8000 Square Foot Lots
- 100 units of Hillside Lots

Community amenities consisting of:

- Village Center Rotary
- Fowler Creek Park Rotary
- Construction of two elementary schools.
- Supplemental funding for a new high school to be located within the Specific Plan area.
- Construction of Fowler Creek Park.
- Provision for a new fire station.
- Dedication of additional acreage to Montgomery Hill Park.
- The preservation of Quimby and Fowler Creeks.
- Creek corridor trails, pocket parks, and internal trail systems.

- Lake elements that also serve as storm water retention facilities.

Infrastructure improvements that include:

- The completion of Quimby Road.
- The construction of Aborn Road.
- The construction of Murillo Avenue.
- The completion of Yerba Buena Avenue.
- The internal circulation pattern for the project.
- The water distribution system, sanitary sewer system, and storm water drainage system necessary to serve the project.

The provision of funding to address the necessary off-site circulation improvements necessary to provide additional capacity to the area's circulation system to include:

- Improvements to Aborn Road.
- Improvements to Quimby Road.
- Improvements to Capitol Expressway.
- Improvements to the U.S. 101/Capitol Expressway on-ramps.

A financing program to fund the provisions established within the specific plan.

## **COMPANION DOCUMENTS TO THE EVERGREEN SPECIFIC PLAN**

- A. Revisions to the Evergreen Development Policy.
- B. The Evergreen Financing Plan.
- C. The Evergreen Planned Residential Community Planned Development Zoning.
- D. The Evergreen Specific Plan Environmental Impact Report.

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## Introduction



## Chapter 2 - Introduction

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**T**he Evergreen Specific Plan represents a new direction addressing land use and development within the City of San Jose by creating a unique partnership between the property owners within the project area, appointed and elected representatives of the City of San Jose, community members, area residents, and the various departments of the City of San Jose.

### BACKGROUND

In November of 1989, the San Jose City Council adopted an amendment to the City's General Plan which designated 865 acres in the Evergreen area as the "Evergreen Planned Residential Community" (EPRC). With this recommendation came the requirement that a specific plan be prepared and adopted prior to the approval of any development in the area (Exhibit 1). The primary purpose of the specific plan is to create an innovative residential community containing a variety of residential densities and housing types as well as supporting uses, services, and amenities. On July 2, 1991 the City Council adopted this specific plan and also amended the Evergreen Planned Residential Community to incorporate the essential elements of this specific plan into the General Plan. The General Plan, therefore provides the City's policy for the implementation of the Evergreen Specific Plan.

### ELEMENTS OF THE EVERGREEN PLANNED RESIDENTIAL COMMUNITY

As mandated by the San Jose City Council, the Evergreen Planned Residential Community will include the development of the following related elements:

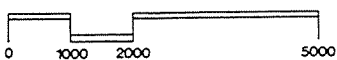
#### **The Specific Plan**

Typically, a specific plan is a medium-to-long range set of policies for land use and development in a defined project area. This document, and accompanying General Development Zoning, will serve as the bridge between the City of San Jose's *Horizon 2000 General Plan*, *Zoning Ordinance*, and individual development proposals. The Specific Plan has enabled the City of San Jose, in coordination with representatives of the Evergreen Specific Plan Task Force and property owners, to make decisions regarding land uses, circulation, public services and utilities, special design standards, and the phasing of development as part of a comprehensive plan rather than attempting these decisions on a parcel-by-parcel basis.

#### **Planned Development Zoning District**

The *General Development Plan* establishes the zoning regulations for the Specific Plan area. In San Jose, Planned Development Zoning Districts typically include a great deal of design, mitigation, and other related detail items. Some of this required detail is





## VICINITY MAP

CITY OF SAN JOSE

EVERGREEN SPECIFIC PLAN

EXHIBIT

incorporated by reference from the Specific Plan, the *Evergreen Development Policy*, environmental impact report, and financial plan into the *General Development Plan*. Development of the project area may proceed in the review process by applying for Planned Development permits consistent with the zoning district.

### **Evergreen Development Policy**

Development within the Evergreen area has been constricted by a traffic circulation system that has been unable to expand to meet additional traffic demands made by incremental development in the area. In an effort to address this capacity problem the City of San Jose adopted the "*Evergreen Development Policy*" in 1976. This limits additional growth in the area so that the area's traffic circulation system remains at an acceptable level of service. Development in Evergreen has been allowed "in an incremental, controlled process so that adequate transportation facilities exist to maintain level of service "D" throughout the study area." (*Evergreen Development Policy*, August, 1976).

In addition to the traffic policy, the *Evergreen Development Policy* also addressed the issue of flood protection within the Evergreen area. Due to the nature of the flooding problems and the available solutions to the problems, the flood protection policies have not been the restricting factor to development within Evergreen. Development within the Evergreen Planned Residential Community is contingent upon the identification and delivery of adequate traffic capacity.

The transportation component of the Evergreen Specific Plan has examined the existing and future conditions within Evergreen and has identified the means to create sufficient traffic capacity to serve not only the Evergreen Specific Plan area but also the cumulative buildout of other vacant parcels within the original *Evergreen Development Policy* boundaries.

### **Environmental Impact Report**

An environmental impact report (EIR) has been prepared as an accompanying document to evaluate the potential impacts likely to be created by adoption of the General Plan Amendment, Planned Development Zoning, and the revision to the *Evergreen Development Policy*. This specific plan contains an overview of the environmental conditions which affect the development of the project area.

## **DETAILED PROJECT DESCRIPTION**

The Evergreen Specific Plan is a comprehensive planning document prepared to address the development of 865± acres in, and adjacent to, the City of San Jose. The specific plan will consist of two distinct sections, which include:

I. A plan for the development of the project area which will include the following land uses:

A. Residential

Approximately 700 acres (or 80%) of the project area is designated for residential dwelling units. The specific plan provides a range of densities from two dwelling units per acre to 25 dwelling units per acre. The specific plan area will accommodate up to 2,996 units.

B. Commercial

A neighborhood commercial center will be located adjacent to the existing Mirassou Winery. The commercial area will include approximately 150,000 square feet of retail and service space. Preliminary uses include a major health club facility, a multi-screen movie theater, two restaurants, and approximately 5,000 square feet of general neighborhood retail space in connection with the existing Mirassou Winery's tasting room and winery facilities.

C. Public Uses

Two ten-acre elementary school sites have been designated within the planning area. The Evergreen School District is currently constructing a new middle school within the project site, which will be open for the 1991-1992 school year. In addition, the East Side Union High School District owns a 50-acre parcel within the project area. The District Board has recently determined that demographic data of the area indicates that a new high school is warranted at this location, but would be subject to the acquisition of funding necessary to construct the facility.

Development of a 12 $\pm$  acre city park is proposed as part of the Specific Plan. (The City of San Jose presently owns the Fowler Creek Park site, but construction of the park facilities has not yet been funded.)

Additional parkland will be dedicated to Montgomery Hill Park, and the Specific Plan area will fund 25% of a new fire station to be located within the project area. The Specific Plan will also develop a series of pocket parks, trails, and open space throughout the project for use by the community.

- II. The Evergreen Specific Plan also identifies strategies for implementing the plan. These include the PD zoning, land swaps, a financing plan, and a comprehensive mitigation program.

In connection with the implementing strategies, the *Evergreen Development Policy* has been revised to include analysis of the traffic and circulation impacts of the development of the project area and to identify the improvements to the circulation system necessary to create sufficient traffic capacity to serve buildout of the *Evergreen Development Policy*, including the Evergreen Specific Plan area.

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## Overview



## Chapter 3 - Overview

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### THE PURPOSE OF THE SPECIFIC PLAN

Specific plans are developed to provide a greater level of specificity in planning sites of special interest or value to a community. Typically, a specific plan contains:

Text and diagrams which call out the following in detail:

1. The distribution, location, and extent of the uses of land, including open space, within the area covered by the plan.
2. The proposed distribution, location, and extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities proposed to be located within the area covered by the plan and needed to support the land uses described in the plan.
3. Standards and criteria by which development will proceed, and standards for the conservation, development, and utilization of natural resources, where applicable.
4. A program of implementation measures including regulations, programs, public works projects, and financing measures necessary to carry out paragraphs 1, 2, and 3.

The purpose of the Evergreen Specific Plan is to provide a comprehensive program for the development of the plan area's 865 acres. This will result in a unique and appealing new community which meshes with the existing community and provides the area with a variety of new amenities while providing the infrastructure improvements necessary to implement the development.

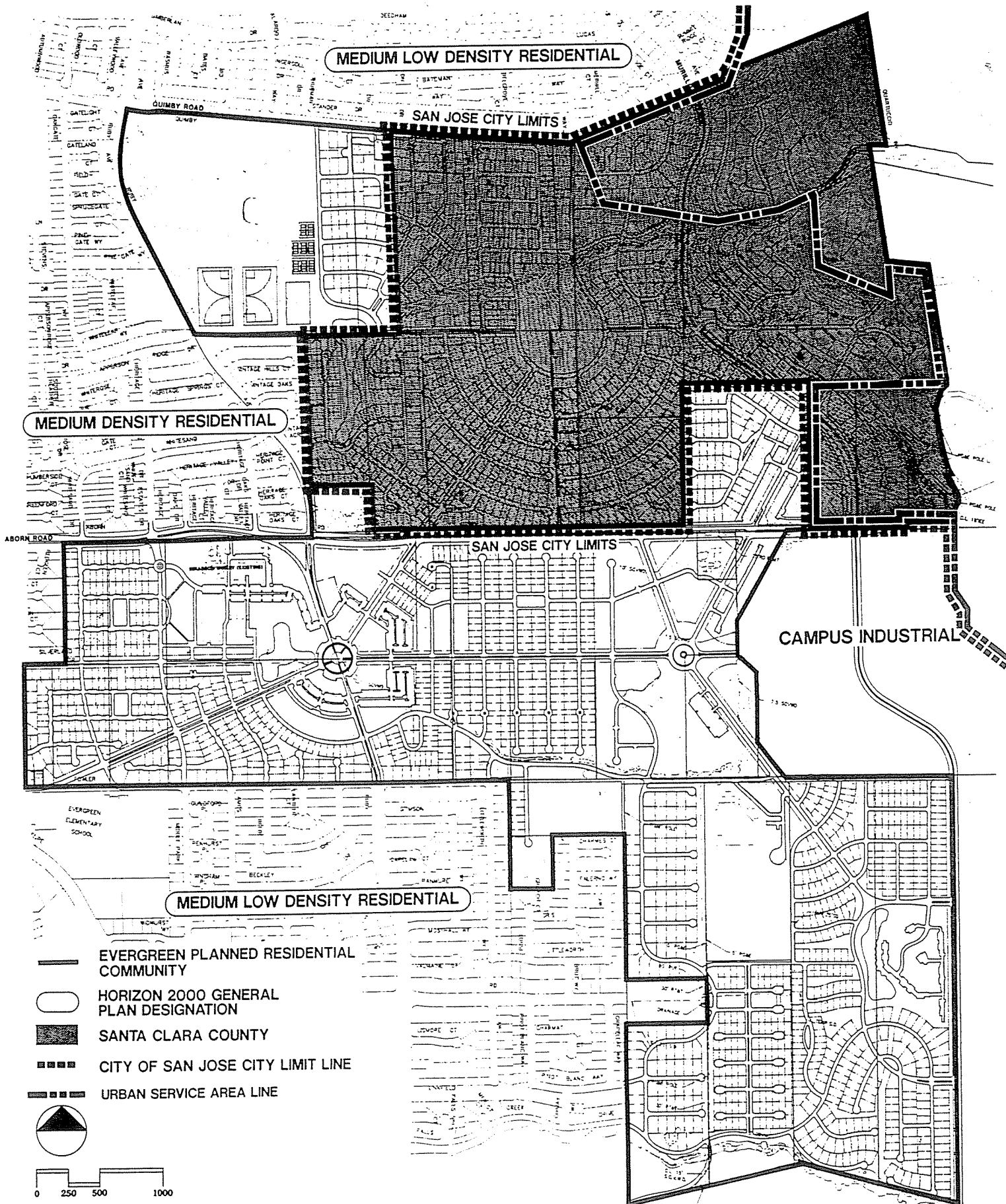
### GOALS, POLICIES, AND SPECIFIC PLAN RESPONSES

This Specific Plan will set forth the location, character, and intensity of land uses envisioned for the Evergreen Specific Plan area (Exhibit 2). The following goals and policies have been set forth by the City of San Jose's *Horizon 2000 General Plan* to enhance the quality for life of the people of San Jose. The Evergreen Specific Plan was conceived as a response to these goals and policies, and the following summary identifies the applicable goals and policies as well as the Specific Plan response.

#### 1. Urban Conservation

**Goal:** Improve the existing quality of life and create a stable, mature community.





## CITY LIMITS OF SAN JOSE

### CITY OF SAN JOSE EVERGREEN SPECIFIC PLAN

**Policy:**

1. In the development review process and in designing service and capital facility programs, the City should strive to create an environment in which the highest value is placed on people.
2. The City should encourage new development which enhances the desirable qualities of the community and existing neighborhoods.

**Response:**

*A comprehensive analysis of the service and capital facility programs necessary to support the development of the Specific Plan area was prepared. The results of this analysis was incorporated into the project in an effort to go beyond the established standards of services set by the City. The Evergreen Specific Plan was designed in a manner that gives great prominence to community structures such as the Fowler Creek Park Community Center, the elementary schools, and its use of open space throughout the project.*

**2. Community Identity**

**Goal:** Enhance the sense of community identity in San Jose.

**Policy:**

1. The City should encourage the development of a more compact, cohesive pattern of urbanization with definite, identifiable boundaries that more readily create a sense of community identity.
2. The City should foster the participation of residents in local government decision-making and in the social, cultural, and recreational activities of the community.

**Response:**

*The General Plan amendment for the subject area approved by the City of San Jose in 1989 established a density which promotes a more compact urban community that, at the same time, is compatible with the existing communities which surrounded the project site. The Evergreen Specific Plan Task Force, made up of property owners, community members, and appointed and elected representatives of the City of San Jose, has been an important component of the design process and has presented its recommendations to the Planning Commission and City Council prior to adoption.*

### 3. Neighborhood Identity

**Goal:** Enhance the sense of neighborhood identity in San Jose.

**Policy:**

1. Neighborhood groups should have input to the decision-making process in City government.

**Response:**

*In addition to the Evergreen Specific Plan Task Force, two community meetings were held to present information gathered by the consultant team, the direction of the Task Force, and receive input from the community.*

**Policy:**

2. City services and facilities should be equitably distributed throughout the community to the extent feasible.
3. Public and private development should be designed to improve the character of existing neighborhoods. Factors that cause instability or create urban barriers should be discouraged or removed.
4. Neighborhoods should include places for interaction among residents such as parks, community centers, schools, commercial areas, churches, and other gathering points.

**Response:**

*The Evergreen Specific Plan has provided a variety of amenities designed to serve both the existing neighborhoods and the new project area. New amenities include two elementary schools, Fowler Creek Park, the Village Center, and open space areas. The Specific Plan, through its use of internal trail systems and landscaped boulevards, provides an "open" community which provides a means of circulation beyond the use of the automobile.*

### 4. Balanced Community

**Goal:** Develop a balanced and complete community in terms of land use distribution and densities, housing types and styles, economic development and job opportunities for social and cultural expression.

**Policy:**

1. The City should foster development patterns which will achieve a whole and complete community in San Jose, particularly with respect to a balance between jobs and economic development on the one hand, and housing resources and a resident work force on the other.
2. Varied residential densities, housing types, and styles should be equitably and appropriately distributed throughout the community and integrated with transportation systems.
3. Encouragement should be given to achieving a social, economic, and housing mix in all neighborhoods.
4. Business and industry should be encouraged to provide job opportunities for all members of the community's work force.

**Response:**

*The property immediately east of the project site is designated as "Campus Industrial" and will ultimately employ up to 12,000 people. In response to this future activity, the Specific Plan designated a variety of residential product types including townhomes, carriage units, and apartments to provide a mix of housing types to provide housing for these employees as well as the entire community as a whole.*

*In addition, the Village Center was developed in an effort to address a portion of the commercial demands that will be created by the Campus Industrial areas as well as the Evergreen residential community.*

## **COMMUNITY DEVELOPMENT**

### **1. Land Use**

#### **A. Residential Land Use**

There are a wide variety of residential neighborhoods in San Jose, each with its own character defined by setting, housing types, densities and, in some cases, cultural heritage. The environment and livability of existing residential neighborhoods are an intangible but important community resource to be preserved. Similarly, these qualities should be fostered in future neighborhoods. To this end, the *Residential Land Use* goals and policies reflect concerns for the protection of neighborhoods from incompatible land uses, the adequacy of public facilities and services, and protection from hazards.

The *Residential Land Use* policies also reflect the City's objective to promote higher density residential development in the future than was typical in the past. This objective recognizes that remaining vacant land resources are finite and should be used as efficiently as possible, that the relative affordability of housing is enhanced by higher densities given the rising price of land, and that higher densities make the delivery of public services more cost-effective. A high standard of site planning and architectural design quality can make higher density housing attractive to both the consumer and the neighborhood where it is located.

The *Residential Land Use* goals and policies are primarily guidelines for the physical development of residential neighborhoods and proximate land uses. The Housing goals and policies, on the other hand, address the maintenance, rehabilitation, improvement, and development of housing, particularly relating to affordability.

**Goal:** Provide a high quality living environment in residential neighborhoods.

**Policy:**

1. Residential development at urban densities should be located only where services and facilities can be provided.

**Response:**

*The development of the plan area will be served by the City of San Jose and all infrastructure necessary to serve the plan area will be funded through the Evergreen Financing Plan.*

**Policy:**

2. Residential neighborhoods should be protected from the encroachment of incompatible activities or land uses which may have a negative impact on the residential living environment. In particular, non-residential uses which generate significant amounts of traffic should be located only where they can take primary access from an arterial street.

**Response:**

*The future high school site is located at the intersection of Ruby Avenue and Quimby Road, which would provide adequate circulation to and from the school site. Churches are permitted as a secondary use in the single family residential areas of the project.*

**Policy:**

3. Higher residential densities should be distributed throughout the community. Locations near commercial and financial centers, employment centers, the light rail transit stations, and along the bus transit routes are preferable for higher density housing.
4. Due to the limited supply of land available for multiple family housing, public/quasi-public uses, such as schools and churches, should be discouraged in areas designated for residential densities exceeding twelve units per acre on the Land Use/Transportation Diagram except in the Downtown Core Area.

**Response:**

*The areas of higher residential densities have been distributed in three different locations near the Campus Industrial and Village Center areas. Public/quasi-public uses have been removed from the permitted and conditional permitted uses for the higher residential categories of the Specific Plan areas.*

**Policy:**

5. Residential development should be allowed in areas with identified hazards to human habitation only if these hazards are adequately mitigated.

**Response:**

*An environmental impact report has been prepared for the Evergreen Specific Plan, General Plan Amendments, Planned Development Zoning, and identifies any potential environmental hazards or impacts and outlines mitigation measures for the implementation of the project.*

**Policy:**

11. Residential developments should be designed to include adequate open spaces in either private yards or common areas to partially provide for resident's open space and recreation needs.

**Response:**

*The Evergreen Specific Plan provides yards, pocket parks, trail systems, and landscaped boulevards, providing a variety of open space areas for both active and passive recreational needs.*

**Policy:**

13. In the design of lower density single family residential developments, particularly those located in the Rural Residential, Estate Residential, and Low Density Residential categories, consideration should be given to the utilization of public improvement standards which promote a rural environment, including such techniques as reduced street right-of-way widths, no sidewalks, and private street lighting.

**Response:**

*The area of the Specific Plan designated as "Hillside" lots is encouraged to promote a rural environment through the reduction of public improvement standards while still maintaining adequate public safety.*

**Policy:**

15. Residential development should be designed with limited access to arterial streets as follows:
  - No direct frontage access on six-lane arterials or within 350 feet of the intersection of two arterials.
  - Direct frontage roads, corner lots, open-end cul-de-sacs or other street design solutions for access are encouraged.

**Response:**

*The circulation system of the Specific Plan meets these or exceeds these standards as listed above.*

**Policy:**

16. Bed and breakfast inns may be located on properties designated for residential land use, regardless of density, provided that parking and other possible impacts on the surrounding neighborhood can be satisfactorily mitigated.

**Response:**

*Bed and breakfast inns are permitted within the Village Center, and would be subject to review and approval by the City of San Jose to ensure compliance with established City policies.*

## B. Commercial Land Use

The commercial land use policies reflect the need to locate new commercial uses in the community which facilitate convenient shopping and easy access to professional services and which contribute to the economic base of the City. Redevelopment of existing commercial strips and areas, and the conversion of existing structures to more appropriate uses should result in the upgrading of these areas.

**Goal:** Provide a pattern of commercial development which best serves community needs through maximum efficiency and accessibility.

### **Policy:**

1. Commercial land in San Jose should be distributed in a manner that maximizes community accessibility to a variety of retail commercial outlets and services and minimizes the need for automobile travel.
2. New commercial uses should be located in existing or new shopping centers or in established strip commercial areas. Isolated spot commercial developments and the creation of new strip commercial areas should be discouraged.
5. Commercial development should be discouraged from locating or expanding within established residential neighborhoods when such development will have a negative impact on the character and livability of the surrounding residential community.

### **Response:**

*The Evergreen Village Center surrounds the primary rotary hub of the circulation system providing maximum accessibility from both the existing and new neighborhoods. The Village Center is close enough to the existing neighborhoods to allow foot traffic, but buffered by Aborn and Fowler Roads to limit any through traffic into the existing neighborhoods.*



### 3. Urban Service Area

The City first adopted a set of *Urban Development Policies* in 1970 to direct development to those areas where services and facilities could be provided. Because these policies deal with the timing and staging of development and are so closely related to other General Plan growth management policies, they were incorporated into the Plan in 1976. The *Urban Service Area* goals and policies address services provided by the City as well as those provided by other public agencies, such as flood control, public schools, and regional transportation.

The Urban Service Area policies are applicable to the entire development review process, including the annexation of territory to the City. As such, the implementation of these policies should be coordinated with the Local Agency Formation Commission (LAFCO).

**Goal:** Insure that San Jose's future growth will proceed in an orderly, planned manner in order to provide efficient and economical public services, to maximize the utilization of existing and proposed public facilities, and to achieve the equitable sharing of the cost of such services and facilities.

**Policy:**

1. The General Plan designates an Urban Service Area where services and facilities provided by the City and other public agencies are generally available, and where urban development requiring such services should be located.
2. The Urban Service Area should be expanded only when it can be demonstrated that existing facilities and services are available and adequate to serve the proposed expansion area. If existing facilities are not adequate, alternate consideration may be given to proposed facilities in the adopted Capital Improvement Program or similar programs of other public agencies. If existing and/or proposed facilities are not adequate to serve the proposed expansion area, the Urban Service Area may be expanded if it can be demonstrated that all necessary facilities will be provided by the developer(s).
6. It is City, County, and LAFCO policy that existing and future urban development should be located within cities. This policy should be implemented through the City's existing agreement with the County which requires that incorporated properties within the Urban Service Area either annex to the City, if possible, or execute a deferred annexation agreement prior to approval of development. The City should also encourage the County and LAFCO to join in cooperative efforts to seek the annexation of urbanized County pockets within the Urban Service Area.

7. Since the provision of sanitary sewers is an urban service, and development served by sanitary sewers is thereby urban, the expansion of sanitary sewer districts is discouraged for areas planned in non-urban uses outside the Urban Service Area.

**Response:**

*The Urban Service Area will be expanded to include all of the Evergreen Specific Plan based on infrastructure studies which have determined that all necessary facilities will be provided by the plan area. Areas within the boundaries of the Evergreen Specific Plan that are not presently within the City limits of San Jose will require annexation prior to development.*

#### **4. Urban Design**

The design of the community affects the quality of life, the character of neighborhoods, and the livability of the city. Members from all segments of the community are involved in the decision-making of the development review process which determines design. The multitude of decisions involved result in the final form and character of the city environment. The public's interest in fostering the highest quality of life is expressed through policies on urban design standards in order to incorporate aesthetic considerations in the development review process.

**Goal:** Require the highest standards of architectural and site design for all development projects, both public and private.

**Policy:**

1. The City should continue to apply strong architectural and site design controls, on all types of development for the protection and development of neighborhood character, and for the proper transition between areas with different types of land uses.
2. Zoning regulation should require, wherever appropriate, that private development include adequate landscaped open space and provisions for ongoing landscape maintenance.

**Response:**

*The Evergreen Specific Plan has provisions which identify distinct architectural character, specific site design layouts for the entire plan area and open space amenities to strengthen the individual character of each of the distinct residential product types as well as the Village Center commercial area. An integrated financing district is planned to provide landscape maintenance to a standard above and*

*beyond the typical city standards along public streets as well as in the pocket parks and trail systems.*

**Policy:**

3. Residential subdivisions should be designed to provide for internal circulation within neighborhoods and to prevent through traffic from traversing neighborhoods.
4. Residential developments which are adjacent to parks or open spaces should be encouraged to provide direct access to, and common open space contiguous to, such areas.

The Specific Plan has a variety of trail systems throughout the plan area which provide pedestrian and bicycle access to the elementary schools, Fowler Creek Park, and other open space features such as Montgomery Hill Park.

**Policy:**

5. The design review process should take into consideration the long term maintenance ramifications of the design of private streets and other private infrastructure improvements.

**Response:**

*Policy noted and may be incorporated into a Lighting and Landscaping District or similar mechanism to provide long-term maintenance of the project's amenities.*

6. Proposed structures adjacent to existing residential areas should be architecturally designed and sited to protect the privacy of the existing residences.

**Response:**

*The project has established specific criteria for the areas of the project which abut existing residential neighborhoods in order to integrate with these areas.*

**Policy:**

7. The City should require the undergrounding of distribution utility lines serving new development sites as well as proposed redevelopment sites. The City should also encourage programs for undergrounding existing overhead distribution lines. Overhead lines providing electrical power to light rail transit vehicles and high tension electrical transmission lines are exempt from this policy.

**Response:**

*Policy noted.*

**Policy:**

8. Design solutions should be considered in the development review process which address security, aesthetics, and public safety.

**Response:**

*The Evergreen Specific Plan has been reviewed by the Evergreen Technical Advisory Committee, which is made up of various City departments, which addressed the issues of security, aesthetics, and safety.*

**Policy:**

12. In order to preserve and enhance the scenic and aesthetic qualities of the natural terrain, development on slopes exceeding 7% should conform to the following guidelines:

- Planned Development zoning is preferable for its flexible design techniques such as clustering, variable lot sizes, and varying setbacks in order to maximize residential densities.
- Construction techniques and housing types adaptable to a variable terrain, such as cluster housing, split pads, and stepped foundations, should be utilized where appropriate. Conventional, single flat pad lots should ordinarily be discouraged.
- Consideration should be given to the siting of homes for privacy, livability, solar, and wind conditions. Siting should take advantage of scenic views but should not create significant visual impacts affecting public places and other properties.
- The preservation of existing trees, rock outcroppings, and other significant features should be encouraged.
- When grading or recontouring of the terrain is proposed, it should be done in such a way as to preserve the natural character of the hills, whenever possible.
- Because street construction on slopes often requires a disruptive amount of grading, modified street sections designed for both utility and minimum grading should be encouraged.

- Development should be discouraged on slopes exceeding 30% and on ridgelines.

**Response:**

*The majority of the Evergreen Specific Plan lies on terrain below slopes exceeding 7%. In an effort to preserve as many existing trees as possible, the project's grading and site design has been developed so that existing terrain, existing significant trees, and significant features such as Fowler and Quimby Creeks, have been respected.*

**Policy:**

13. At the edge of the valley floor, development should incorporate loop streets and cul-de-sacs rather than streets stubbed into lands planned for non-urban use in order to minimize development pressures on such non-urban areas.
14. New urban development should be designed to minimize impacts in areas with an established and permanent rural or semi-rural character, often typified by large-lot "ranchette" development.

**Response:**

*In the areas of the plan which approach the non-urban hillside, cul-de-sac, and loop streets have been proposed in an effort to minimize impacts on the hillsides and to establish a semi-rural character in the hillside lot areas.*

**Policy:**

15. In order to realize the goal of providing street trees along all residential streets, the City should:
  - Prepare and update, as necessary, a master plan for street trees which identifies approved varieties.
  - Require the planting and maintenance of street trees as a condition of development.
  - Prepare a program for management and conservation of street trees which catalogs street tree stock replacement and rejuvenation needs and identifies alternative means by which these needs can be satisfied. This program should be reviewed periodically as part of the Capital Improvement Program process.

**Response:**

*Street trees will be provided in accordance with City of San Jose standards and maintained through a Lighting and Landscaping District.*

**Policy:**

16. When development is proposed adjacent to existing or planned parks or park chains, that development should include public park frontage roads, wherever feasible, in order to maximize access to park lands, to provide a reasonable separation between urban land uses and park lands without the use of "back up" design, and to maximize exposure of park lands for scenic and security purposes.

**Response:**

*All pocket parks and Fowler Creek Park include roads which maximize the visibility of the open space features as well as provide maximum access to the parks.*

**Policy:**

17. Development adjacent to creekside areas should incorporate compatible design and landscaping.

**Response:**

*Creek areas will be maintained in as natural a state as possible and new creek areas that will be created will be landscaped to be compatible with some level of recreational use.*

**Policy:**

18. To the extent feasible, sound attenuation for development along City streets should be accomplished through the use of landscaping, setback, and building design rather than the use of sound attenuation walls. Where sound attenuation walls are deemed necessary, landscaping and an aesthetically pleasing design shall be used to minimize visual impact.

**Response:**

*Policy noted and incorporated into the noise section of the environmental impact report of the Specific Plan.*

**Policy:**

20. As resources are available, the City should assign priority to the implementation of programs for the installation and maintenance of landscaping in median islands and back-up strips along major thoroughfares.

**Response:**

*The Evergreen Specific Plan has incorporated median landscaping along the portion of Aborn Road and Murillo/Yerba Buena Avenue. As discussed previously, an integrated financing district will address the maintenance of these features.*

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## Environmental Setting





## Chapter 4 - Environmental Setting

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This section summarizes environmental conditions within the project site. The environmental characteristics of the site help to define the parameters for the specific plan area. An accompanying environmental impact report (EIR), prepared to meet the requirements of the *California Environmental Quality Act* (CEQA), adopted Guidelines thereto, and Title 21 of the *San Jose Municipal Code* evaluates the environmental effects of the development outlined within this specific plan. The information provided within this section and the "Site Constraints Exhibit" (Exhibit 3) is to serve as an overview of the environmental constraints within the context of this specific plan.

### SITE CHARACTERISTICS

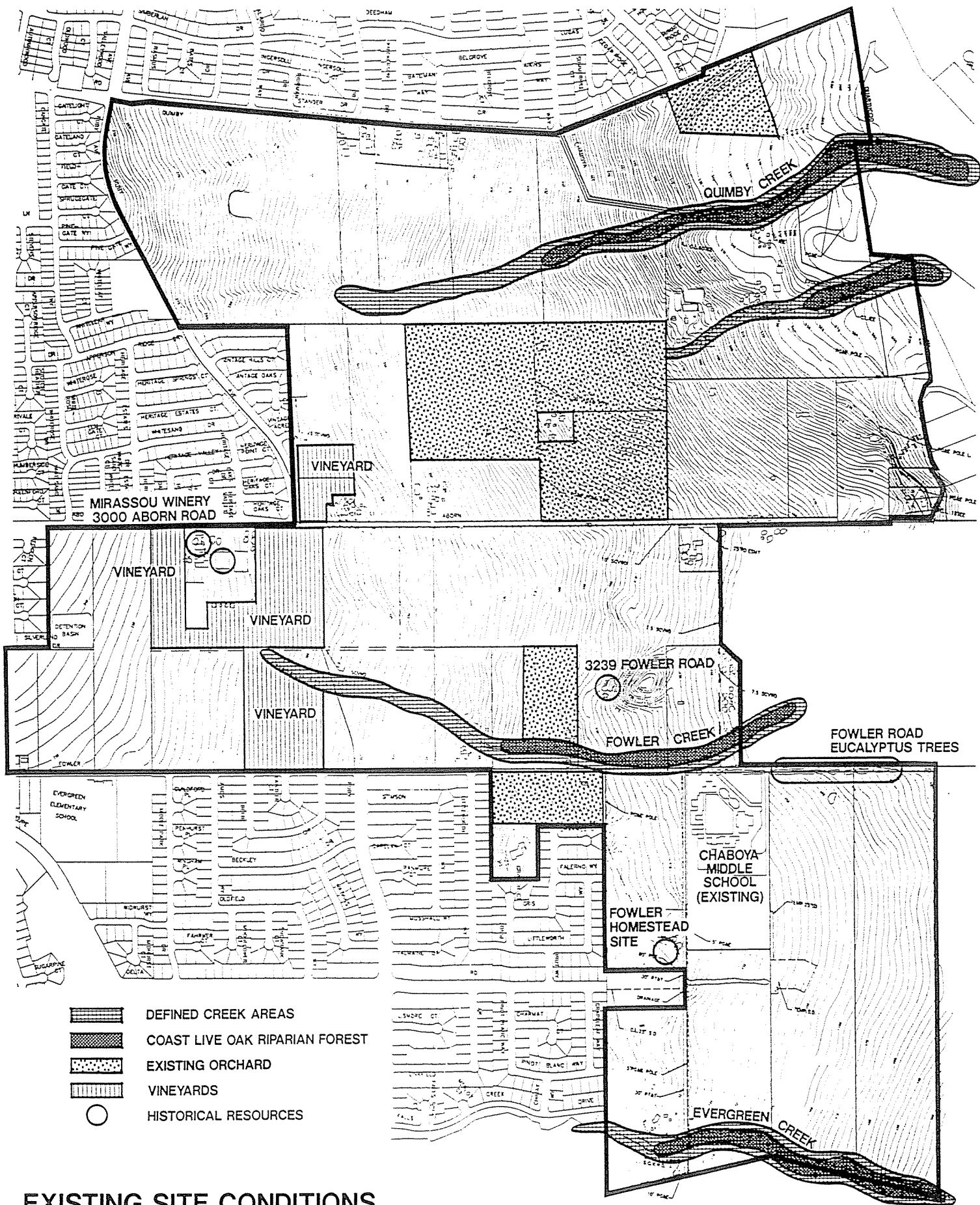
#### Existing Land Uses

The existing land uses within the Evergreen Specific Plan Area reflect the historic land uses of the Evergreen area since the late 1800's. The project area is characterized by agricultural uses such as hay production, stock grazing, grain cultivation, orchards, and the Mirassou Winery, all of which are traditional uses within the area.

#### Historic Resources

Twenty residential structures dot the project area, most of which are found along Quimby, Aborn, and Fowler Roads. Six of these structures were initially identified as potential historical structures and were evaluated by archival research, field survey, and subsurface testing. This research identified four areas which qualify as historical resources and will require environmental mitigation measures within the EIR. These structures are:

- A. The Mirassou Winery (3000 Aborn Road). Based on the long history of the Mirassou family's involvement in the winemaking business in Evergreen, and the integrity of the existing winery complex, the Mirassou Winery qualifies as a "Candidate City Landmark". The EIR recommends that the original residence (known as the "Heritage House"), the original winery buildings, and a portion of the vineyards be retained and the existing winery equipment be photographically documented prior to removal.
- B. 3239 Fowler Road. The residence on this site dates, at least, back to the 1876 ownership of James Lendrum; and may date back as far as 1866. Based on the criteria used by the historical resources consultant, this structure rates as a "Structure of Merit". The significance of the structure rests with its age and the example it provides of a modest rural farmhouse from the 1870's. Few good examples of this type of vernacular architecture from this early period



## EXISTING SITE CONDITIONS

CITY OF SAN JOSE  
EVERGREEN SPECIFIC PLAN

survive in the Santa Clara Valley today. The EIR recommends that project plans be evaluated to determine if the existing structure can be retained and integrated into the reconfigured Fowler Creek Park. If preservation is not possible, detailed documentation should be prepared by the Historic Landmarks Commission, and useful architectural elements from the building should be salvaged and stored for eventual reuse in the restoration of buildings of similar style and vintage. In addition, a detailed monitoring plan should be developed by a qualified historical archaeologist with the developer and approved by the City of San Jose prior to removal of the structure. All earthmoving activities within fifty meters of the house be monitored by a qualified historical archaeologist.

- C. Fowler Homestead Site (3262 Fowler Road). This parcel was originally part of the 192 acre parcel acquired by Andrew Jackson Fowler in the early 1870's. Historic maps indicate that the location of the original A. J. Fowler homestead was located on this parcel near the dense growth of eucalyptus trees, poison oak, and other vegetation south of Fowler Road. A survey of this area revealed remnants of a frame tower, possibly for a water tank in this area. Since the possibility of post-1870 subsurface archaeological features are likely to exist on this parcel, it is recommended that a detailed monitoring plan be developed by a qualified historical archaeologist and the developer, approved by the City of San Jose prior to any construction within the area, and all earthmoving construction activities be monitored by a qualified historical archaeologist.
- D. Fowler Road Eucalyptus Trees. These trees, presently seen flanking the north and south sides of Fowler Road from Chaboya Middle School to the City's water reservoir at the end of Fowler Road, were planted by A.J. Fowler during the latter half of the 19th century. The present development plan calls for the rerouting of Fowler Road from Littleworth Way, and allows for the preservation of the eucalyptus trees. However, the extreme age of the trees, the very thing that gives them historical value, also indicates that the trees may be coming to the end of their life-span. The environmental impact report's arborist has examined these trees and indicated which of the trees are worth preserving, the safety considerations of placing residential structures in and around mature eucalyptus trees should be balanced against the historical value of the trees and their life-span.

### **Geology and Soils**

The soils of the site are moderately expansive, have good strength characteristics, and there are no liquefiable soils present.

## **Seismic Effects**

A total of ten trenches were excavated across previously mapped positions of the Evergreen, East Evergreen, Pappani, and Quimby faults. These trenches, as well as geophysical surveys and magnetometer surveys, found no evidence of active faults and no fault-related building setbacks will be recommended. Additional site-specific studies will be required on a parcel-by-parcel basis as identified in the EIR.

## **Vegetation and Wildlife**

Five different vegetation types have been identified within the specific plan project area. The following categories are discussed within the EIR:

- 1) Non-native grasslands established primarily for hay production.
- 2) Orchards and vineyards.
- 3) Diablan sage scrub found generally along the steeper hillside areas of the project.
- 4) Coast live oak riparian forest found generally within Quimby, Fowler, and Evergreen Creeks.
- 5) Exotic and non-native vegetation associated with the existing residential dwellings throughout the project site.

A detailed tree survey was undertaken to evaluate all nonriparian trees 12 inches (12") and greater in diameter within the Specific Plan area. There are a total of 495 trees within this category. The most significant trees in the project area are the eighty (80) Coast Live and Valley oak trees. (This total does not reflect the many oak trees within the riparian creek corridors, which were evaluated in a different manner.) The design of the specific plan was conducted in such a manner to specifically avoid impacting significant trees as identified by the project arborist.

The riparian habitat areas which lie around Quimby, Fowler, and Evergreen Creeks constitute the greatest area of concentrated vegetation and wildlife in the project area. Wildlife traditionally uses the creeks as corridors to move to and from the hillside areas into the lower elevations. Vegetation also is concentrated in these corridors due to the relatively higher amounts of water which flow out of the hills during the winter rains. The specific plan has been developed in a manner as to minimize the disturbance in and around the three creek corridors so that the creeks and associated vegetation and wildlife will remain undisturbed. In areas where creek crossings are proposed, detailed transects have been prepared to provide a detailed survey of the creek vegetation.

No rare, endangered, threatened vegetation, or wildlife species were found within the specific plan area, although some properties within the plan area will require additional testing prior to development.

## **Waterways and Flooding**

Areas adjacent to Quimby and Fowler Creeks are subject to flooding, as are some areas downstream of the project site along Lower Silver Creek. The proposed plan would result in a small percentage increase in peak discharge and volume of stormwater run-off and 10 acre-feet of on-site storm water retention will mitigate the project-related increase in run-off.

Development in and around the existing creeks can create the potential for conflicting recommendations in order to maintain the natural settings of the creeks versus the need to address the storm water run-off. The creeks in the Evergreen area have traditionally left the creek banks and caused flooding problems for many years. The challenge is to determine how to deal with the storm water run-off during peak flows without widening the creek banks, which would remove the existing riparian and vegetation habitat.

The specific plan proposes a system of underground stormwater pipes to carry both off-site and on-site stormwater run-off through the specific plan area to existing pipe inlets at the project's western boundaries. The year-round water features on Quimby and Fowler Creeks will be designed to accommodate the additional stormwater retention during peak flow periods, while serving as project amenities by maintaining a lake appearance throughout the year. The natural stream channels of Quimby, Fowler, and Evergreen Creeks are proposed to remain and be enhanced to carry storm run-off, and debris basins at the head of each stream will divert peak flows into the underground pipe systems to preserve the natural conditions of the existing creeks.

## **Archaeology**

A review of existing maps and records on file at the Archaeological Regional Research Center revealed no previously recorded prehistoric archaeological sites within the project area. A field inspection of the project site was conducted and no new archaeological sites were discovered; however occasional isolated aboriginal artifacts were observed.

Given the potential for subsurface material in and around the creek corridors, further mechanical testing is recommended within one hundred feet (100') of the centerline along the natural drainage courses of Quimby, Fowler, and Evergreen Creeks. No further testing within the project area will be required.

## **Hazardous Materials**

A reconnaissance of the specific plan area was conducted in an effort to identify the potential for hazardous materials. In addition, regulatory agency records have been reviewed and site history research has been conducted.

Although no hazardous materials were specifically observed or identified, the project area does contain four underground storage tanks. Two of these tanks are on the Mirassou Winery and are permitted and monitored. Two other tanks have reportedly not been used for several years and do not have permits. A total of nine above-ground fuel storage tanks have been observed. Slight soil staining has been noted under the tanks, but the potential for soil contamination resulting from leaking or spillage during dispensing or filling is probably small. Specific mitigation measures involving these tanks will be addressed within the environmental impact report and will involve additional on-site testing prior to development.

The historical use of the project area for various agricultural uses infers the use of agricultural chemicals. Most of the pesticides in use today, if uniformly applied in accordance with the manufacturer's specifications do not present the potential for hazardous waste. However, concentrated pesticides resulting from leakage or spillage in chemical storage areas can result in substantial soil contamination. In addition, soils containing residual concentrations of DDT or its derivatives greater than 1.0 parts per million are considered hazardous waste under State of California regulations. Although DDT has been banned from use since the 1970's, it was widely used for pest control in orchards, which once covered a great deal of the project site. Individual on-site soils testing for these materials will be required on a site-by-site basis prior to development.

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## The Plan





## Chapter 5 - The Plan

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**T**he Evergreen Specific Plan is a marriage between the practical effort to address the constricted circulation system in the Evergreen area and the aesthetics of designing a distinctive community. The challenge in the urban design process is to create a project that merges the practical and the aesthetic into a cohesive package. It is hoped that the Evergreen Specific Plan will serve as more than just new subdivisions among old, but as the foundation of a new community.

### **Varied Amenities**

Major parks and playfields, schools, a community center, lakes, hillside and riparian open spaces, extensive streetside and focal landscaping, pocket parks, and tot lots are distributed throughout the plan area. These amenities provide neighborhood focus and differentiation while fulfilling specific area needs.

### **Prominent Location for Public Buildings**

The northern elementary school, community center, fire station, bandstand, and village center plaza park are all given sites of key prominence.

### **A Traditional Community-Serving Village Center**

The Village Center surrounds the primary radial hub of the circulation system. It is open and visible from Aborn Road and adjacent to the existing Mirassou Winery visitors center.

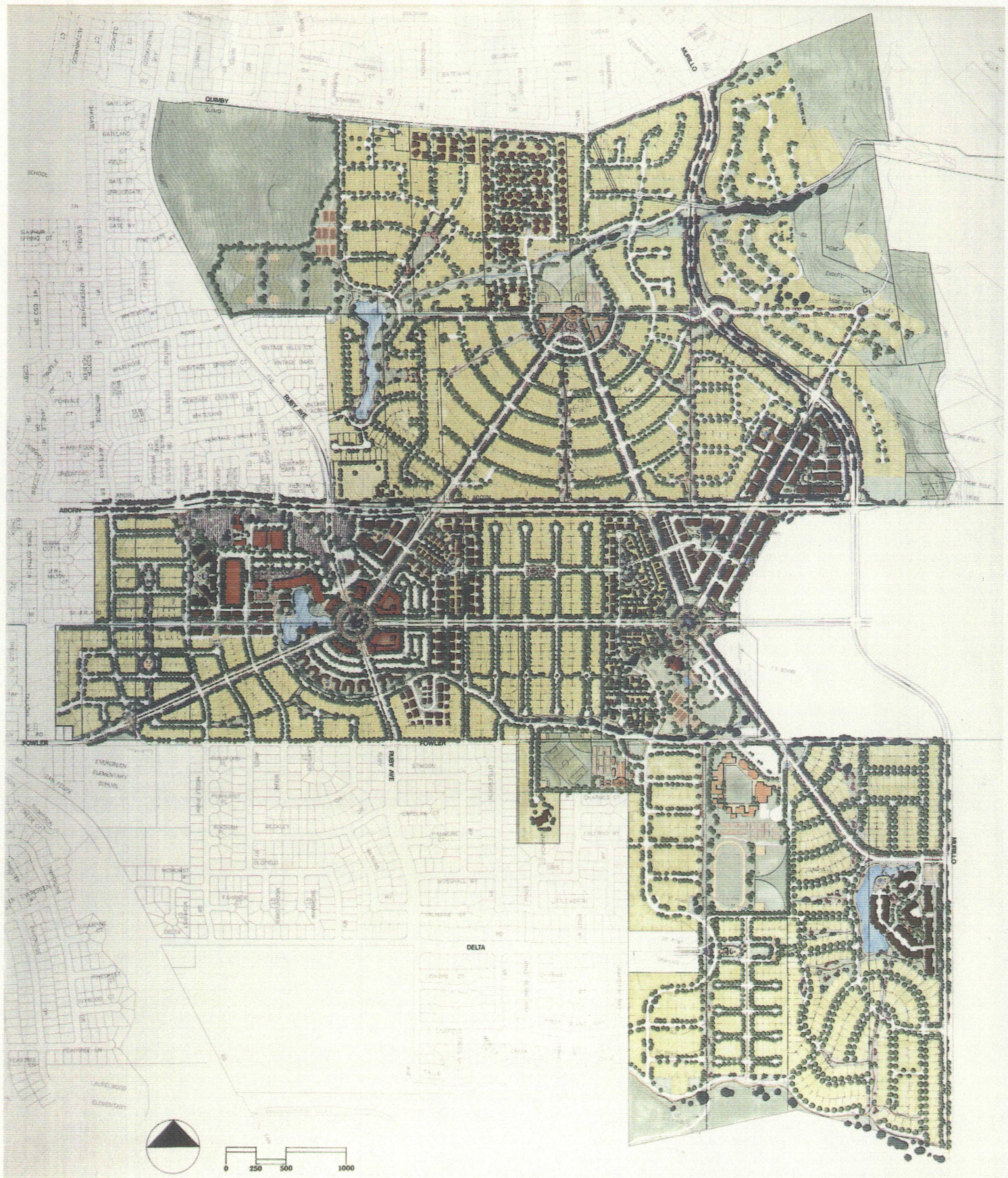
### **A Finely Grained Mix of Residential Densities**

The street patterns, amenities, and open space define more than thirty individual residential neighborhoods ranging in size from just over thirty units to approximately 180 units. These relatively small "product runs" provide a fine mix of housing types, and densities. From multi-family apartments at 12 - 25 units per acre through townhomes, carriage homes, small lot single family, to custom hillside estate lots, the Evergreen Specific Plan offers a wide range of residential opportunities.

### **Circulation**

More than just circulation; streets, walkways, and trails can provide organization, form, vistas, and focus. The Evergreen Specific Plan protects residents from major through-traffic corridors with ample setbacks, sound walls, and landscape buffers while at the same time embracing inter-community street patterns of radial streets and focal rotary hubs. These rotaries and radials provide dramatic axial vistas to important civic structures, organize and link individual neighborhoods, and, in conjunction with trails and creek corridors, tie together major community amenities.





**ILLUSTRATIVE SITE PLAN**  
**CITY OF SAN JOSE**  
**EVERGREEN SPECIFIC PLAN**



## **ELEMENTS OF THE LAND USE PLAN**

A successful community is more than a collection of homes, it recognizes the various elements that are needed and desired by its residents. A community requires a balance between the provision of housing, employment, entertainment opportunities, public parks and recreational services, and services such as markets, shops and restaurants. As discussed in the previous chapters, the Evergreen Specific Plan includes a variety of existing land uses, required plan elements such as schools and parks, a commercial area, and a comprehensive infrastructure system for the Specific Plan area.

### **Existing Land Use Conditions**

To the north, west, and south of the project area lie a variety of existing residential subdivisions. To the east lie areas of non-urban hillsides and property slated for development as campus industrial facilities. In establishing residential densities throughout the project site, it is important to recognize these existing land uses and how they determine the future interaction between the present and future land uses in designating adjoining land uses and densities.

The most notable existing land use within the specific plan area is the Mirassou Winery and adjoining vineyards. This facility is located on the south side of Aborn Road and is arguably the most visible landmark within the entire plan area. Along Fowler Road lies the Chaboya Middle School, which is expected to open in the fall of 1991. On the north side of Fowler Road, and near the new middle school, is the Fowler Creek Park site. Although the City of San Jose owns the park property and has an approved master plan for the site, there are no current plans for the City to construct the park.

### **Required Elements Within the Specific Plan**

In addition to the existing land uses discussed above, it is necessary to incorporate new uses into the plan. Based on the expected population of the Evergreen Specific Plan area, two new elementary schools are necessary to serve the future children of the community. The East Side Union High School District has expressed an interest in developing a high school on fifty acres of property the District owns at the intersection of Quimby Road and Ruby Avenue. (The specific plan area's future population will not, by itself, generate enough students to require a new high school, but will incrementally contribute to an area-wide need.)

In addition, the Specific Plan includes the provision for a new fire station, the development of Fowler Creek Park, the dedication of land to the Montgomery Hill Park, creek trail corridors, pocket parks, trail systems, and a commercial village.

The City of San Jose has established several housing goals and policies in the *Horizon 2000 General Plan* that relate to the development of a variety of housing types and densities. By providing a variety of housing styles, types, and densities, the

Evergreen Specific Plan can provide housing for a variety of future residents. The provision of this housing diversity is expected to be instrumental in the successful development of the adjacent campus industrial property and to give future residents an opportunity to live and work in the same area.

## **LAND USE PLAN**

The Land Use Plan, as shown in Exhibit 5, is a schematic representation of the Evergreen Specific Plan. The plan consists of eleven separate land use categories. Of the eleven, nine of the categories are stratifications of residential densities.

### **Residential**

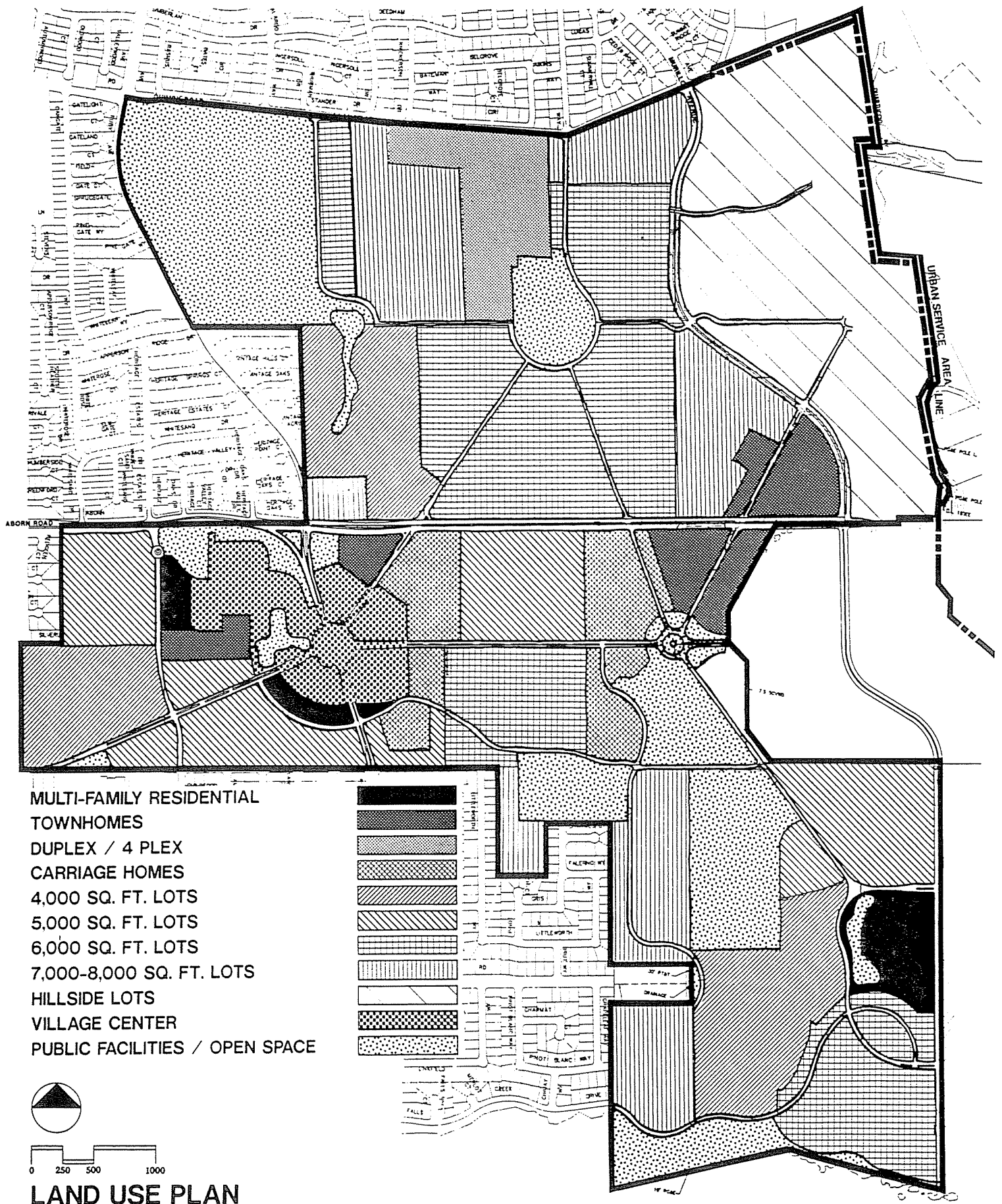
The Evergreen Specific Plan proposes a variety of residential densities to provide the housing types previously discussed above. The higher residential densities (multi-family apartments, townhomes, and carriage homes) are located adjacent to the western campus industrial area and the central Village Center commercial area. Single family residential areas are located adjacent to the existing single family neighborhoods. Where small lot single family areas occur next to existing 6000 and 8000 square foot lots, similar sized lots are required along the adjoining edge. In the area east of Murillo Avenue, larger hillside lots are proposed up to the 15% slope non-urban hillside boundary. (Chapter Eight "Private Development" provides an in-depth discussion on the concepts for each residential density as well as providing development criteria.)

### **Commercial**

The Evergreen Specific Plan includes a commercial village center adjacent to the existing Mirassou Winery and visitor's center. The Village Center surrounds the primary radial hub of the circulation system. It is open and visible from Aborn Road. (Chapter Eight "Private Development" provides an in-depth discussion on the Village Center as well as providing development criteria.)

### **Public Facilities/Open Space**

The Evergreen Specific Plan designates two new elementary school sites within the plan area, as well as reconfiguring the boundaries of the Fowler Creek Park to take advantage of the circulation pattern of the plan. In addition, the land use plan also specifies the future high school site at the corner of Ruby Avenue and Quimby Road as well as the existing Chaboya Middle School along Fowler Road. The land use plan also identifies land proposed to be dedicated to Montgomery Hill Park in the southeast corner of the project, south of Evergreen Creek. (Chapter Seven "Major Parks and Recreational Amenities" provides an in-depth discussion on the community amenities contained within the Evergreen Specific Plan as well as providing development criteria.)



## **CIRCULATION PLAN**

The Circulation Plan found on Exhibit 6 is a schematic representation of the various circulation elements of the Evergreen Specific Plan. The plan area implements a hierarchy of five types of streets.

### **Major Arterial (Aborn Road)**

A major arterial accommodates the majority of movement of through-traffic and is designed to have few connecting streets. Aborn Road, the only major arterial within the project area, serves to move traffic to and from Capitol Expressway, which is a facility designed primarily for traffic movement on a regional scale.

### **Major Collectors (Murillo Avenue, Yerba Buena Avenue, Quimby Road, Ruby Avenue, Delta Road)**

Major Collectors are designed to serve internal traffic movements within an area and connect with major arterials. Major collectors do not handle long through trips but do provide access to abutting properties.

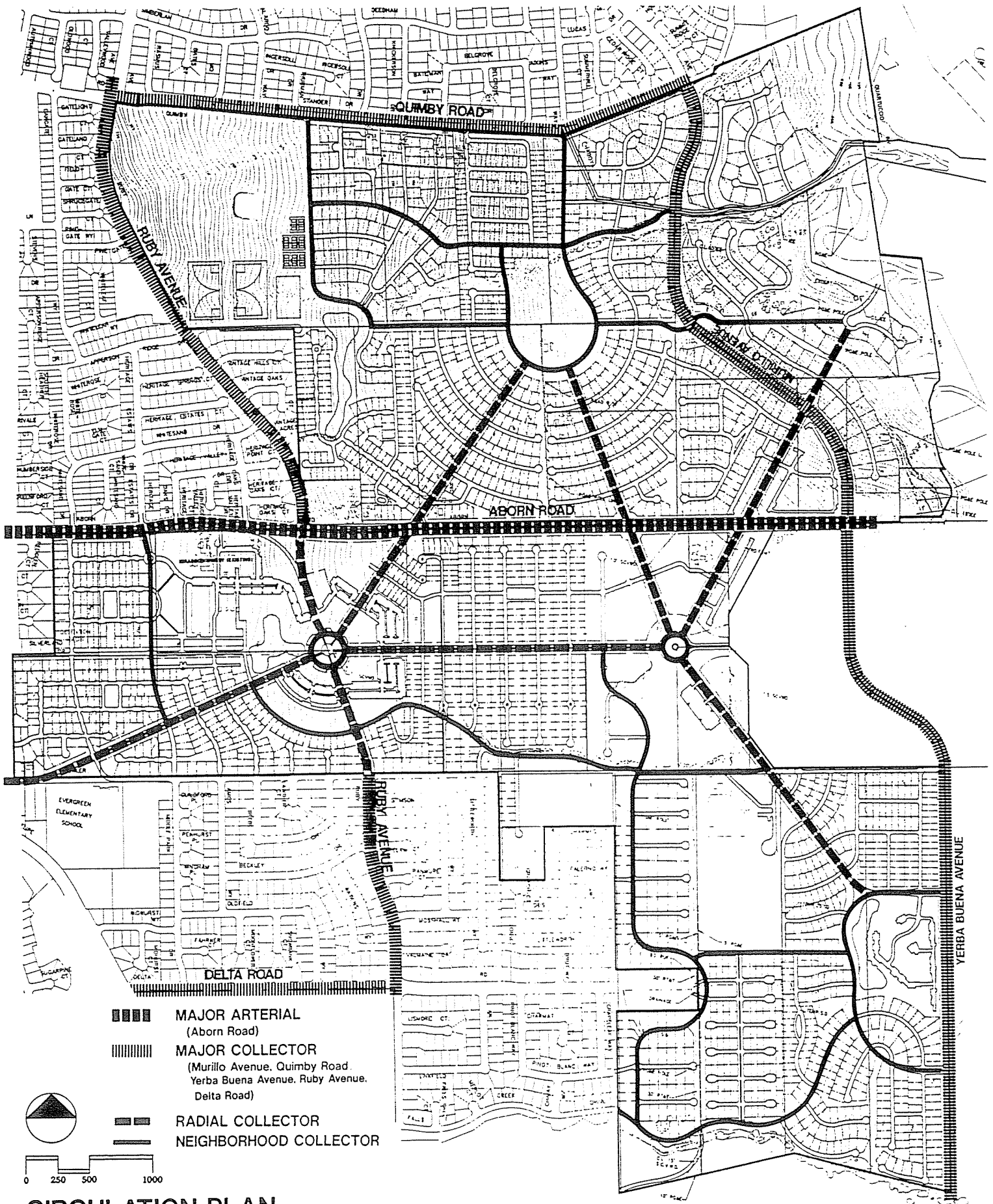
The Evergreen Specific Plan is bounded on the north by Quimby Road, which ultimately connects to Capitol Expressway. The plan area is also served by Murillo/Yerba Buena Avenue which crosses the site along the east. Murillo Avenue crosses the site from Quimby Road to Aborn Road at the base of the foothills serving as the boundary for the hillside lots. Murillo/Yerba Buena Avenue also serves as the project's eastern boundary south of Fowler Road. (Murillo Avenue changes names at Aborn Road and becomes Yerba Buena Avenue.) Murillo Avenue proceeds north to Tully Avenue and Yerba Buena Avenue ultimately connects with U.S. Highway 101.

Ruby Avenue connects northbound and southbound traffic to Aborn Road and Quimby Road and northbound traffic to Tully Road. Moving south of Aborn Road, there are no major destinations beyond the Village Center, and this segment will probably attract fewer trips.

Delta Road is an east-west collector street that runs parallel to Aborn Road. The primary purpose of Delta Road is to carry residential traffic within the existing neighborhoods to San Felipe Road, where the traveler has the option to go north to Aborn Road and beyond or south to Yerba Buena Avenue. The Evergreen Specific Plan has modified the Delta Road alignment within the project to avoid future "shortcut" trips through the residential neighborhoods to the future campus industrial areas.

### **Radial Collectors**

Radial collectors are designed to serve in the same fashion as the collector streets described above by handling internal traffic movements within an area and connect



## CIRCULATION PLAN

CITY OF SAN JOSE

EVERGREEN SPECIFIC PLAN



with major collectors and arterials. In the case of the Evergreen Specific Plan, the radial collectors serve not only the traffic function as described, but also provide dramatic axial vistas to important civic structures, organize and link individual neighborhoods, and, in conjunction with trails and creek corridors, tie together major community amenities.

### **Neighborhood Collectors**

In addition to the radial collectors, a system of neighborhood collector streets are used throughout the specific plan area to connect the various individual developments and allow better inter-community circulation.

### **Local Residential Streets**

Although not specifically called out in the Circulation Plan (Exhibit 6), the local residential streets provide access within individual neighborhoods with a minimum amount of street paving area to place street trees as close together as possible.

### **Pedestrian/Bicycle Circulations**

The Evergreen Specific Plan recognizes the importance of alternative ways to move throughout the community without using an automobile.

Within the Specific Plan area, an evolved system of boulevards with ample landscaped setbacks, internal trail systems, and creek corridors serve to tie the entire project together and provide pedestrian and bicycle access to Fowler Creek Park, the two elementary schools, Chaboya Middle School, the future high school, and the various pocket parks scattered throughout the community. In addition, this system provides access through the project site to the Silver/Thompson Creek trail system to the east, and regional foothill trail system to the west.

Chapter Nine "Infrastructure" provides additional information on the Evergreen circulation system.

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# Community Design Strategy



## Chapter 6 - Community Design Strategy

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The Community Design Strategy of the Evergreen Specific Plan is intended to create an infrastructure and amenity framework which, when combined with design guidance and control, will steer the growth of this new community toward a high quality and a truly memorable neighborhood district. As the community develops, a measure of success in achieving this goal will be evidenced by the extent to which the mention of Evergreen brings to mind images of uniquely pleasant spaces and unique, coherent, and endearing structures.

### IMAGERY AND IDENTITY

While a well organized land use and circulation plan can provide the infrastructural framework of a pleasant and convenient community, it, by itself, does not create a memorable place. Indeed, stylistically chaotic, contextually irrelevant, parking-surrounded, single use projects can reduce the best of plans, at least perceptually, to simply more homogenous sprawl. Imagery and identity deal with the perception of community, not in plan form or from an airplane, but from the ground as we live, work, recreate, and simply move about our "turf".

In defining a relevant identity for the Evergreen Specific Plan cues are taken from the site's natural features, climate, and history, as well as the larger context of San Jose and the Bay Area.

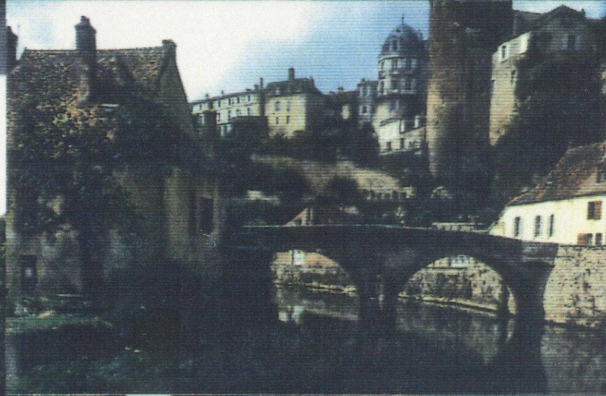
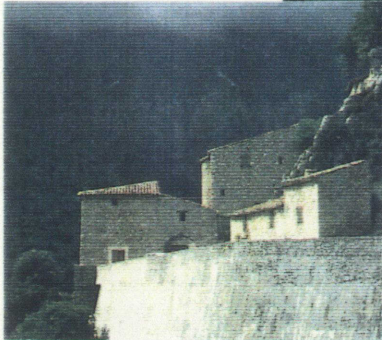
The Evergreen Specific Plan Area is a gently ( $\pm 5\%$ ) and very evenly sloping piedmont at the base of San Jose's eastern hills. It is traversed by several westerly flowing creek channels. These are in part natural riparian habitats and in part channelized waterways. Old orchards, hay crops, vineyards, natural grasslands, riparian corridors, a few specimen oaks and Eucalyptus groves comprise the existing vegetation. The most historically significant and character-giving existing structure is the Mirassou Winery at Aborn Road and Ruby Avenue.

Evergreen enjoys the mild, semi-arid weather typical of South San Francisco Bay. With a climatic heritage akin to the inland Mediterranean region of France and Italy, mild winters, warm dry summer days, and cool summer evenings afford a casual recreation-oriented lifestyle. Here, deciduous trees provide welcome summer shade and winter sun; semitropical freeze-tolerant plantings can provide counterpoint and accent; the thermal mass, dry heat hardening, and fireproof quality of stucco outperforms wood on building exteriors. Here too, alfresco dining, outdoor community activities, casual recreation, and children's sports schedules extend into the summer evening.

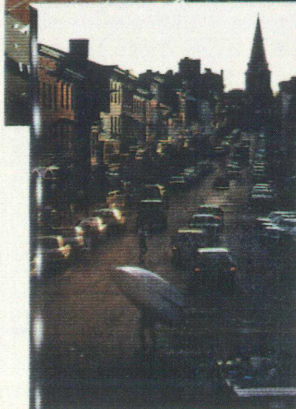
San Jose's residential heritage, like many west coast cities, is most charmingly embodied in its several 1920's and 1930's vintage neighborhoods. Flanking the Alameda or around Williams Street Park can be found romantic expressions of the European free-style architecture of the best prewar neighborhoods. Mediterranean, French, and English country vernaculars are freely interpreted and adapted to the area's climate and lifestyle.



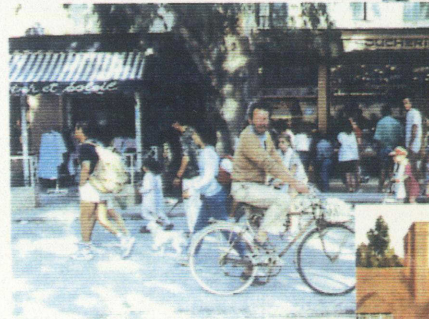
The Mirassou Winery recalls the colors, textures, materials, and ambiance of a French Village.



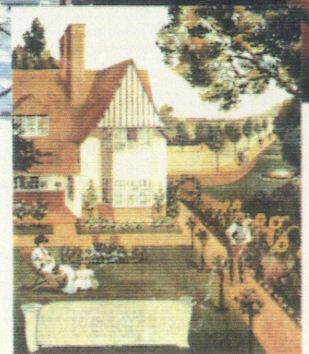
Topography, varying building mass and prominence of important buildings give definition to a community.



More than just circulation, streets can provide organization, form, vista and focus.



The opportunity to observe and participate in community life is as valuable as the ability to retreat from it.





Commonality of color, materials, and landscaping, and the grace of age blend this architectural potpourri into harmonious, coherent, and memorable "place".

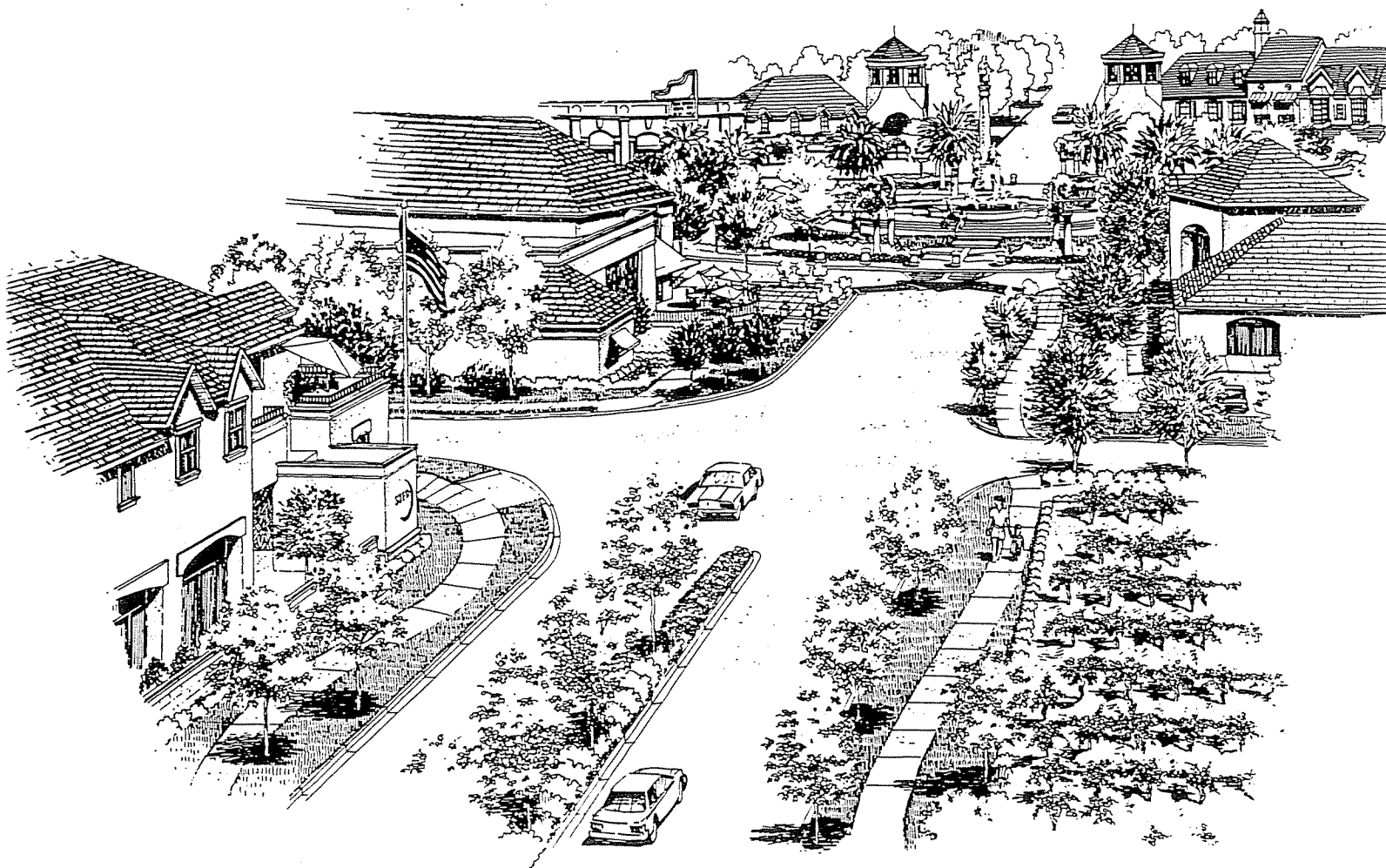
From these cues we can capitalize on, and enhance, the plan relationships to create a traditional village center neighborhood district with an inland Mediterranean ambiance and a freely interpreted European, and 1920's and 1930's American architectural character (Exhibit 7).

Entering the Specific Plan Area travelling east on Aborn Road, the first impression is of the historic Mirassou Winery buildings and the surrounding preserved heritage vineyards. A right hand sweep through these vineyards leads one directly past the fire station and into the village circle (Exhibit 8). Here, encircling 2-story facades of stone, masonry, and stucco, with steeply pitched roof elements, awnings, balconies, and arcades evokes a traditional character. Keying off the Mirassou Winery, color, textures, and materials recall the ambiance of a French village. The rotary provides limited on-street parking and encircles a village plaza large enough for community events such as art shows, Christmas tree lightings, etc. A radial street going northeast from the Village Center focuses directly on the bell tower or spire of the north side elementary school, while the radial street going due east focuses on another rotary at Fowler Creek Park, noted for its landmark hilltop community center. Exploiting topography and the larger building mass and prominence of these important buildings give definition to the community.

In organizing residential neighborhoods the Evergreen Specific Plan values the ability to observe and participate in community life as much as the ability to retreat from it. Higher densities overlooking and within walking distance of the town center offer an alternative to the more traditional and private single family neighborhoods.

To achieve this strategy, general and specific guidelines are proposed both in this and in subsequent sections of the Evergreen Specific Plan. The design character of common walls, hardscape, monumentation, and focal art are specifically defined in this specific plan and can be greatly influenced by the Evergreen Architectural Review Board (see Chapter Ten, "Implementation" re: Evergreen Architectural Review Board). Given their prominence, the architecture of public and commercial buildings is critical to the area's image and identity. These buildings are discussed specifically in subsequent sections of this plan.

Residential architecture, while less significant when looking at any single structure is, in aggregate, the most significant definition of community identity. While individual unit design cannot be defined in this document, general guidelines are described in this section and product type specific-guidelines and controls are included in subsequent sections. Throughout these residential neighborhoods the same traditional interpretive European architectural vernacular will prevail.



## EVERGREEN VISTAS

CITY OF SAN JOSE

EVERGREEN SPECIFIC PLAN

EXHIBIT

8

## DESIGN CRITERIA

The Mediterranean ambiance combined with a freely interpreted European and 1920's and 1930's American architectural character will be achieved through these design and detail criteria.

### Variety

*Encouraged* - Building design relations, groupings, and streetscapes that demonstrate variety and appear to have grown over time.

*Discouraged* - Unvarying repetitive facades that present a monolithic "development".

### Project Definition

*Encouraged* - Develop individual project and market differentiation through innovative site relationships (zero lot line, "Z" lots, wide shallows, etc.), market-focused floor plans, signature detailing, and special neighborhood monumentation while harmonizing and supporting the Evergreen architectural character in form, color, and materials maintaining an open relationship to adjacent neighborhoods.

*Discouraged* - Developing "individuality" through "walling in" and adopting abruptly disharmonious and monolithic architectural style, color, and material.

### Public Face Of Buildings

A building's public face is important to the whole community and will be given special attention in design review. While this is typically the street-facing front elevations, equal concern will be given to side elevations on corner lots and rear elevations where visible to the public (i.e. hillside lots, lots backing onto open space, parking areas, etc.).

### Minimize Streetscape Impact Of Garage Doors

*Encouraged* - Side entry garages, street-facing garages set back from living areas, tandem garage stalls for housing more than two cars, alleys or shared drives, split garages, plant pockets, paving pattern changes and narrow curb cuts to soften the impact of driveways, single car garage doors, plant pockets between doors, trellis screening over doors, varying garage configurations among different floor plans in a neighborhood "project".



*Discouraged* - Unadorned street-facing garages forward of living areas, continuous 3-car, street-facing garages, large proportions of identical garage configurations on all plans in a neighborhood "project".

### **Articulated Building Form And Form/Style Harmony**

*Encouraged* - Articulated individual building form or variety of forms between buildings in a neighborhood project (Exhibits 9 and 10). Strong vertical accents; varied wall plate lines; building forms appropriate to suggested "style" (i.e. lower pitched roofs with barrel tiles on "Mediterranean" elevations, steeper pitches on "English" elevations).

*Discouraged* - Repetitive, unarticulated building forms. Style "applique" on inappropriate building forms (i.e. English half-timbering on 4:12 pitch roof ranch house), articulated roof forms set on constant wall plate height.

### **Architectural Expression Of Indoor/Outdoor Living**

*Encouraged* - Publicly visible balconies, verandas, porches, arcades, and loggias; gazebos, trellises, and seating areas in hardscape; gracious entry doors and porches; entry courtyards; living area windows facing the street; French doors to outdoor courtyards.

### **Creative Siting To Maximize Public And Private Outdoor Areas**

*Encouraged* - Zero lot line, "Z" lots, zipper lots, and other ways of using neighboring windowless walls to define private yard areas; split garages to create entry courtyards; sidewalk cafes, and balcony dining; open-air markets, arcades, street vendors, and vendor carts in the Village Center.

*Discouraged* - Window placement and siting that violates privacy of a neighbor's private outdoor area; narrow and unusable outdoor space; "no man's land" spaces such as slopes, corner lot sideyards, transitional spaces between uses, etc.; blank walls at pedestrian level in the Village Center.

## **DETAILS**

### **Roof, Eaves, and Gable Ends**

Roof pitches and materials should be appropriate to the traditional style the building is expressing; roof to wall details are a point of "finesse" that can legitimize a stylistic interpretation. Resolution of corners, roof-to-wall and rake-to-eave details have a dramatic impact on quality perception (Exhibits 9 and 10).

Chimney  
accent

Tight rake  
edge

Gutter/rainwater  
leader head

Projected  
dormer

Chimneys can "grow"  
out of building form

Vertical treillage  
enhances a simple  
form

Picket fences and low walls define  
semi-private outdoor spaces on a  
home's public side

Bay Windows

Arcades

Awning  
shutters

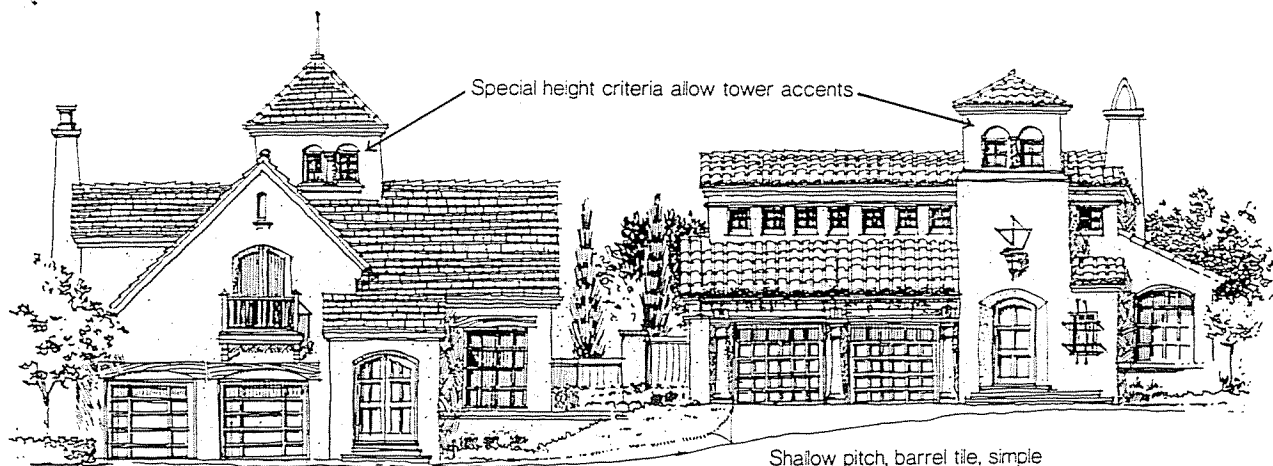
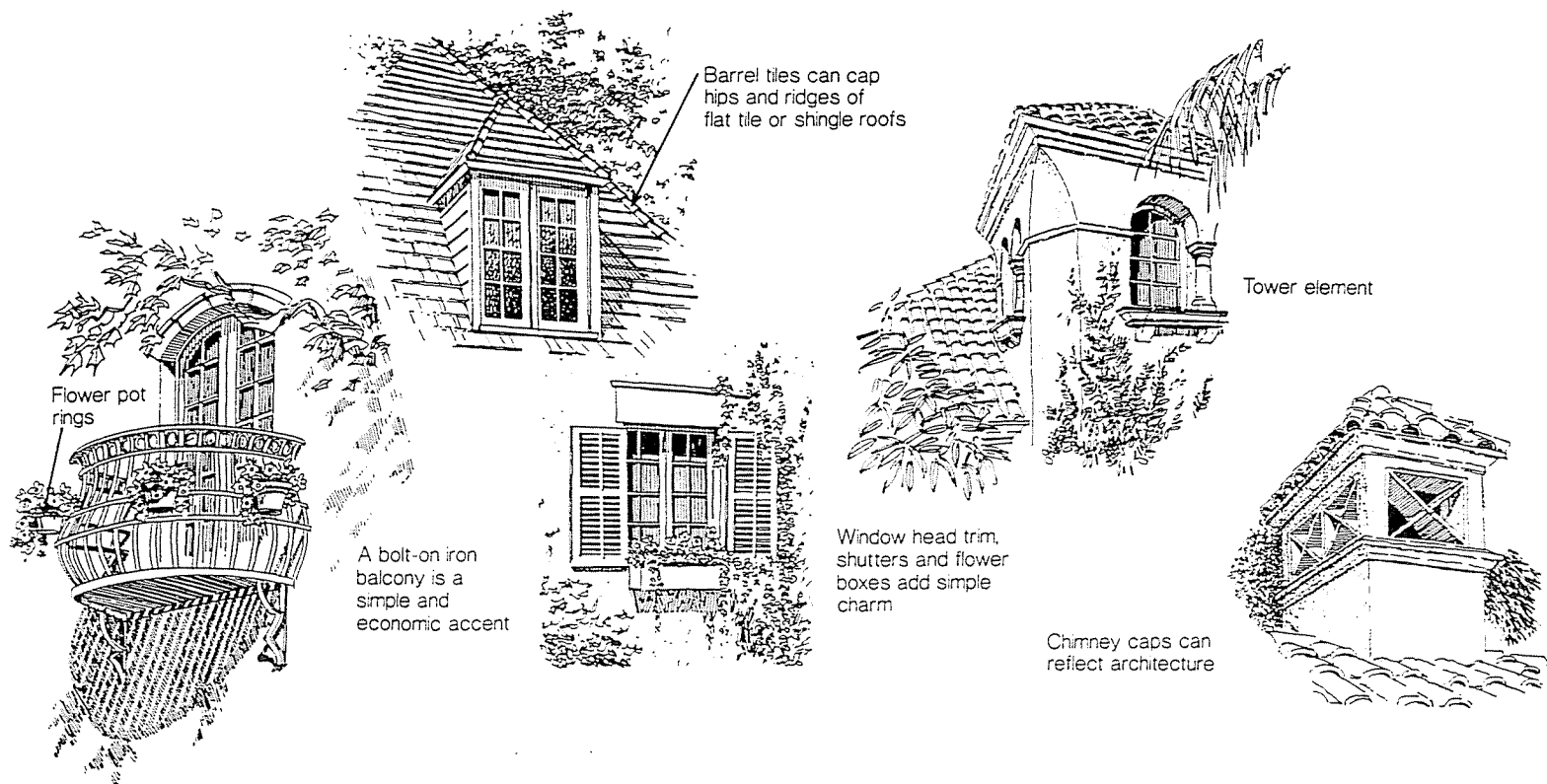
Iron  
balcony

Classic  
entries

Balustraded  
porches

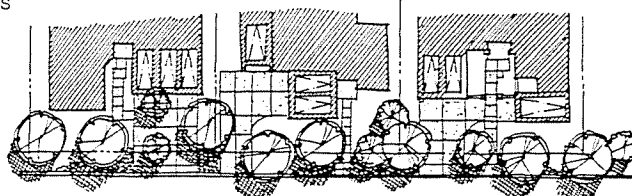
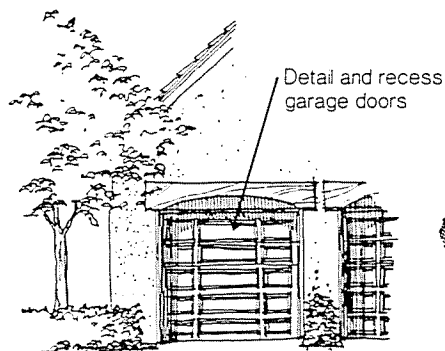
## ARCHITECTURAL DETAILS

CITY OF SAN JOSE  
EVERGREEN SPECIFIC PLAN

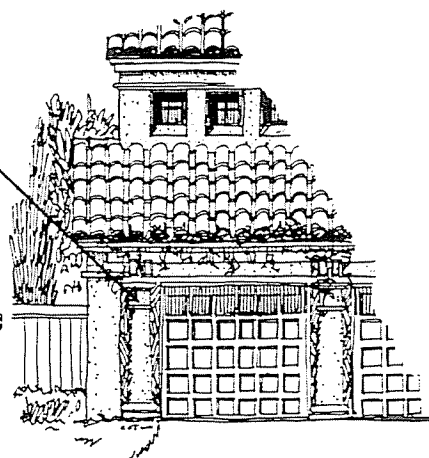


Steeply pitched, flat shingle or tile cascading roofs, define French and English Country vernacular

Shallow pitch, barrel tile, simple stand alone forms, stucco walls define a Mediterranean style



Vary plan configuration of garage doors



## ARCHITECTURAL DETAILS

CITY OF SAN JOSE  
EVERGREEN SPECIFIC PLAN

EXHIBIT

10

Mediterranean buildings generally have lower pitched (4:12 typical, 6:12 maximum) barrel tile roofs in simple classic hip and gable forms; eaves are tight; dormers and clipped gables are inappropriate.

English and French country buildings have steeper pitched roofs (9:12 to 12:12 typical, 6:12 minimum) and more complex forms. Hips, valleys, gables, clipped gables, dormers, and varying plate heights are all appropriate. Eaves often have a shallow pitched "kick". Rakes are very fine and tight.

Gutters, downspouts, and rainwater leader heads are integral to the roof/wall detailing and should be designed as part of the trim.

*Encouraged roof materials and their related style and pitch include -*

1. Barrel or "S" terra cotta, or integral color concrete tile in terra cotta tones is appropriate for Mediterranean-style buildings with pitches below 6:12.
2. Flat terra cotta, flat integral colored concrete, and wood shake are appropriate in roofs of 6:12 pitch and greater. Barrel tiles as caps on ridges and hips of flat tile roofs is a traditional accent. Colors should be terra cotta to brown to warm grey or natural wood.
3. Heavyweight composition shingles, slate, and wood shingles may be used with pitches over 9:12.
4. Flat roofs are allowed where appropriate to an acceptable architectural form, however, all visible areas including areas viewed from hills above are considered critical to the overall design. Therefore, rooftop material and color as well as flashing details, vents, and mechanical equipment will be approved only if they are incorporated into aesthetically acceptable roof designs.

*Discouraged roofing material includes -* composition shingles (below 9:12 pitches) or sheet roofing, synthetic wood shakes or shingles, high-glazed clay tile or glossy painted concrete tile, fiberglass, asbestos, or hard board shingles, unless specific product is approved.

Vertical accents such as cupolas, tower rooms, detailed chimneys, and "widow's walks" are encouraged and accommodated in the height limit criteria.

## Walls

*Encouraged materials* - Cement plaster, trowel-applied synthetic plaster (i.e. Dryvit), stone or brick masonry, cast concrete, shaped plaster, and tile are encouraged. Dimensional lumber or composite siding is allowed but requires adequate painting, staining, preserving, and maintenance to insure against uneven weathering, "sprinkler scallops", black mold or severe checking and splitting. Sheet siding such as plywood or masonite may be allowed (sparingly), but must be used in conjunction with sufficient masonry and trim accents to maintain an overall high quality appearance.

*Discouraged materials* - False stone or brick, false fiber board stucco, metal siding, vinyl siding, unfinished wood, exposed plain concrete block, sheet siding such as plywood or masonite.

Walls should be soft (radiused or chamfered stucco corners, "soft" brick or stone detailing). Stucco or cast concrete "plinths" and feathered bases at stone or brick are very appropriate.

## Openings

Windows should be adequately trimmed or set into walls.

*Encouraged window and skylight materials* - Clear or tinted glass, dark anodized or painted aluminum frames, wood frames, clear or tinted skylights integrated into architecture and with frames that blend with the roof, multi-paned windows.

*Discouraged window and skylight materials* - Reflective glazing, clear anodized or mill-finished aluminum frames, white or frosted plastic skylights.

Entries should be given special attention as a whole system including door, side windows, vestibule, porch, and entry walls. They should be inviting from the street and while weather protected, not overly buried. Bringing the richer materials of the building into play at the entry is a plus to both the street and the building.

Garage doors should be recessed into the building plane at least ten inches (10") when they are visible from the street. (Front facing garage door setbacks are to face of door not wall.) Roll-up garage doors are encouraged. Appropriate panel detailing and window panels are encouraged.

Framed openings such as porticos, arcades, colonnades, covered porches, and balconies are encouraged.

## **Decorative Details**

Applied decorative details are very appropriate to this architectural character and if done with finesse will distinguish this community from more typical suburban tracts. These details can be purely decorative applique such as plaster design applied to a stucco gable end, masonry pattern trim or coinage. They can "finish" an architectural element such as a cornice below the eave gutter, elaborate window heads and sills, shelves, and chamfers in a chimney composition or a tile or scraffito door surround. They can give a decorative flair to a functional element such as an awning shutter or fabric awning to shade a window; a bolt on wrought iron balcony; a cast balustrade guard rail; wrought iron flower pot rings, wood flower boxes; window shutters; gates; roof vents, etc. It takes only a few of these decorative elements to add a unique charm to a small home and they can often replace more costly and complex decorative framing.

## **Hardscape**

In this context, hardscape refers to all private development paving; outdoor "furniture", and structural landscape accents such as retaining walls, pot shelves, railings, etc.; garden structures such as bandstands, gazebos and trellises; utilitarian elements such as street lamps, mail boxes, signage, etc.; walls and gates; monumentation and focal art.

Public and common space walls, monumentation, focal art landscape structures as well as Village Center hardscape are described in other sections.

Publicly visible private hardscape shall compliment the architectural expression. Structural elements may be an extension of building architecture or a detailed, finely-scaled counterpoint. Paving should emphasize variety and generally be minimized in favor of landscaping. Juxtaposing dramatically different paving pattern and degrees of formality add a quaint "evolved" quality to pedestrian areas (Carmel is a classic example). Formal patterns can serve as base to a building yet step onto a meandering stone path into a garden area.

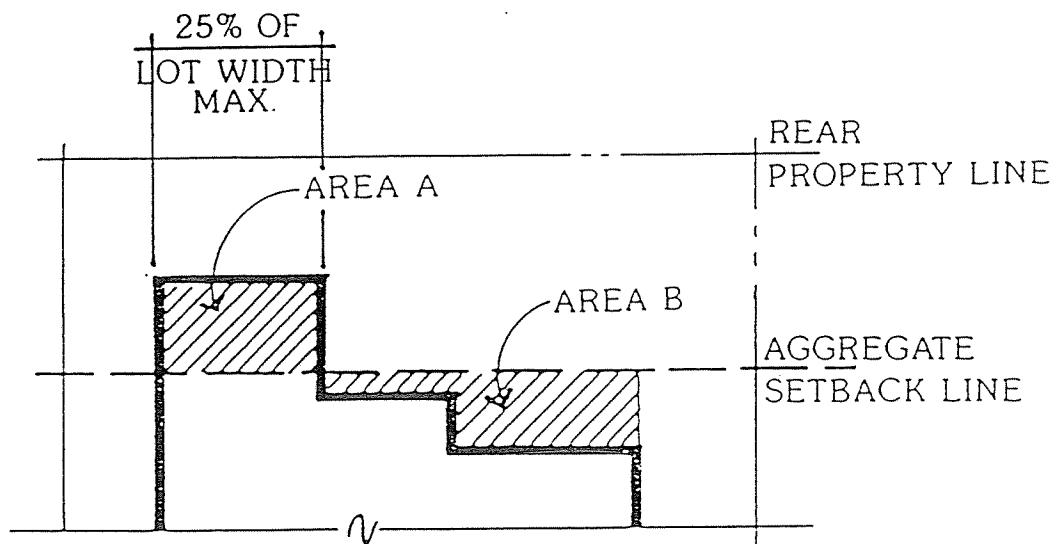
Outdoor furniture (such as seating, trash receptacles, tables, bollards, etc.) should be incorporated into other hardscape monumentation (i.e. fountain bases as wide benches), and of a complimentary style to the surrounding architecture and monumentation; or of a lightly scaled "patio furniture" type. Characterless, contemporary "municipal" standard designs are discouraged.

Lamp posts, signal light standards, and traffic signage should be organized to minimize visual chaos, especially at the Village Center and Fowler Creek Park rotaries.

## DEFINITIONS

### Aggregate Rear Yard Setback

The area beyond the setback line offset by an equal open area within the building envelope as illustrated below:



AREA A MUST BE NO GREATER THAN AREA B

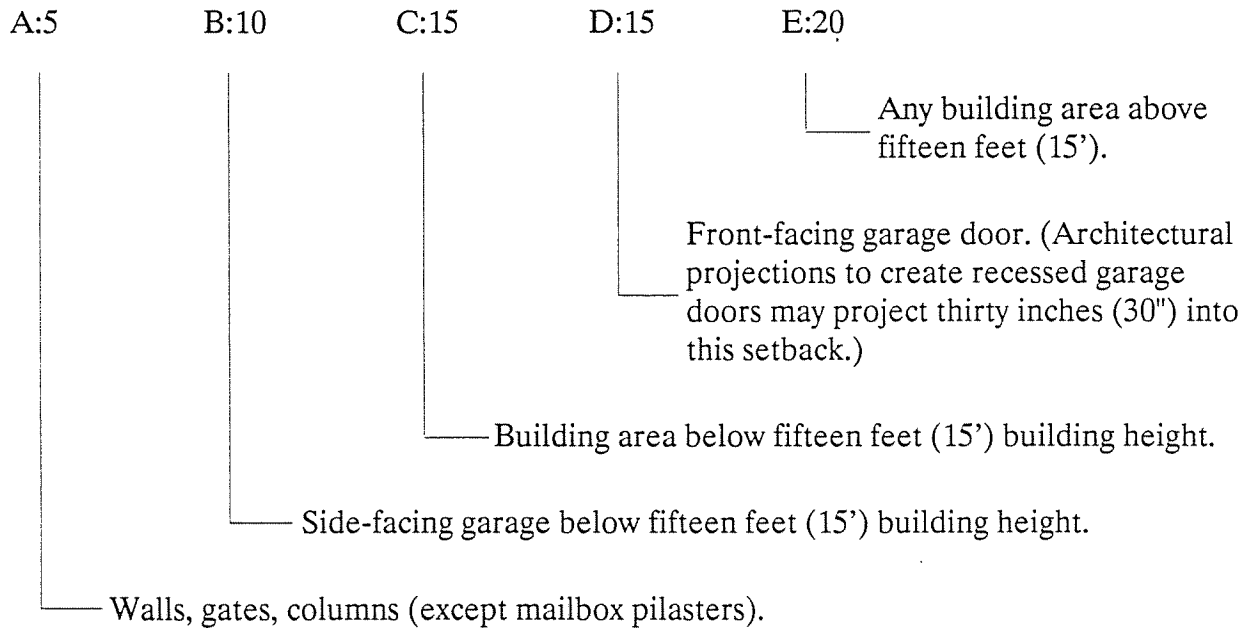
Projections across the aggregate setback line may not encroach onto the required side yards as defined in the setback guidelines section.

### Aggregate of Both Sideyards

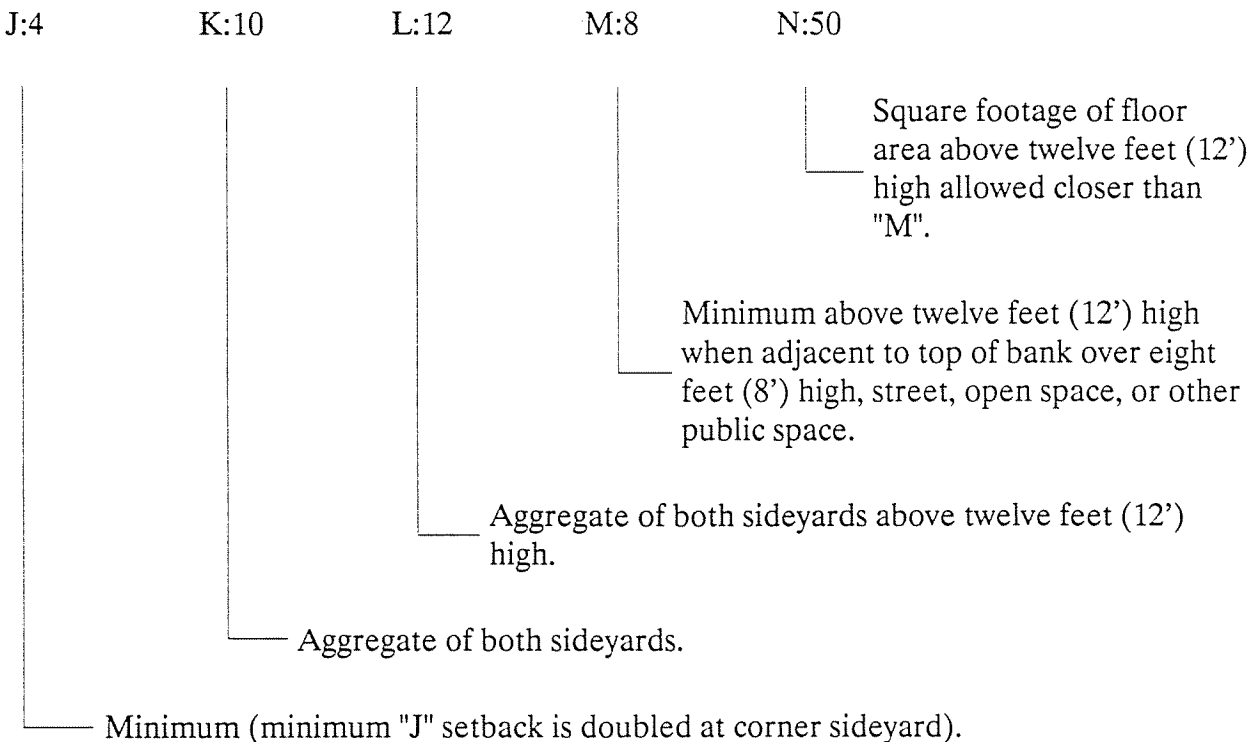
The total minimum setbacks of both sideyards combined.

## Setback Definitions

**Front Yard Setbacks** (Measured in feet from the property line along the street in a line perpendicular to the street, i.e. A:5 means "A" elements must be five feet (5') from the property line. **These numbers are used as examples only. Refer to the development criteria for actual setbacks.**):

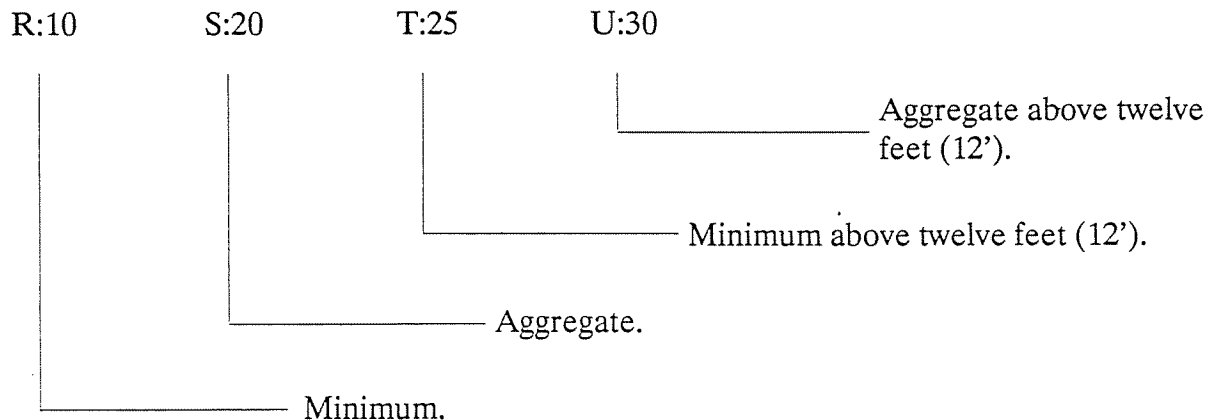


**Side Yard Setbacks** (measured from the side yard property line):





**Rear Yard Setbacks** (measured from the rear property line):



**Building Height:**

Building height is as defined below from Chapter 20.04.306 of the *San Jose Municipal Code* or as amended.

The "height" of a structure at any point is the vertical distance between its top surface or its building height plane and the "grade" immediately below it. For purposes of the Section, "grade" shall be determined as follows:

- A. For any point on the top surface of a structure, "grade" is the intersection of the vertical line, through said point, with a straight line which passes through:
  1. the vertical point; and
  2. the nearest point of the property line nearest to the vertical line; and
  3. a point on a second property line opposite to the first property line.

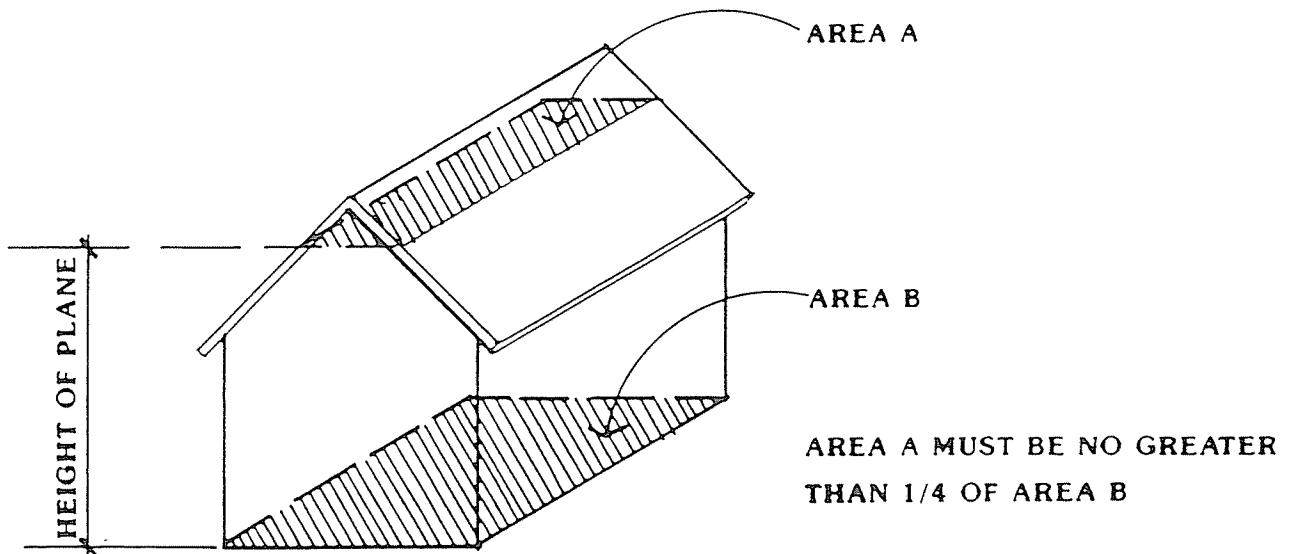
Said property lines shall be property lines of the same lot on which the structure being measured is constructed or installed and said property lines shall be assumed to lie on the surface of the finished ground elevation.

- B. When the point described in Subsection A.2 or in Subsection A.3 of this Section is more than seventy-five feet (75') from the nearest exterior surface of the structure, such point shall be moved along the same bearing, as was established pursuant to Subsection "A" of this Section, to a point at finished ground elevation and seventy-five feet (75') from said exterior surface.
- C. When there is an on- or off-site retaining wall located within six inches (6") of the property line at the point established by Subsection A.2 or by Subsection A.3, said point shall be moved along the same bearing, as was established pursuant to Subsection "A", to a point at finished ground elevation and twelve inches (12") off-site.

**Building Height Plane:** In an effort to avoid a "chopped off" appearance created by height restrictions, "building height plane" has been established to provide variety to buildings, is defined as follows:

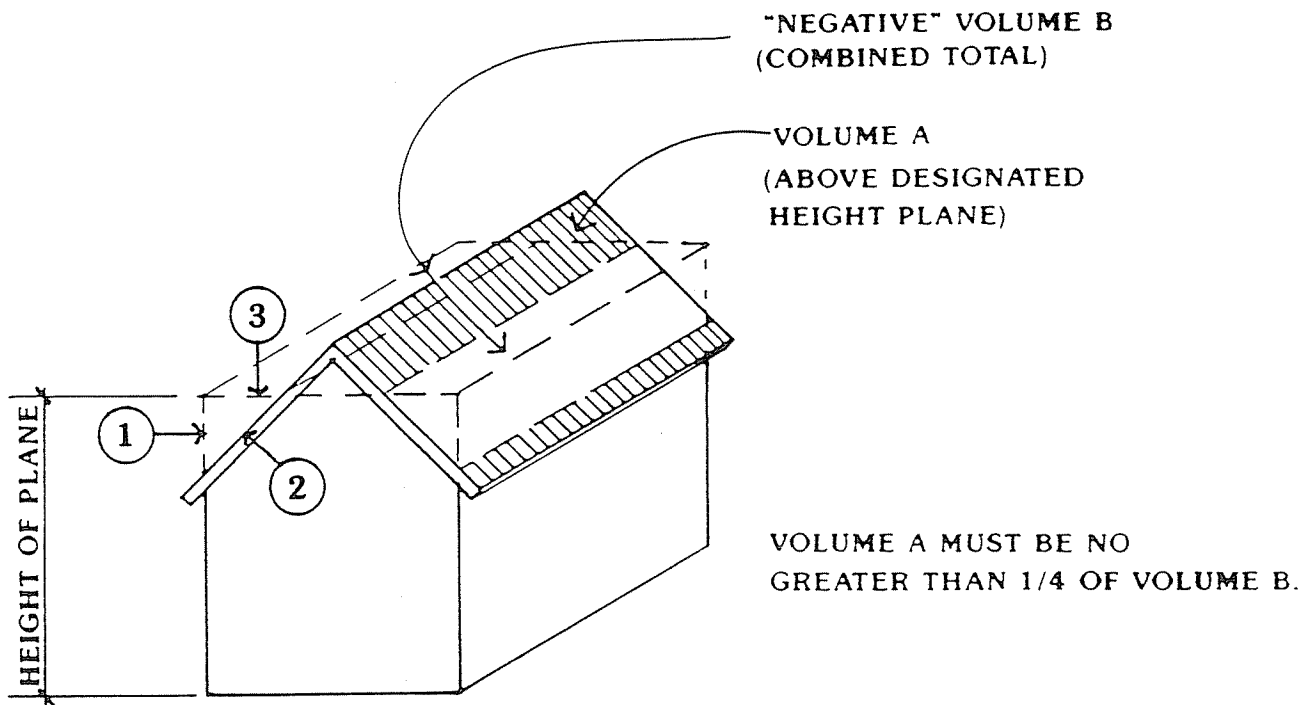
Designated building height shall define a plane, approximately parallel to the ground plane as it occurs around the perimeter of the structure. Building roofs may protrude above this height plane subject to the following limitations:

1. No part of the roof may exceed the designated building height plane by more than eight feet (8').
2. The area of the protrusion(s) where it penetrates above the designated building height plane cannot exceed  $1/4$  of the building footprint.



3. The volume of the roof protrusion(s) (A) above the building height plane may not exceed 1/4 of the "negative" volume (B) defined by the following:

- 1) The vertical projection of the perimeter walls above the roof.
- 2) The roof slope.
- 3) The building height plane.



4. Vertical accents such as chimneys, spires, tower rooms, widow's walks, etc. are encouraged and not subject to the above described height limitations. Vertical accents may exceed the building height plane by a maximum of twelve feet (12') or up to the General Plan height policy maximum, which ever is less. Vertical accents are limited to a horizontal cross sectional area (cut anywhere above the building height plane) of 1/8 of a single family building or of 5% of a multi-family footprint. No single vertical accent may exceed 250 square feet. Vertical accents are allowed as a prominent design feature. Conformance to the height and area criteria alone does not guarantee their approval. PD permit approval of such accents will be carefully considered with regard to architectural quality, as well as impact on public and neighboring private spaces.

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## **Major Parks and Recreational Amenities**



## Chapter 7 - Major Parks And Recreational Amenities

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Critical to the establishment of a new community in the Evergreen area is the creation of community facilities such as public buildings and recreational amenities. Community and recreational amenities not only provide activity areas such as playfields or tennis courts, but also traditionally serve as community and civic gathering points. In today's world, the community votes at schools, child care is offered at community centers, and neighborhood interaction occurs in connection with other activities such as little league baseball or soccer sign ups. Community amenities such as Fowler Creek Park will serve the broader Evergreen area beyond that of just the boundaries within the Specific Plan area. In the recent past, the majority of residential developments within the Evergreen area were developed independent of one another, and were of relatively small scale. This prevented the development of a cohesive plan to provide community facilities and did not encourage the placement of community facilities in prominent locations. Through the Evergreen Specific Plan, the Evergreen community will be served by a variety of new community-scale amenities (Exhibit 11).

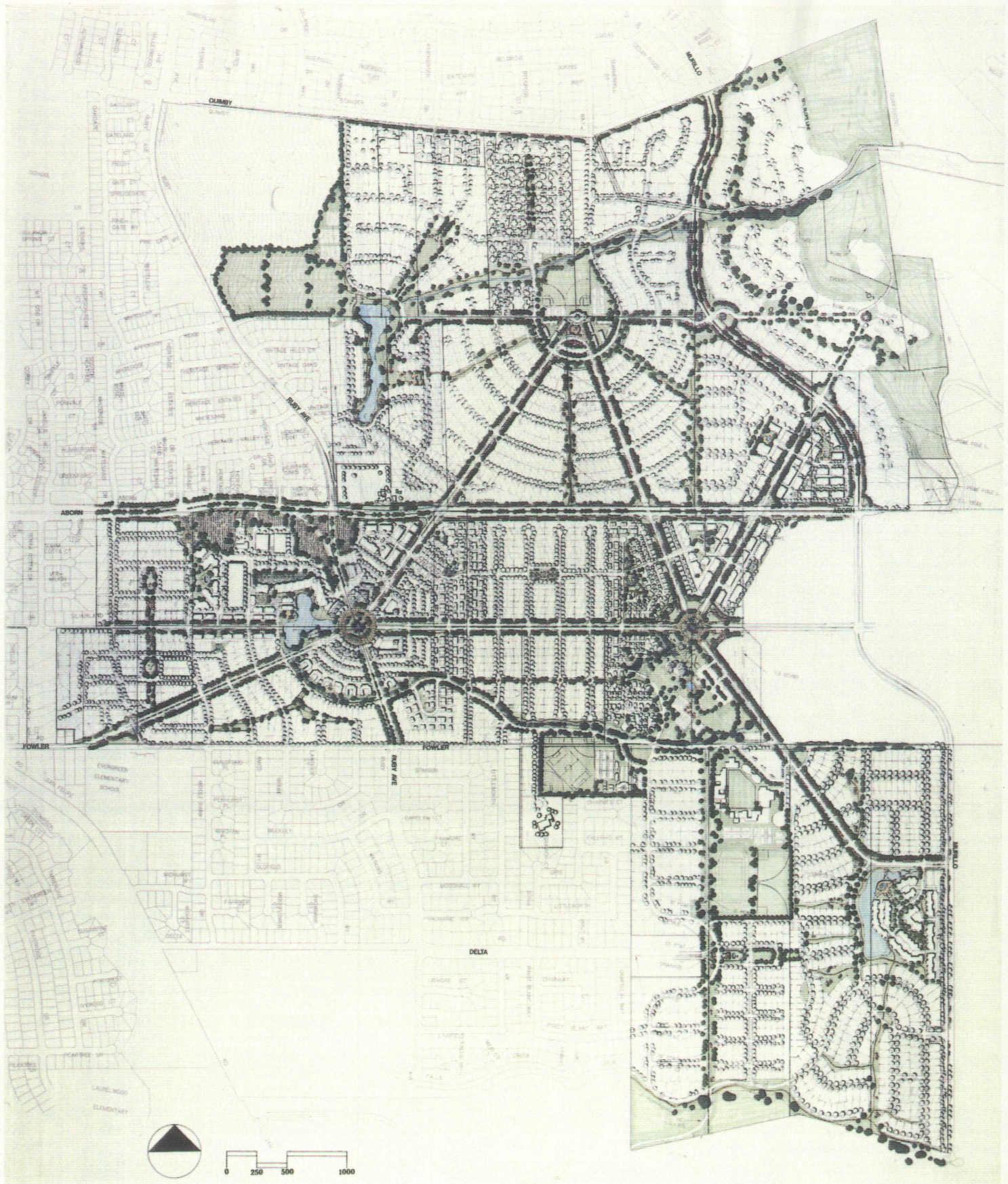
### RECREATIONAL AMENITIES

The Evergreen Specific Plan will create three recreational areas within the plan area (Exhibit 12). These recreational amenities are as follows:

#### **High School Playfields**

The Specific Plan has identified the placement of recreational facilities on the vacant high school site as shown on Exhibit 12. At the time of drafting this document, the East Side Union High School District has stated that recent demographic information indicates that a new high school would be needed in the area and the District has indicated that they will proceed with plans to construct a high school on their property within the Evergreen Specific Plan by 1996. If the School District's capital improvement program includes construction of the school within three years of the formulation of the second, and presumably final, Evergreen Specific Plan Financing district, the Evergreen Specific Plan area will not be responsible for constructing the playfields but will make a contribution beyond the AB 2926 funds to the School District for general construction costs. If the School District plans construction of the school beyond the three year time frame identified above, the Evergreen Specific Plan area will be responsible for the construction of the playfields using the intended contribution beyond the AB 2926 funds. Should the School District decide not to build the new high school on the Quimby Road site, the Evergreen Specific Plan area will have no obligation to construct or contribute to these playfields.





## **COMMUNITY AMENITIES ILLUSTRATIVE SITE PLAN**

**CITY OF SAN JOSE**

**EVERGREEN SPECIFIC PLAN**

EXHIBIT

11







## **Fowler Creek Park**

The City of San Jose currently owns approximately 12 acres of land along Fowler Road for a future community park. Although the City has developed a master plan for the park, the actual construction of the park has not been funded. The Evergreen Specific Plan proposes to reconfigure the park boundaries (Exhibit 13) to conform with the Specific Plan's radial street pattern and to incorporate a prominent knoll into the park boundaries. The reconfigured park could accommodate the park amenities as outlined in the City's original Fowler Creek Master Plan such as the swim center, tennis courts, informal turfed play area, tot lot, picnic area, and the community center described below. The community center has been sited atop the knoll and overlooks the amphitheater and the Fowler Creek Park rotary.

Although the reconfigured Fowler Creek Park conforms with the uses and facilities outlined in the original Fowler Creek Park Master Plan, it should be pointed out that the adoption of the Evergreen Specific Plan does not amend the Fowler Creek Master Plan. A separate review process under the direction of the City of San Jose Recreation, Parks and Community Services Department will be necessary to amend the master plan.

## **Dedication of Land to Montgomery Hill Regional Park**

Approximately 8 acres of land within the Evergreen Specific Plan area is effectively separated from the rest of the plan area by Evergreen Creek. This property will be dedicated to the City of San Jose for inclusion into the adjacent Montgomery Hill Regional Park. No additional improvements to this area are proposed.

## **PUBLIC BUILDINGS**

Traditionally, our public buildings were often prominent landmarks, our most elaborate edifices, and an enduring source of community pride. Their very prominence gave special stature and community presence to those public servants who occupied them. With such prominence, the architecture of such structures can be a major definer of community identity.

The Evergreen Specific Plan provides prominent landmark locations for several public buildings (Exhibit 14). To assure that these buildings "live up" to their location this specific plan proposes architectural review and defines development and architectural criteria for each of these structures.

### **Fowler Creek Park Community Center (Exhibit 15)**

Fowler Creek Park has been reconfigured to incorporate a small hill at its northern apex. The community center is sited atop this hill. Building proportions accent the vertical, and a spire, cupola, or finial rises from the top. Detailed plaster, cast

CARRIAGE HOMES •

AMPHITHEATRE •

COMMUNITY CENTER •

PARKING •

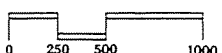
SWIM CENTER •

TENNIS COURTS •

PICNIC AREA •

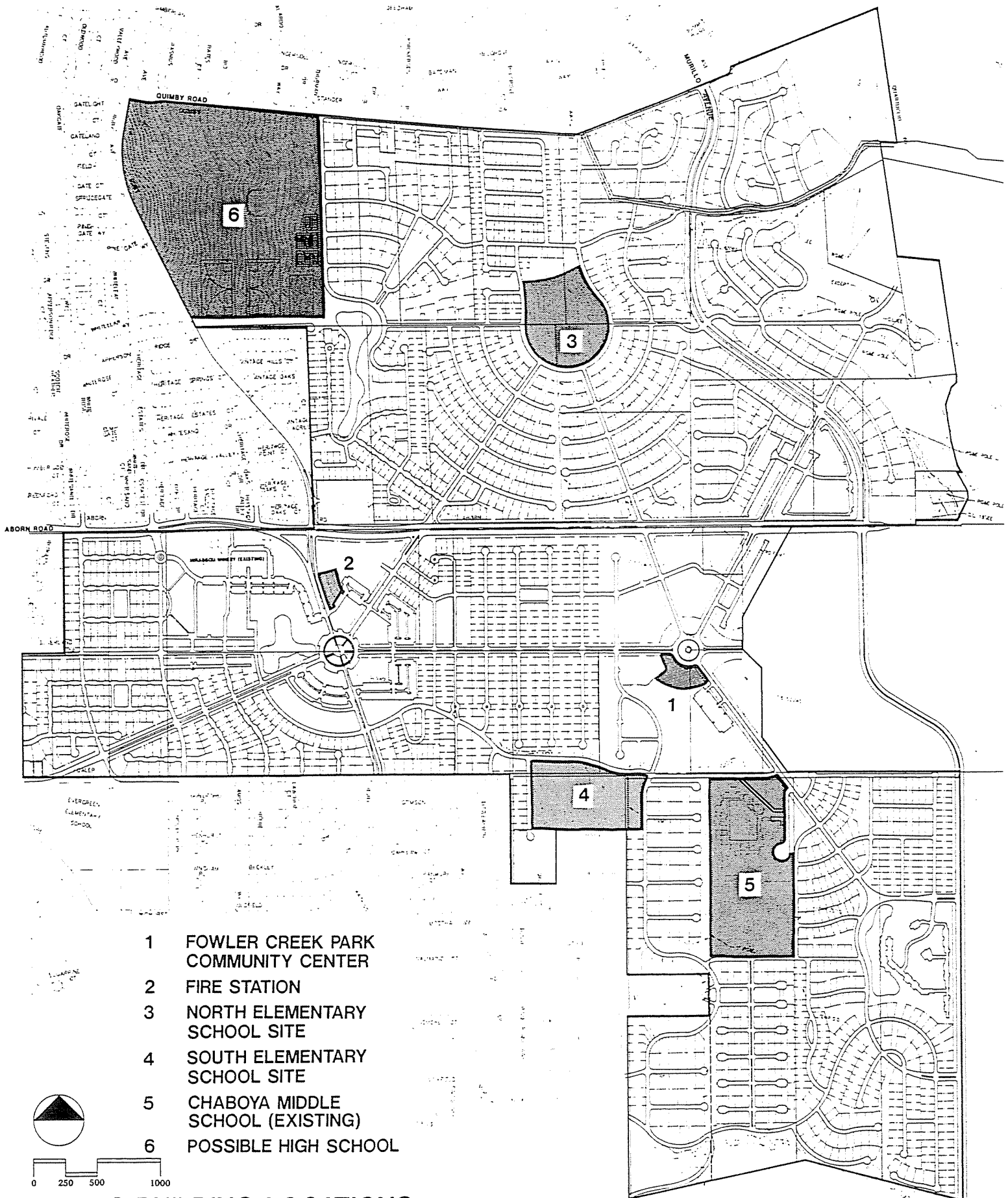
FOWLER CREEK •

INFORMAL TURFED  
PLAY AREA



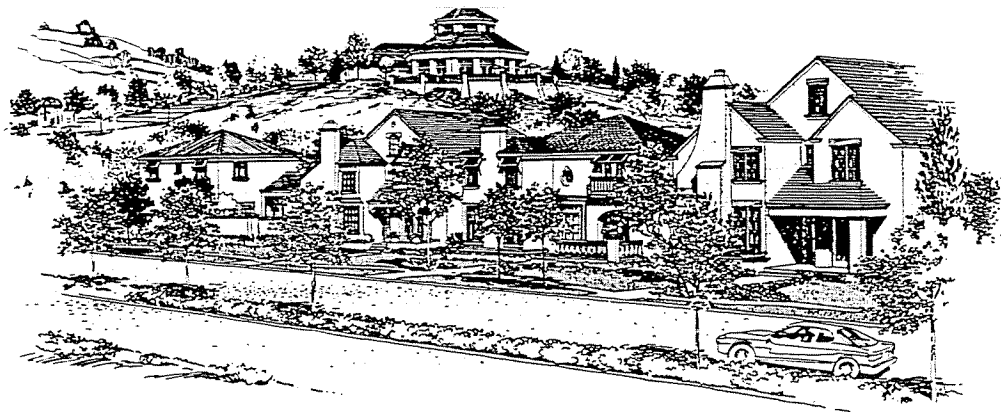
# FOWLER CREEK PARK (CONCEPTUAL)

CITY OF SAN JOSE  
EVERGREEN SPECIFIC PLAN

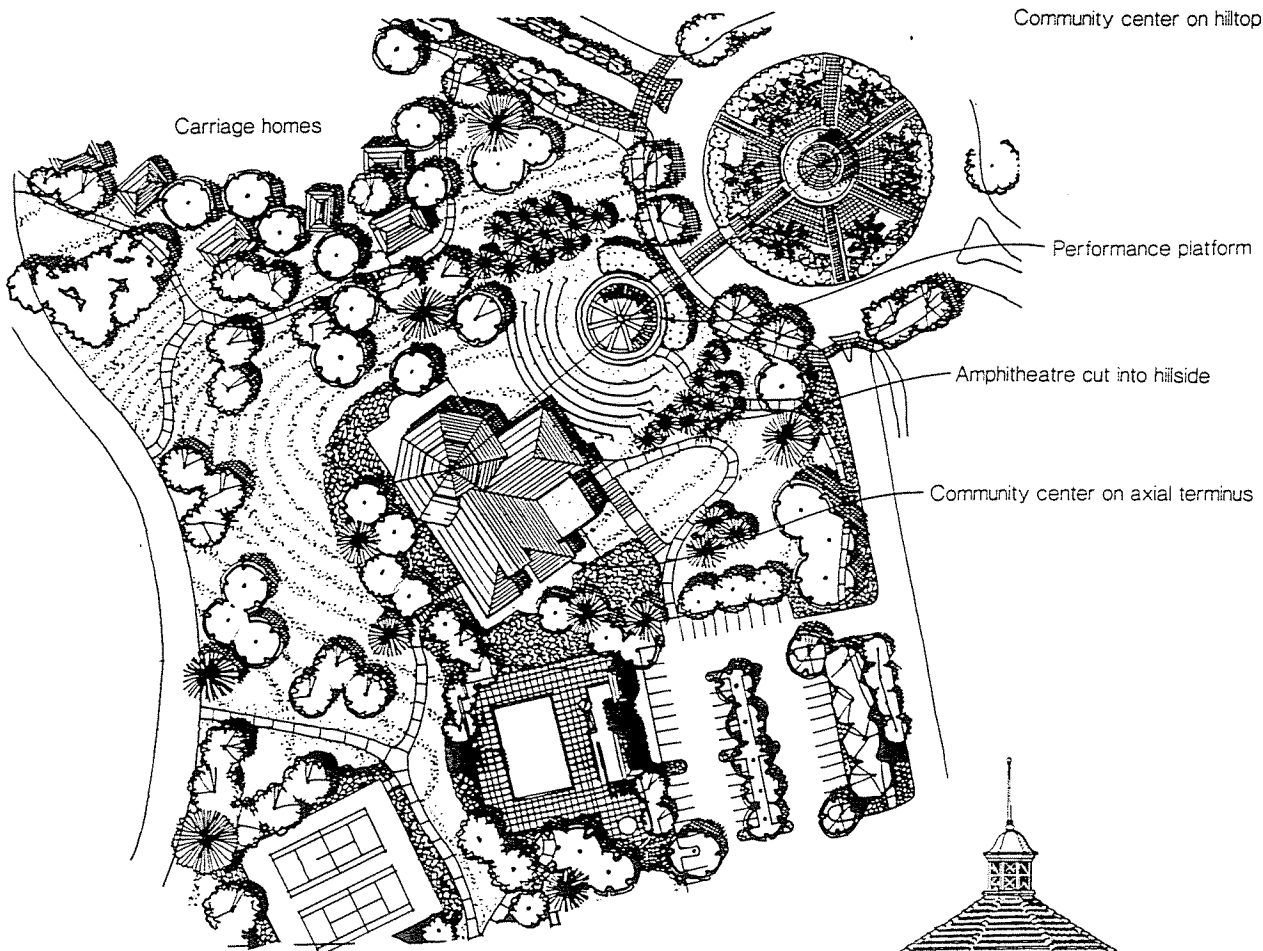


## PUBLIC BUILDING LOCATIONS

CITY OF SAN JOSE  
EVERGREEN SPECIFIC PLAN



Community center on hilltop is a visible landmark

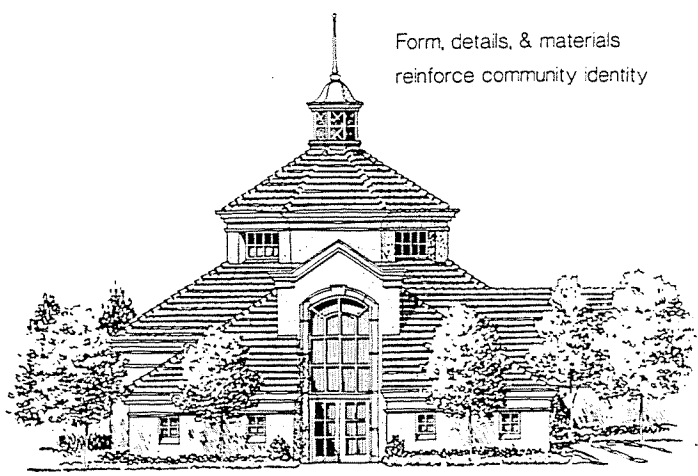


Carriage homes

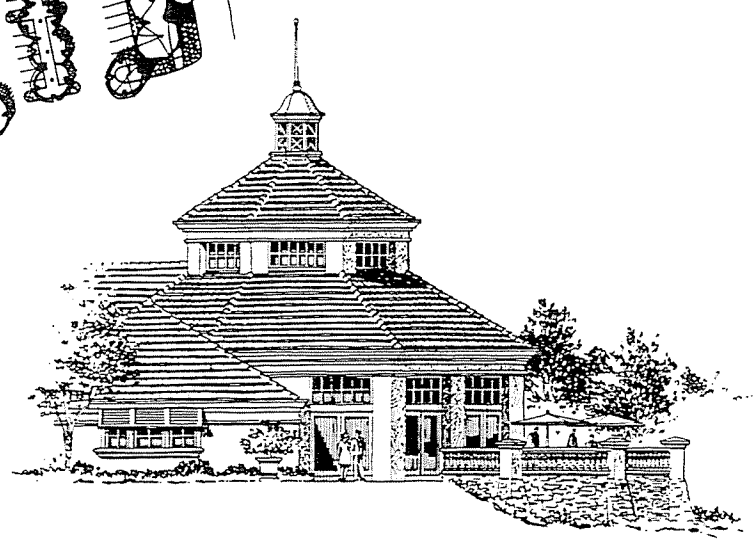
Performance platform

Amphitheatre cut into hillside

Community center on axial terminus



Form, details, & materials  
reinforce community identity



Tail roof over meeting rooms accentuates  
height & "landmark" status

## COMMUNITY CENTER

concrete, and steeply pitched flat tiled roofs will reinforce the Evergreen architectural identity. To the northeast-facing Fowler rotary, an amphitheater and bandstand extend the building form down the hill, accentuating its height.

#### **Fire Station (Exhibit 16)**

The fire station is one of the first buildings experienced upon entering the Evergreen Community. It is adjacent to, and introduces, the Village Center.

The San Jose Fire Department has established a standard building plan for new fire stations. This plan can be accommodated, but the building's appearance in form, elevation, and detail should be designed to contribute to the architectural character of Evergreen. Steeply pitched roofs and dormer windows reflect the residential function of a fire house. Orientation faces the public entrance toward the Village Center. The truck bays are accented by a traditionally detailed hose tower.

The City of San Jose Fire Department will require additional input into the actual configuration of the final site plan to ensure adequate access to Ruby Avenue, as well as resolving on-site issues such as the configuration of hose drying areas, the placement of generators, and other requirements of the station.

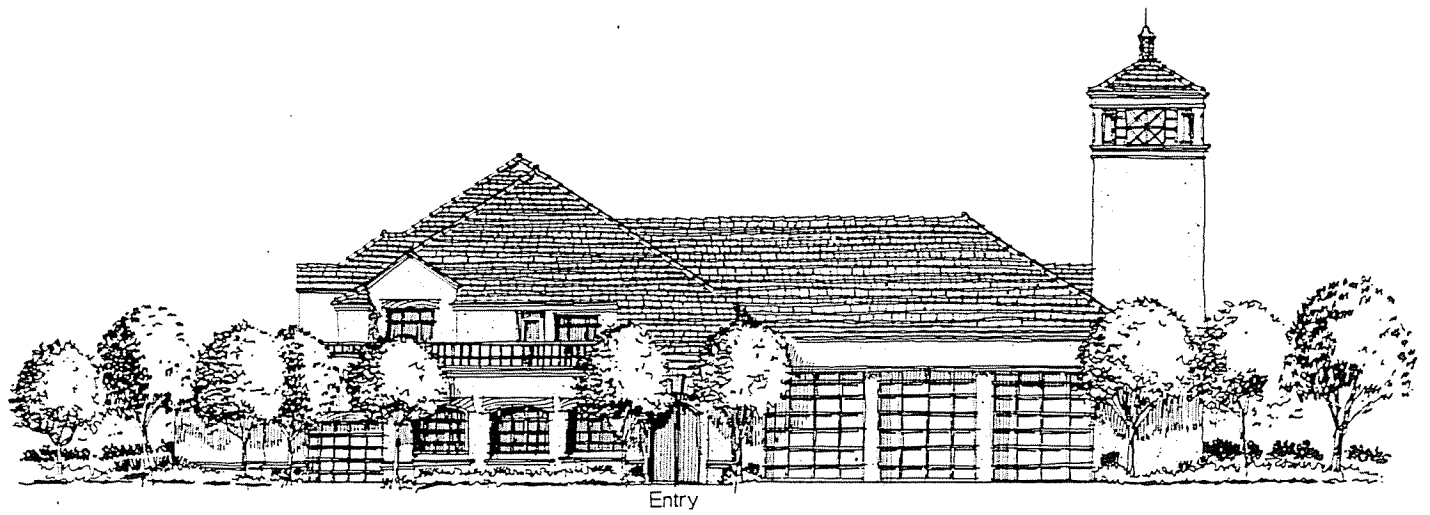
#### **Northern Elementary School (Exhibit 17)**

The central building (administration office, multipurpose room, special education facilities, etc.) is at the axial terminus of radials linking to the Village Center and Fowler Creek Park rotaries. It is a critical design element in linking the Evergreen Community north and south of Aborn Road. These radials are approximately 1/2 mile long and should terminate at the school in a very strong, tall ( $\pm 50'$ ) vertical landmark.

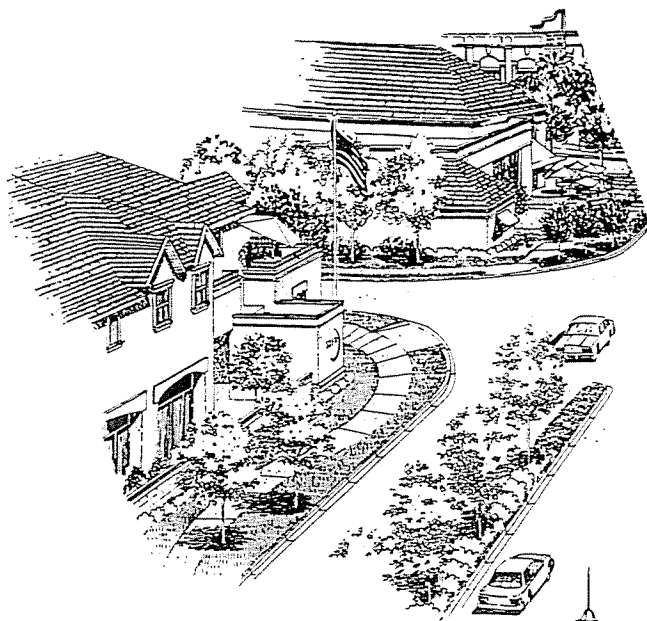
The size of this central building and the need to link to many one-story classroom wings suggests a strongly horizontal form with the vertical landmark more of a tower than the apex of a roof. This tower, a traditional school bell tower, creates a formal grand entry to the multi-use space; giving special importance to school and community functions. A low, horizontal arcade links this building to the much simpler and economic one-story classroom wings where low pitched barrel tile roofs and plaster (synthetic plaster on portables) compliment the central structure.

#### **Southern Elementary School**

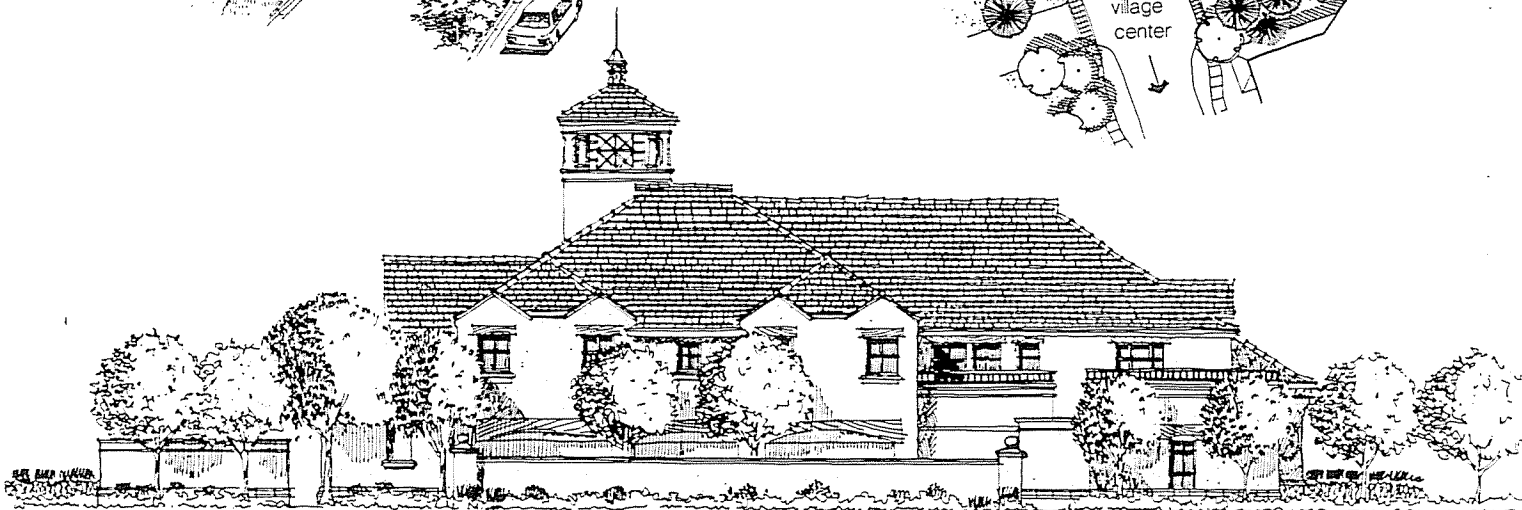
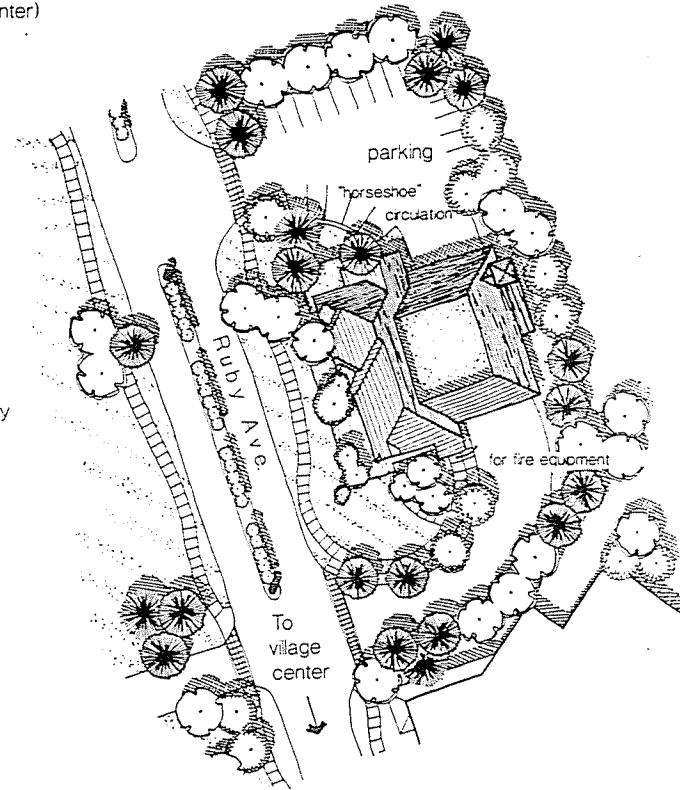
This school's siting does not place its buildings in a critical focal position. However, it is a gateway to the southeastern section of the specific plan. Here again, the architecture of public facilities can also do much to define community character. Siting should address the school's relationship to Fowler Creek and adjacent city park and Chaboya Middle School. A low pitched tight-eaved classic Mediterranean architectural experience with virtually the same detail forms (except tower) and colors as the northern school will serve as a strong identity link between north and south Evergreen.



**PUBLIC ENTRY ELEVATION**  
(Faces south to village center)



Fire station is  
located at gateway  
to village center

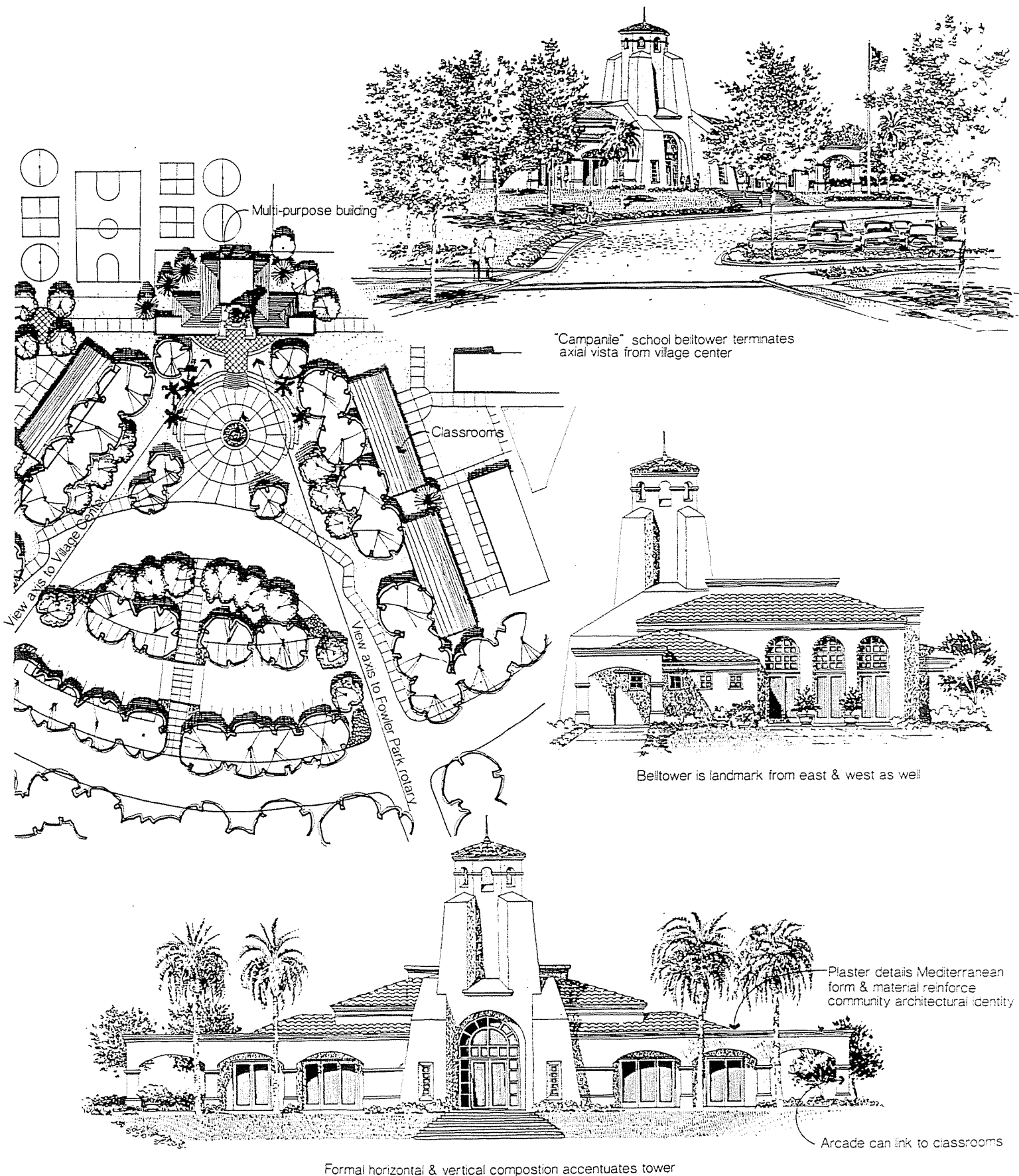


**RUBY AVENUE ELEVATION**

## FIRE STATION

CITY OF SAN JOSE  
EVERGREEN SPECIFIC PLAN





## NORTHERN ELEMENTARY SCHOOL

CITY OF SAN JOSE

EVERGREEN SPECIFIC PLAN

EXHIBIT

17

## **Material and Detail Threads for Public Buildings**

To further enhance the function of these public buildings as identifiers of a community architectural character and as a common linking element throughout the specific plan area, common architectural details, materials, and colors are proposed for all public buildings to be consistent with Exhibits 15, 16, and 17.

## **WALLS, MONUMENTATION, AND FOCAL ART**

Sound buffering and privacy walls, point-of-entry monumentation, and focal monumentation and art are the threads of the community fabric. They are important as foreground accents like a grand fountain or figurative sculpture atop a pedestal. They also create a recurrent background as walls, special repeated pilasters, and finials.

### **Walls**

Throughout the Evergreen Specific Plan area, sound buffering, and streetside privacy walls should be of a common design. An appropriate example (Exhibit 19) shows a precast concrete wall in a traditional large ashlar pattern with a classic cap and plinth. Pilasters (connecting precast panels) can feature a signature cap which reflects the community character. Wall heights can vary as required for sound attenuation (Exhibit 18); low walls can create planters; and bench walls coupled with a wrought iron top can be used along tops of banks at rear-viewing hillside lots.

### **Monumentation**

Community entry points such as Aborn Road at Heritage Estates (Exhibit 26), Old Fowler Road and New Fowler Road (Exhibit 27), and key focal points such as the center of rotaries and park accents (Exhibits 28, 29, 30) are points of special monumentation. Traditional fountains, raised planters, and special wall treatments as detailed in Exhibit 19 should accent these key locations.

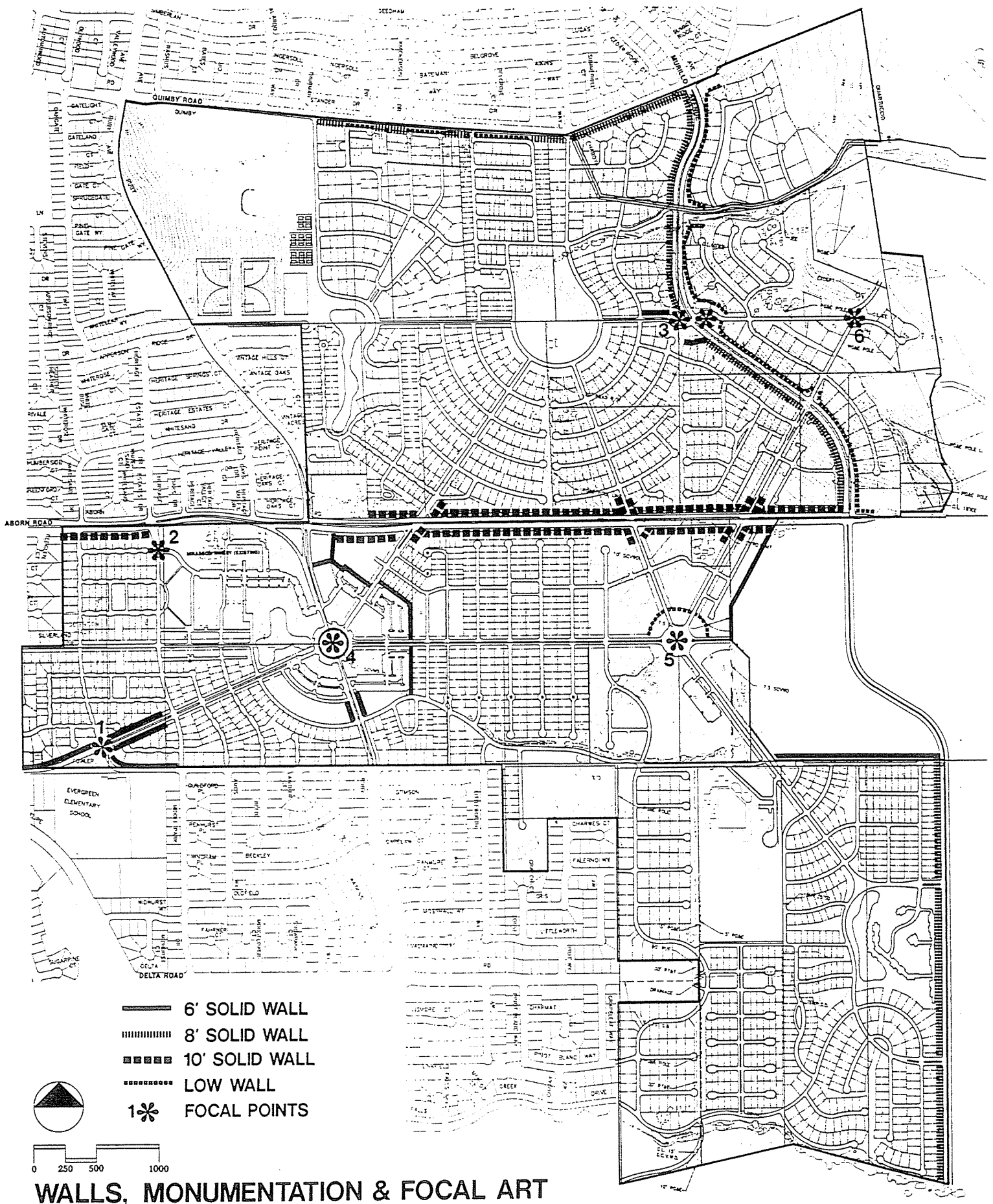
### **Focal Art**

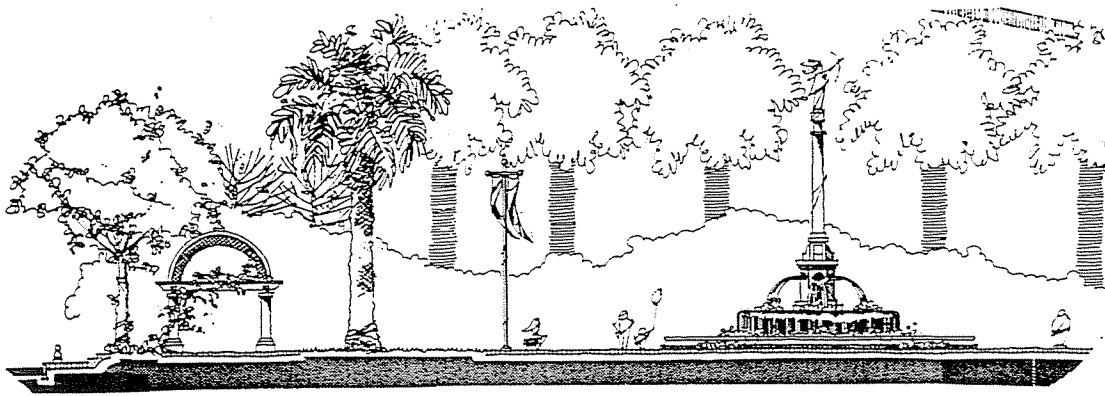
At very important community locations, historically-meaningful figurative sculpture can serve its traditional role of "grounding" the community in its history, as well as being a focal point of community pride. The center of the village rotary provides a key opportunity for such an experience. Other areas include Fowler Park and rotary, and the vista point pocket park (Focal Point #6 on Exhibit 36).

## **STREET LANDSCAPING**

One of the disadvantages of living in California is dealing with an automobile-orientated society. Although necessary for circulation, streets can do more to separate a community than to tie it together. Careful thought in the design and detail of the project's street landscaping, however, can help to minimize the impacts of the automobile on the future residents of the community. The Evergreen Specific Plan has identified a variety of street



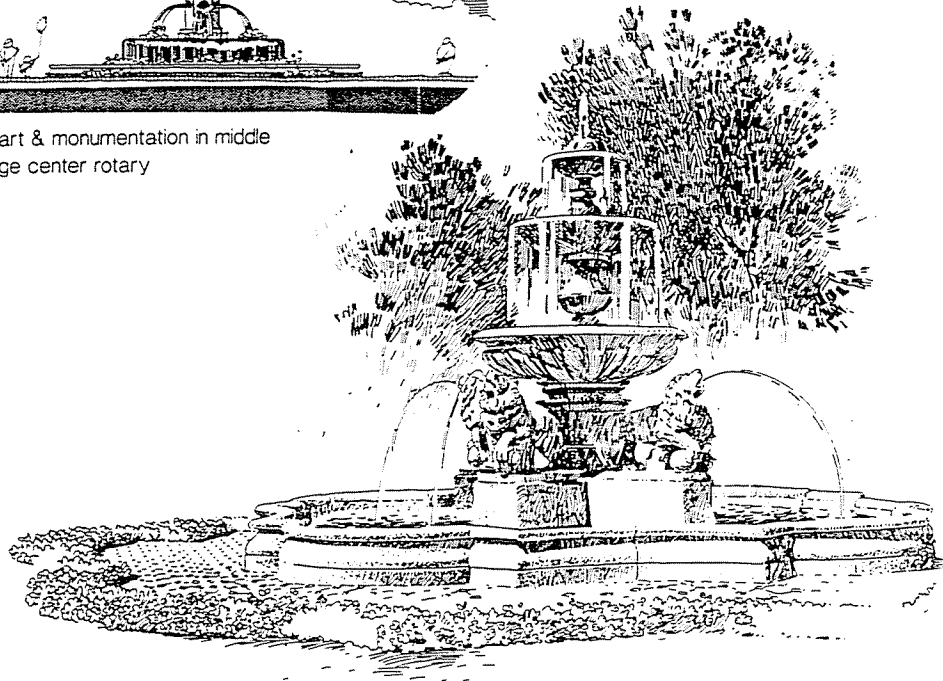




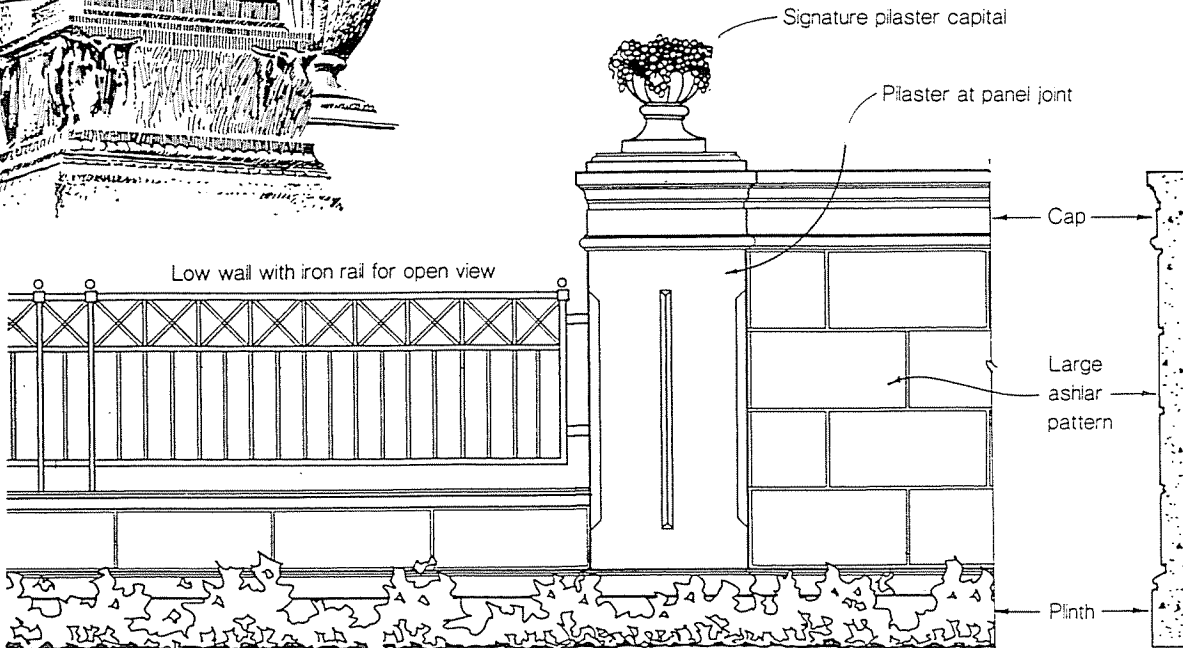
Focal art & monumentation in middle of village center rotary



Historically relevant figurative sculpture



Fountains as focal monumentation



TYPICAL PRECAST SOUND & PRIVACY WALL

## WALLS, MONUMENTATION & FOCAL ART

CITY OF SAN JOSE

EVERGREEN SPECIFIC PLAN

landscaping conditions (Exhibit 20) and have prepared responses to these conditions in order to provide a consistent character in the project's landscaping. All landscaping within the Specific Plan area shall be consistent with the *Landscape and Irrigation Guidelines for the City of San Jose*.

### **Through Community Streets**

#### **Aborn Road:**

One of the primary entrances to the Evergreen Specific Plan area will be Aborn Road. The City's Circulation Element of *Horizon 2000 General Plan* calls for Aborn Road to be constructed as a six lane arterial through the project area to Murillo Avenue. Aborn Road, as a six lane arterial, is expected to carry a great deal of traffic to the campus industrial employment center east of the Specific Plan area. In response to the ultimate paved width of a six lane roadway, a twenty-three and one-half foot parkway on either side of Aborn Road has been created in an effort to buffer the through route to the campus industrial areas. These parkways will be landscaped densely with deciduous and evergreen trees in an effort to "screen" Aborn Road. The landscaped median will also contain the deciduous broad-leaf canopy trees such as ash, pistache and sycamore.

#### **Murillo/Yerba Buena Avenue:**

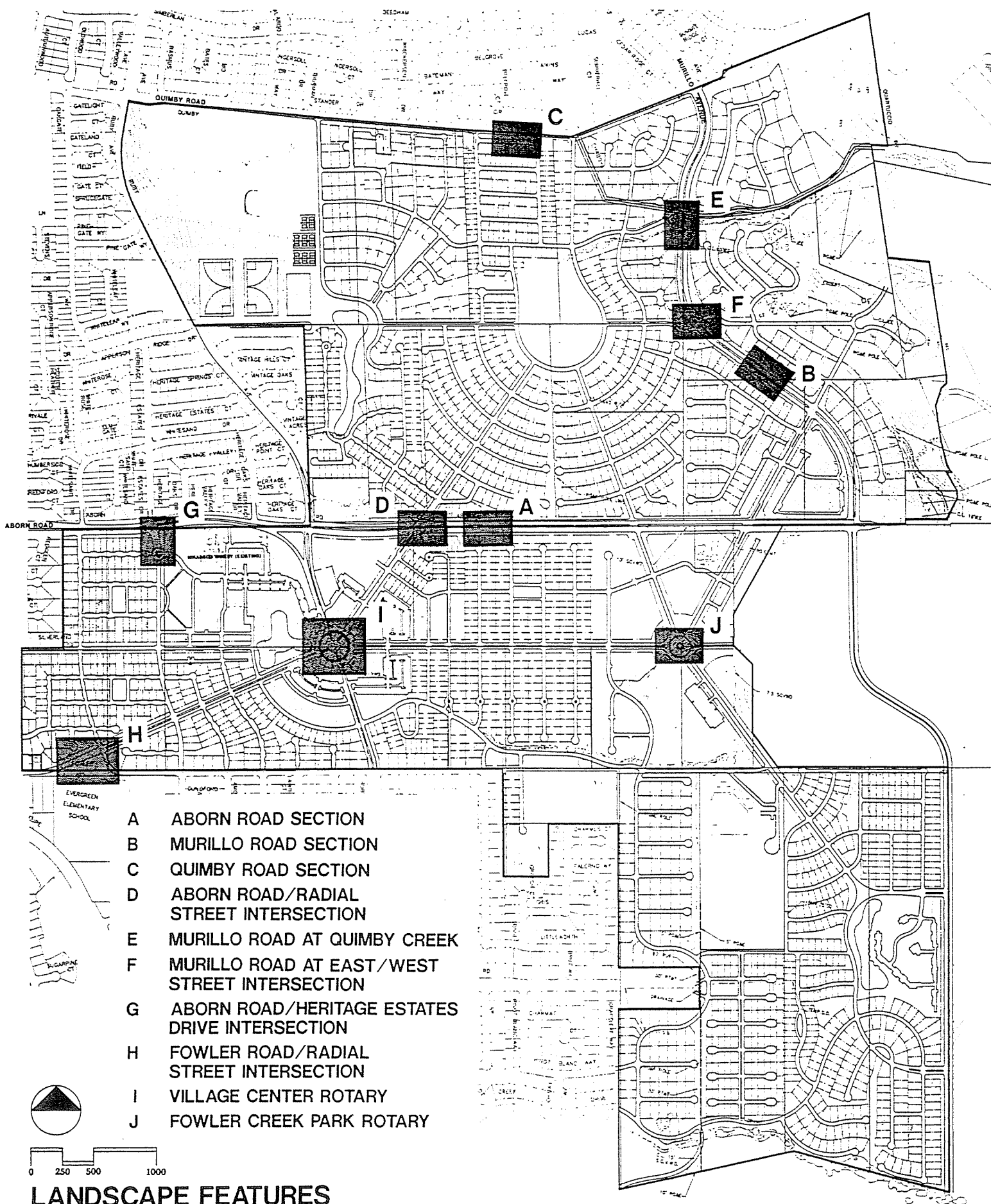
The treatment of Murillo/Yerba Buena Avenue will also be a through-community street similar to Aborn Road. Murillo/Yerba Buena Avenue will also serve a great deal of the traffic flow to and from the campus industrial property. Murillo/Yerba Buena Avenue will be treated with dense landscaping along the street parkways and deciduous broad-leaf canopy trees.

Exhibit 21 provides plan and section views of Aborn Road and Murillo/Yerba Buena Avenue.

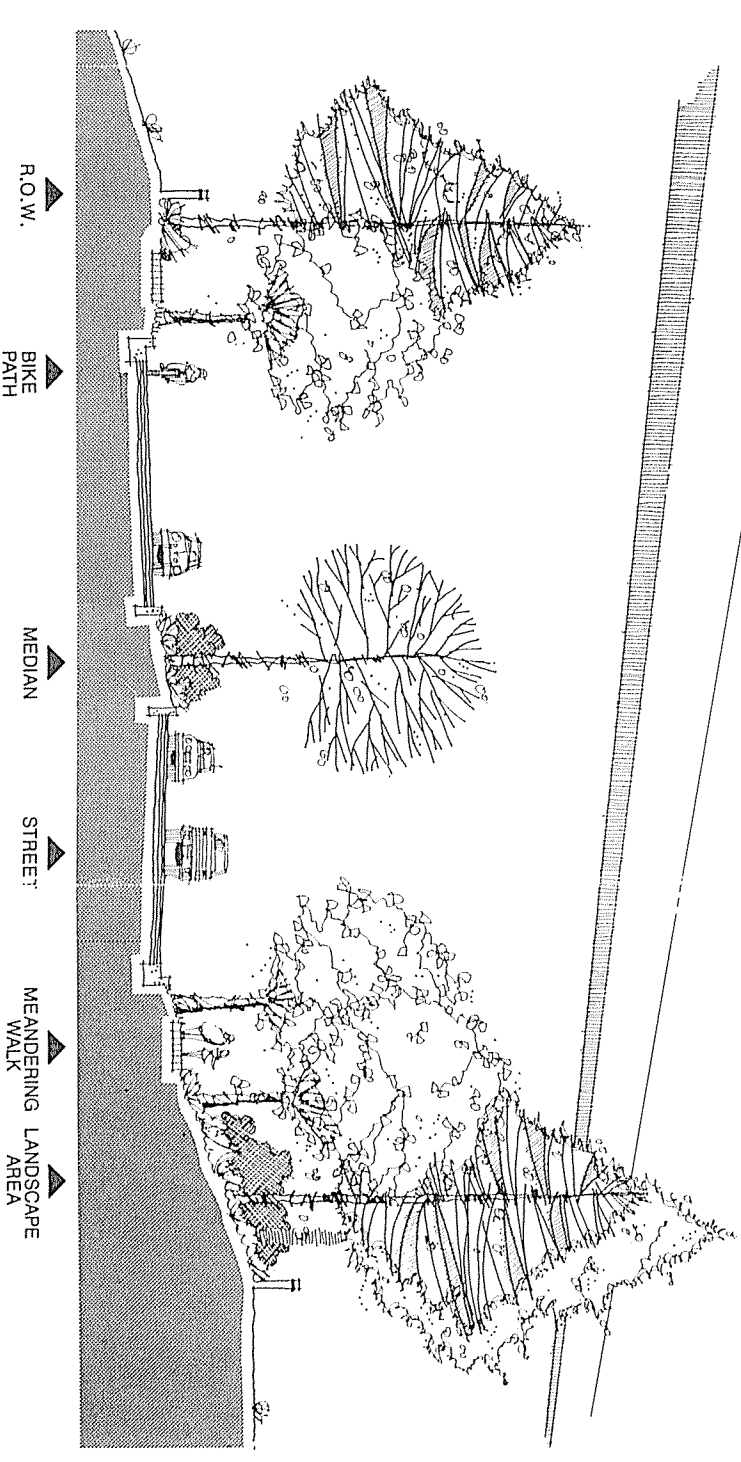
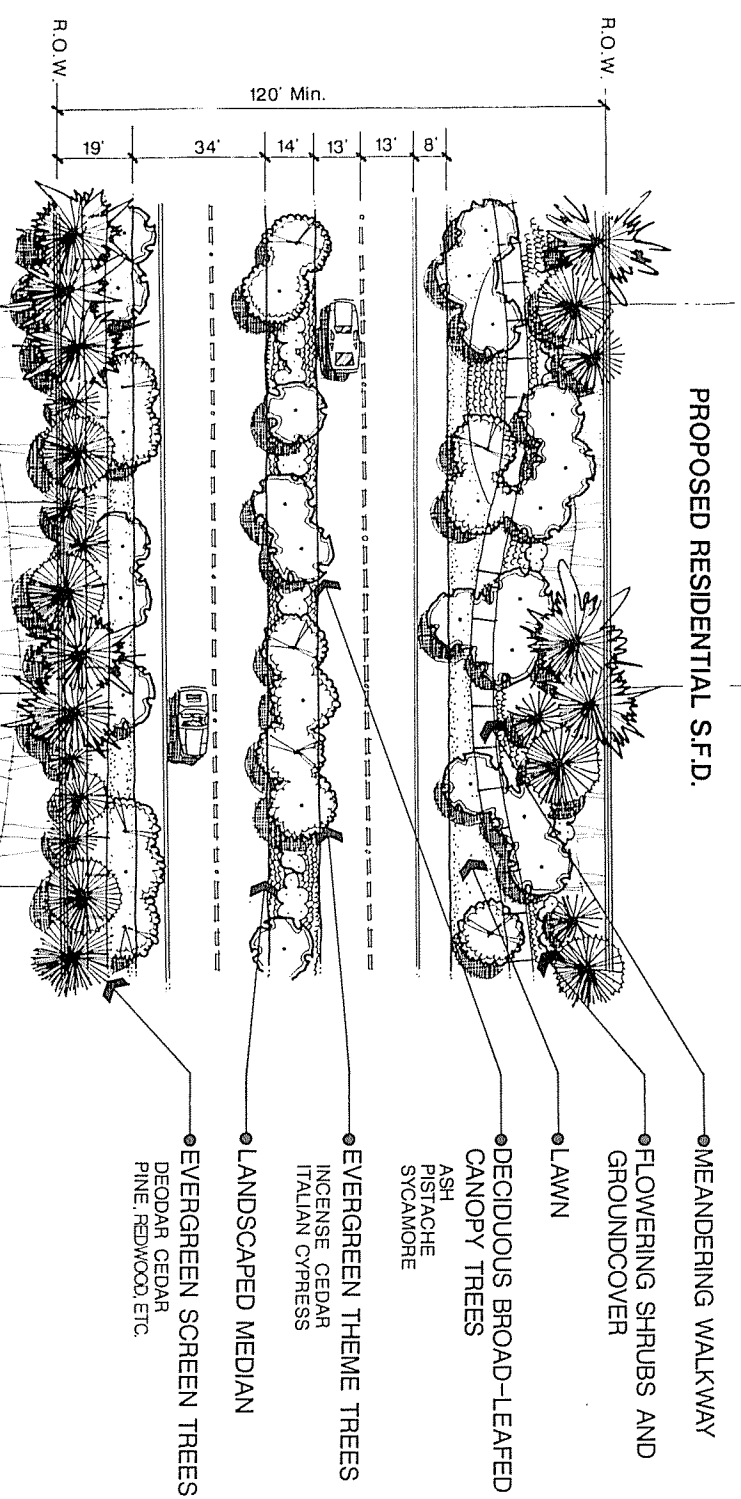
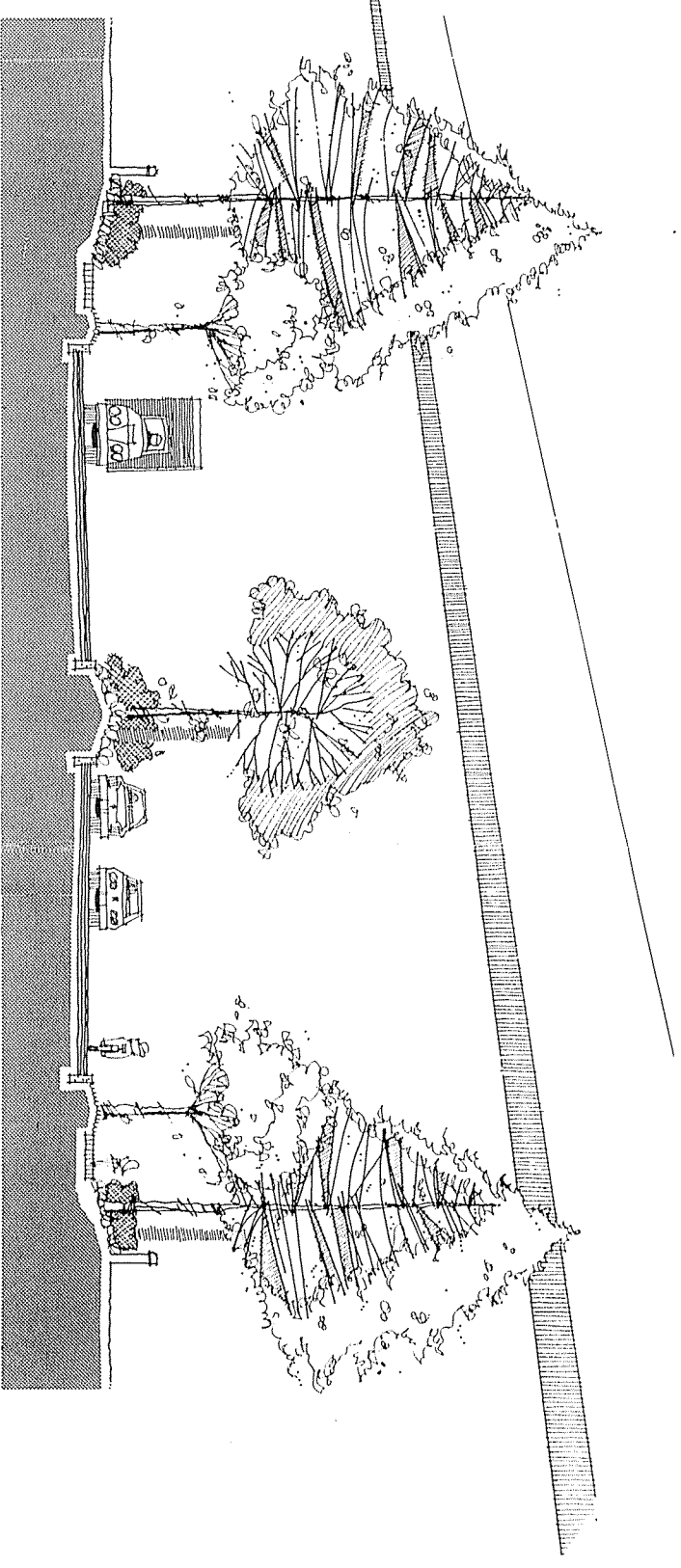
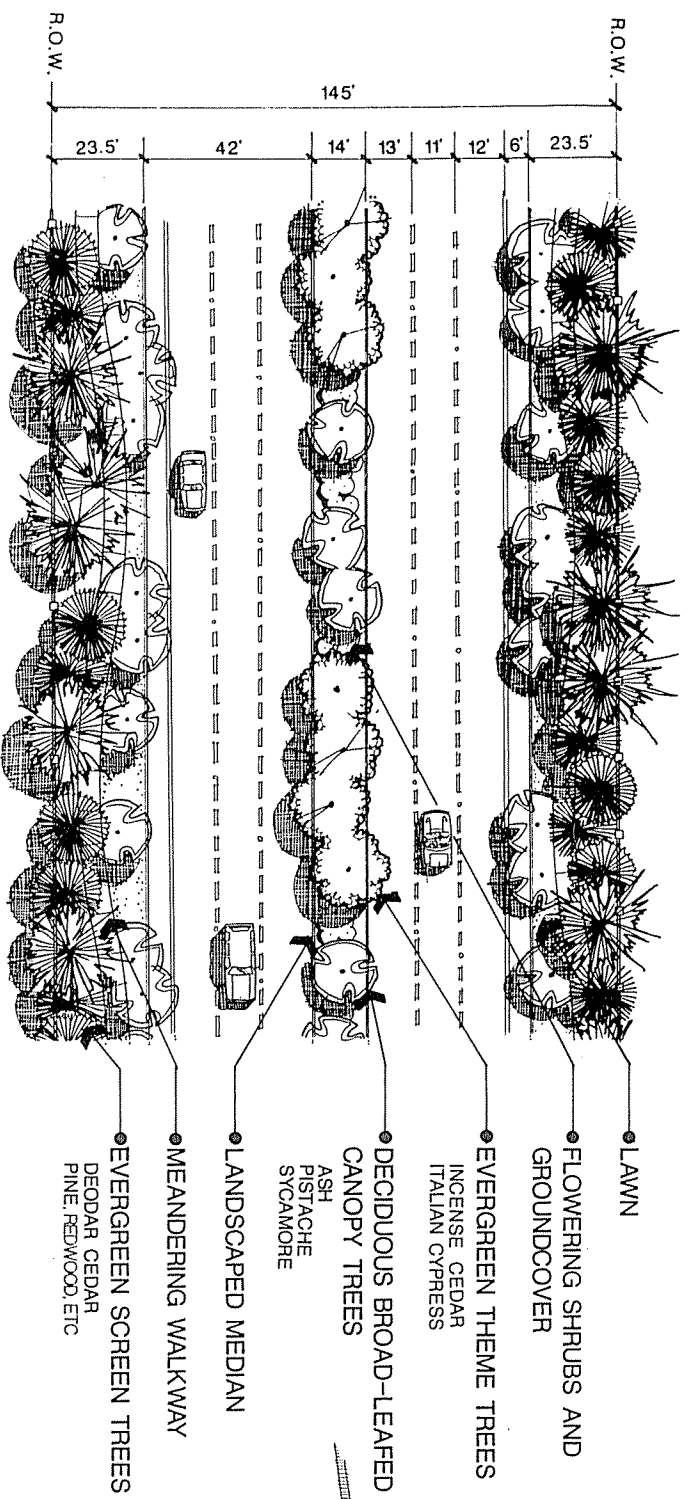
#### **Quimby Road:**

Quimby Road borders the Specific Plan area on the north and serves as a transition to the project. The existing portions of Quimby Road to the north provide some of the better examples of street landscaping in the immediate area. The street landscaping is characterized by half walls along Quimby Road and single-fronted residential streets on the other side of the wall. This allows a significant street such as Quimby Road to maintain an open feeling through the use of the half walls, yet still provides definition between the residential neighborhood and the street.

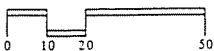
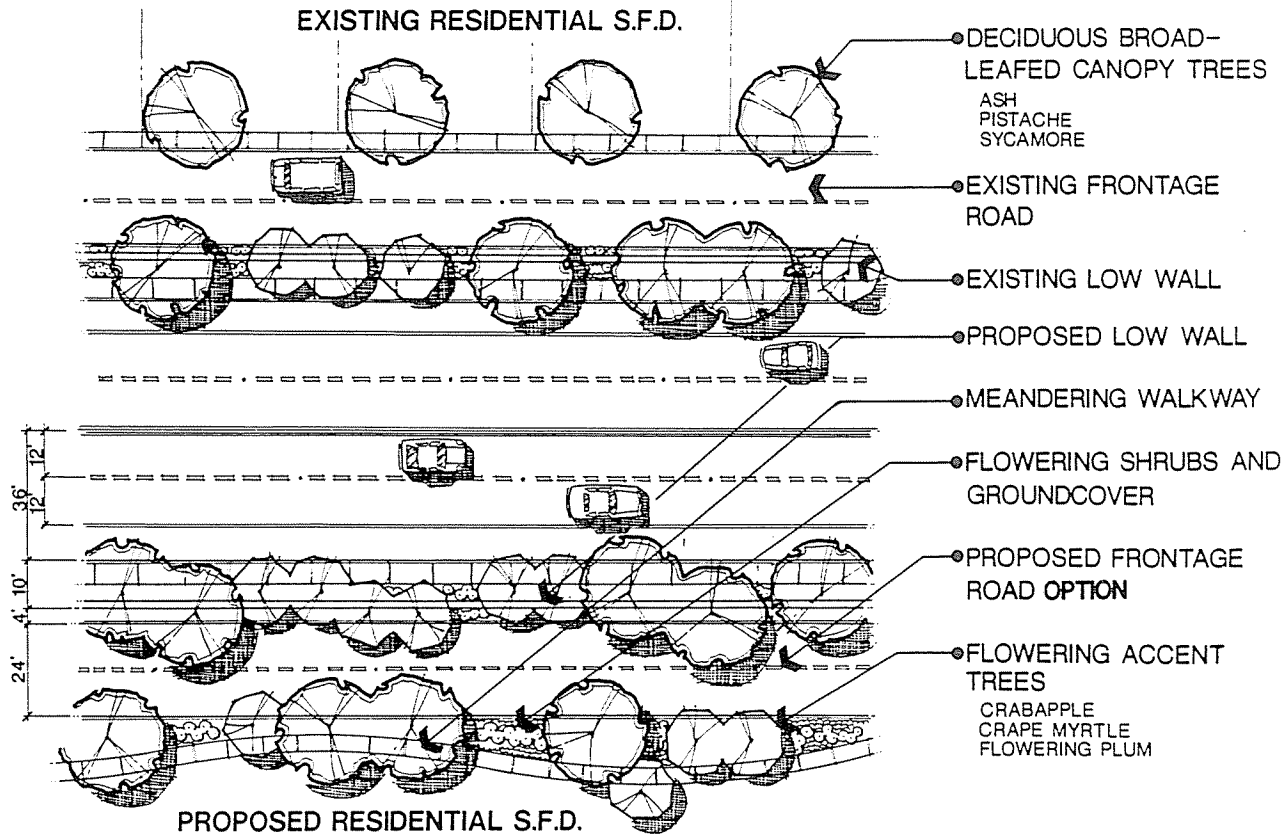
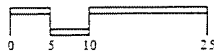
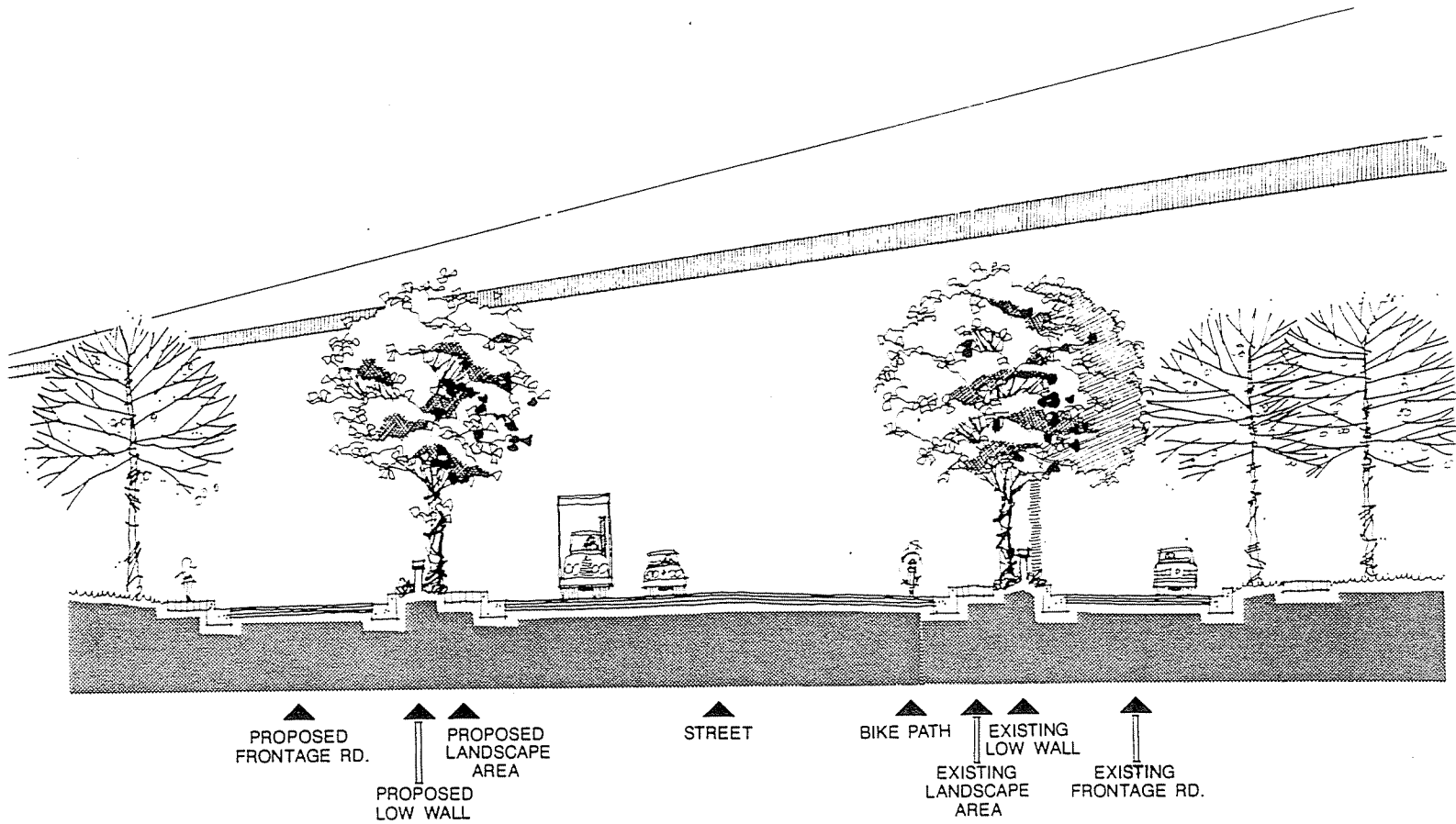
The Specific Plan recognizes the aesthetic value of the existing street landscaping and wall treatment on the north side of Quimby Road, and proposes to duplicate the treatment on the south side. Exhibit 22 provides plan and section views of Quimby Road.











## "C" QUIMBY ROAD PLAN AND SECTION

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EVERGREEN SPECIFIC PLAN

EXHIBIT

22

### **Treatment of Major Intersections (Exhibit 23)**

The establishment of the diagonal radial streets which cross Aborn Road at acute and obtuse angles provide an interesting streetscape opportunity. By designing the soundwalls to favor the alignment of the radial streets, generous landscape pockets are created, which can be planted with the theme Evergreen trees and flowering shrubs. The addition of upgraded paving within crosswalks which are parallel to the radial street, and intentionally omitting the upgraded paving from the crosswalks perpendicular to the radial streets provides additional emphasis to the radial streets as they cross Aborn Road. The Evergreen Specific Plan will provide upgraded paving at walkway intersections, landscaped medians, and turf-lined streets.

### **Quimby Creek Crossing Murillo Avenue (Exhibit 24)**

The intersection of Murillo Avenue at Quimby Creek represents a unique condition within the Evergreen Specific Plan. Due to the alignment of Murillo Avenue, as well as the existence of Quimby Creek, the Evergreen Specific Plan has identified this particular intersection as having great significance. As is shown on Exhibit 24, a desiltation/detention basin is placed at this location, which is also the location where winter storm water run-off is diverted into underground storm drain pipes. Because of the visibility of this intersection, the detention basin will be densely landscaped to screen the basin and preserve the natural setting of the existing creek.

### **Murillo Avenue at the East/West Radial Intersection (Exhibit 25)**

The unusual alignment of this intersection is created by the east/west radial street intersecting with Murillo Avenue. The unique "S" shape of the east/west radial street is necessary to create a ninety degree angle at the intersection in order to provide maximum visibility and safety. The unique shape also offers an opportunity to create a community focal point at this location. With a backdrop of massed Evergreen theme trees, two symmetrical fountains or landscape accents are featured with cupped decorative walls and flowering accent trees. The low decorative walls reflect the similar pattern of the larger soundwalls along Murillo Avenue, then terminate into a pedestrian gateway featuring pilasters capped with themed metal arches. Sweeping masses of canopy trees balance the opposing accents and complete the symmetry of the intersection.

### **Aborn Road at Heritage Estates Drive (Exhibit 26)**

As one of the prominent entrances into the Specific Plan, the intersection of Aborn Road at Heritage Estates Drive offers one of the first glimpses into the heritage vineyard and the Village Center beyond. To highlight this location, a traditional-style fountain or landscape accent surrounded by decorative bollards, upgraded paving, and seasonal annuals acts as the gateway centerpiece. Decorative soundwalls have been placed as to frame the intersection as well as to expand the visual corridor across the vineyards to the tree-lined entrance to the Mirassou property. Small clusters of flowering accent trees contrast the masses of Evergreen



DECIDUOUS BROAD-LEAFED CANOPY TREES ●

ASH  
PISTACHE  
SYCAMORE

EVERGREEN THEME TREES ●

INCENSE CEDAR  
ITALIAN CYPRESS

FLOWERING ACCENT TREES ●

CRABAPPLE  
GRAPE MYRTLE  
FLOWERING PLUM

EVERGREEN SCREEN TREES ●

DEODAR CEDAR  
PINE  
REDWOOD, ETC.

PROPOSED RESIDENTIAL S.F.D.

PROPOSED TOWNHOMES

PROPOSED CARRIAGE HOMES

●LAWN

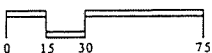
●UPGRADED PAVING

●SIGNAL LIGHT

●DECORATIVE  
SOUNDWALL

MEANDERING SIDEWALK

●LANDSCAPED MEDIAN WITH  
UPGRADED PAVING AT NOSE



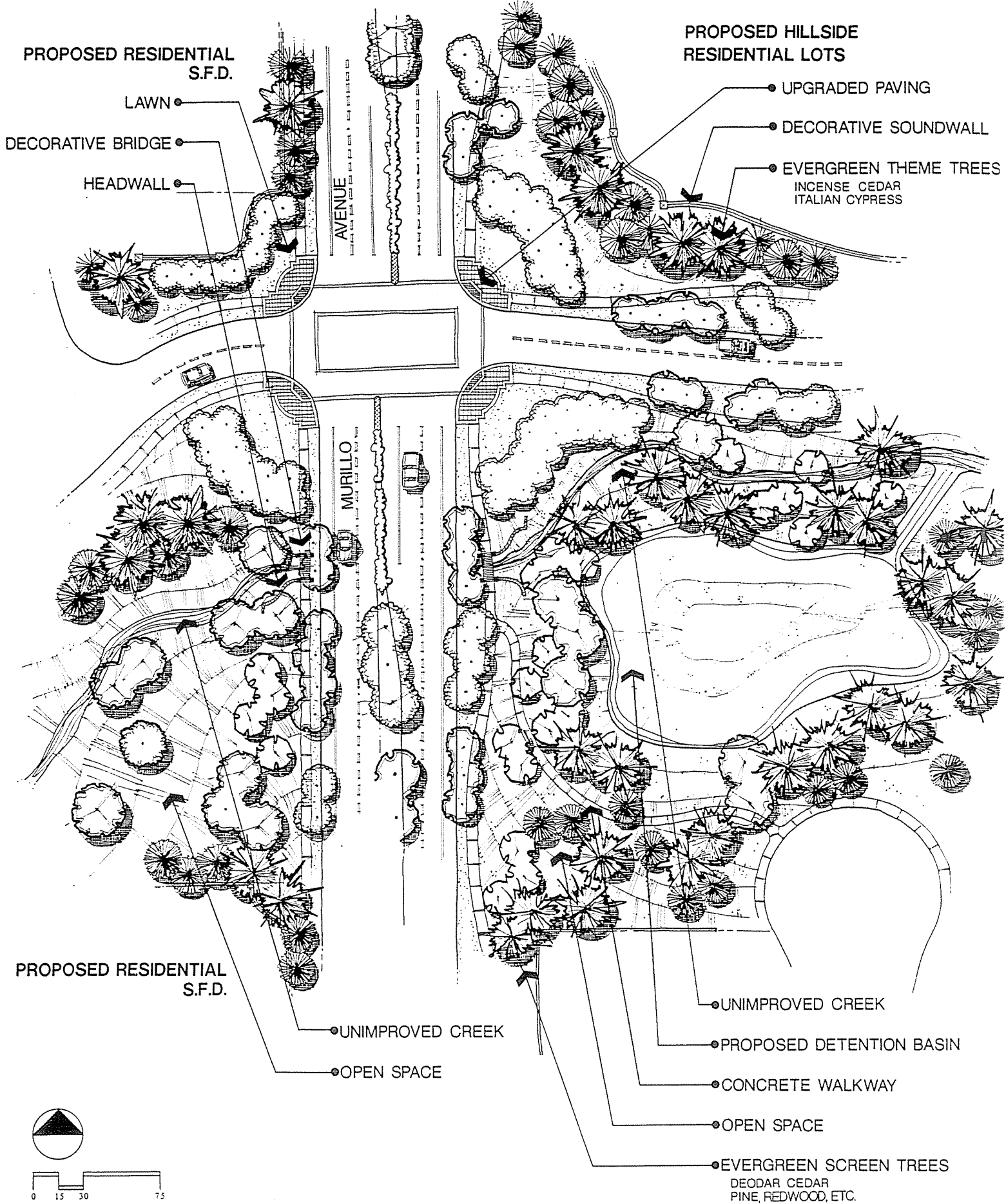
**"D" ABORN ROAD AT THE NORTH EAST RADIAL STREET**

CITY OF SAN JOSE

EVERGREEN SPECIFIC PLAN

EXHIBIT

23



## "E" MURILLO AVENUE AT QUIMBY CREEK

CITY OF SAN JOSE  
EVERGREEN SPECIFIC PLAN

EXHIBIT

24

PROPOSED RESIDENTIAL S.F.D.

DECIDUOUS BROAD-LEAFED  
CANOPY TREES  
ASH  
PISTACHE  
SYCAMORE

DECORATIVE SOUNDWALL

DECORATIVE FOUNTAIN  
(OR LANDSCAPE ACCENT)

EVERGREEN THEME TREES  
INCENSE CEDAR  
ITALIAN CYPRESS

DECORATIVE WALL AND  
PILASTERS

PROPOSED RESIDENTIAL  
S.F.D.

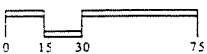
FLOWERING SHRUBS AND  
GROUNDCOVER

FLOWERING ACCENT TREES  
CRABAPPLE  
CRAPE MYRTLE  
FLOWERING PLUM

6' WIDE MEANDERING WALKWAY

UPGRADED PAVING

LAWN

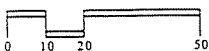
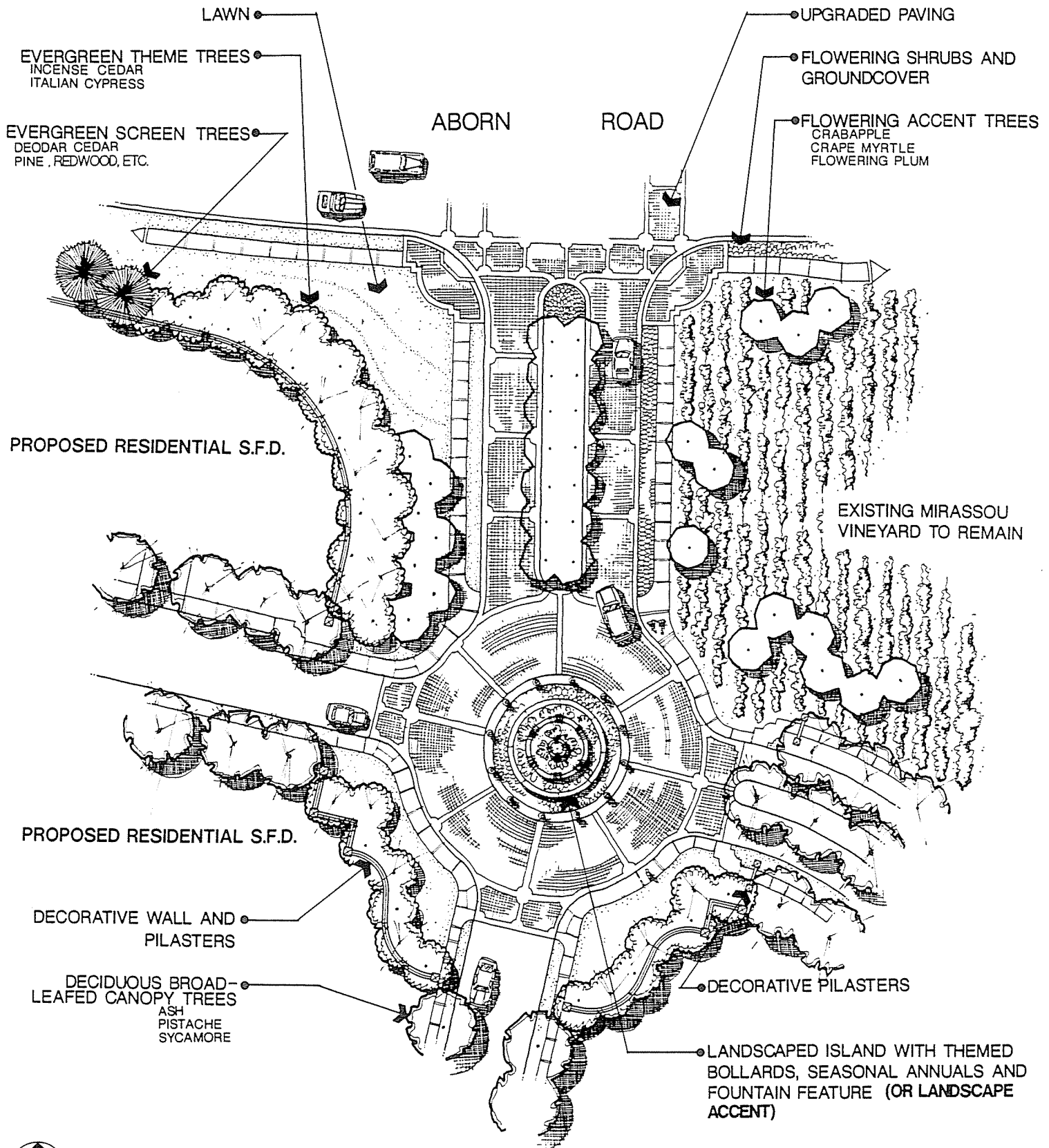


## "F" MURILLO AVENUE AT THE EAST/WEST STREET INTERSECTION

CITY OF SAN JOSE

EVERGREEN SPECIFIC PLAN

EXHIBIT



## "G" ABORN ROAD AT HERITAGE ESTATES DRIVE

CITY OF SAN JOSE

EVERGREEN SPECIFIC PLAN

EXHIBIT

26

theme trees and mounded turf, but also provides a subtle transition from the proposed landscaping to the relaxed charm of the heritage vineyards.

#### **Fowler Road/Radial Street Intersection (Exhibit 27)**

The Evergreen Specific Plan has proposed that Fowler Road be redirected into the project area and a landscaped "island" be created in front of the Evergreen Elementary School. Several factors led to the decision to redirect the street. The two most important factors were the location of the elementary school directly on Fowler Road and the high number of existing residential homes which front directly onto Fowler Road east of the elementary school. If the existing conditions along Fowler Road were maintained, and the specific plan developed the expected number of residential units in the project area, the traffic impacts on the existing school and residents could have been extreme.

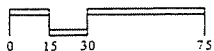
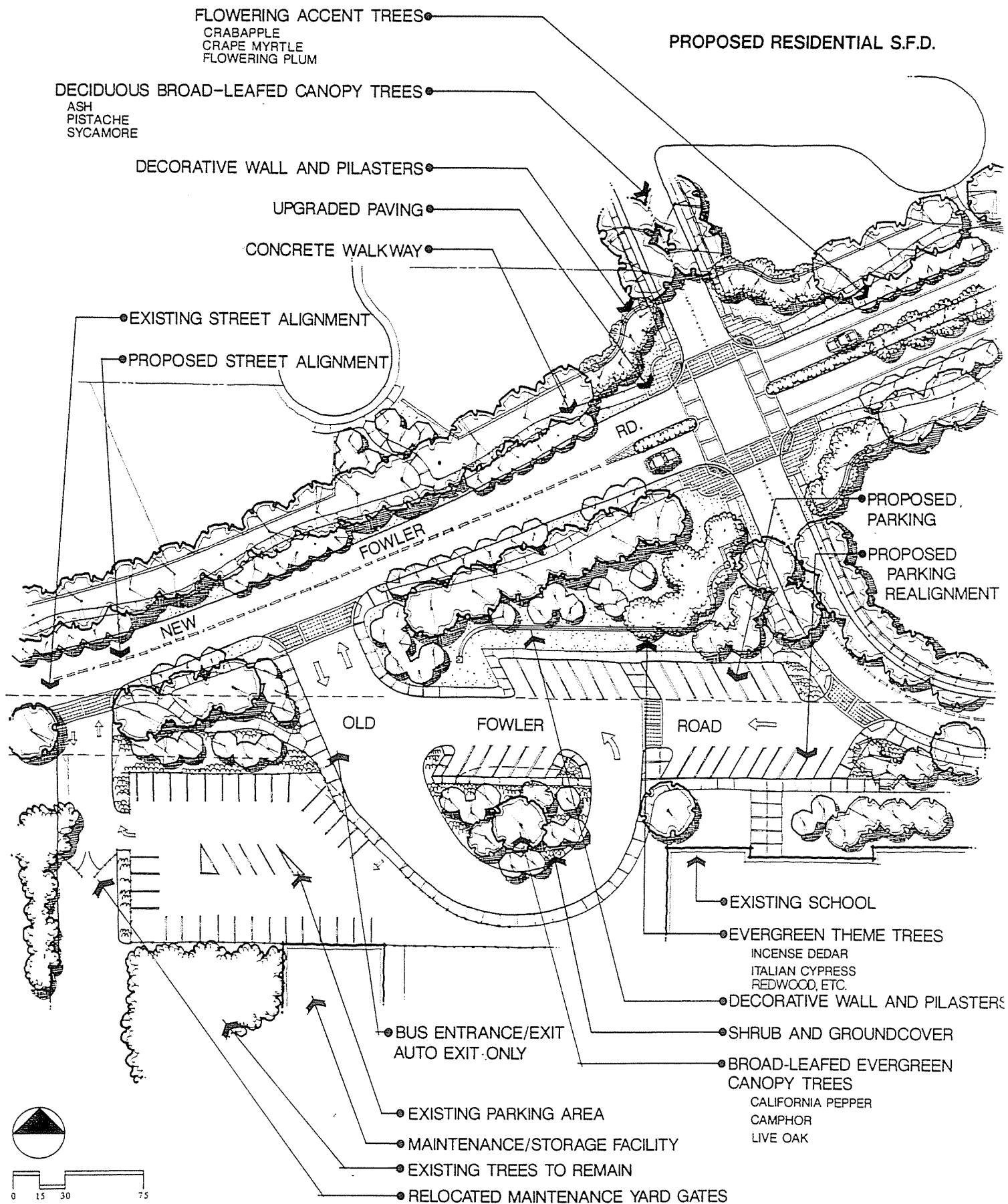
In an effort to redirect the majority of the new project traffic into the plan area, the redirected Fowler Road protects the existing school and residents by removing them from the "through" route.

#### **Village Center Rotary (Exhibit 28)**

As the most prominent public space within the Evergreen Specific Plan, the Village Center rotary can be considered the community core. At the heart of the rotary stands the central fountain spire, topped with a figurative sculpture (Exhibit 30). This spire serves as a landmark visible throughout the community and is on an axis with the radial street pattern. Accent planting of seasonal annuals surround the fountain pool along with upgraded paving, decorative benches, and themed banners. The central area of the rotary is intended to function as a pedestrian gathering place, which can host numerous public activities such as art festivals, cultural events, and noontime lunches. Distinctive Canary Island palms and flowering accent trees accentuate the central fountain spire, while arched metal trellises, laced with flowering vines, provide seasonal color and shelter from the summer sun. Upgraded paving throughout the street and the park, along with themed bollards, reinforce the link with the Village Center shops and define this area as a pedestrian-oriented space.



#### **Fowler Park Rotary (Exhibit 29)**

Second only to the Village Center rotary, the Fowler Creek Park rotary also serves as one of the most prominent public spaces within the Evergreen Specific Plan area. Though slightly smaller in size and design, the Fowler Creek Park rotary also serves as a visible landmark throughout the community and is on an axis with the radial street pattern. Featured within the park is a themed architectural pavilion (Exhibit 30). A direct link between the pavilion and the Fowler Creek Park amphitheater allows the space to be utilized separately or in conjunction with activities within the park.

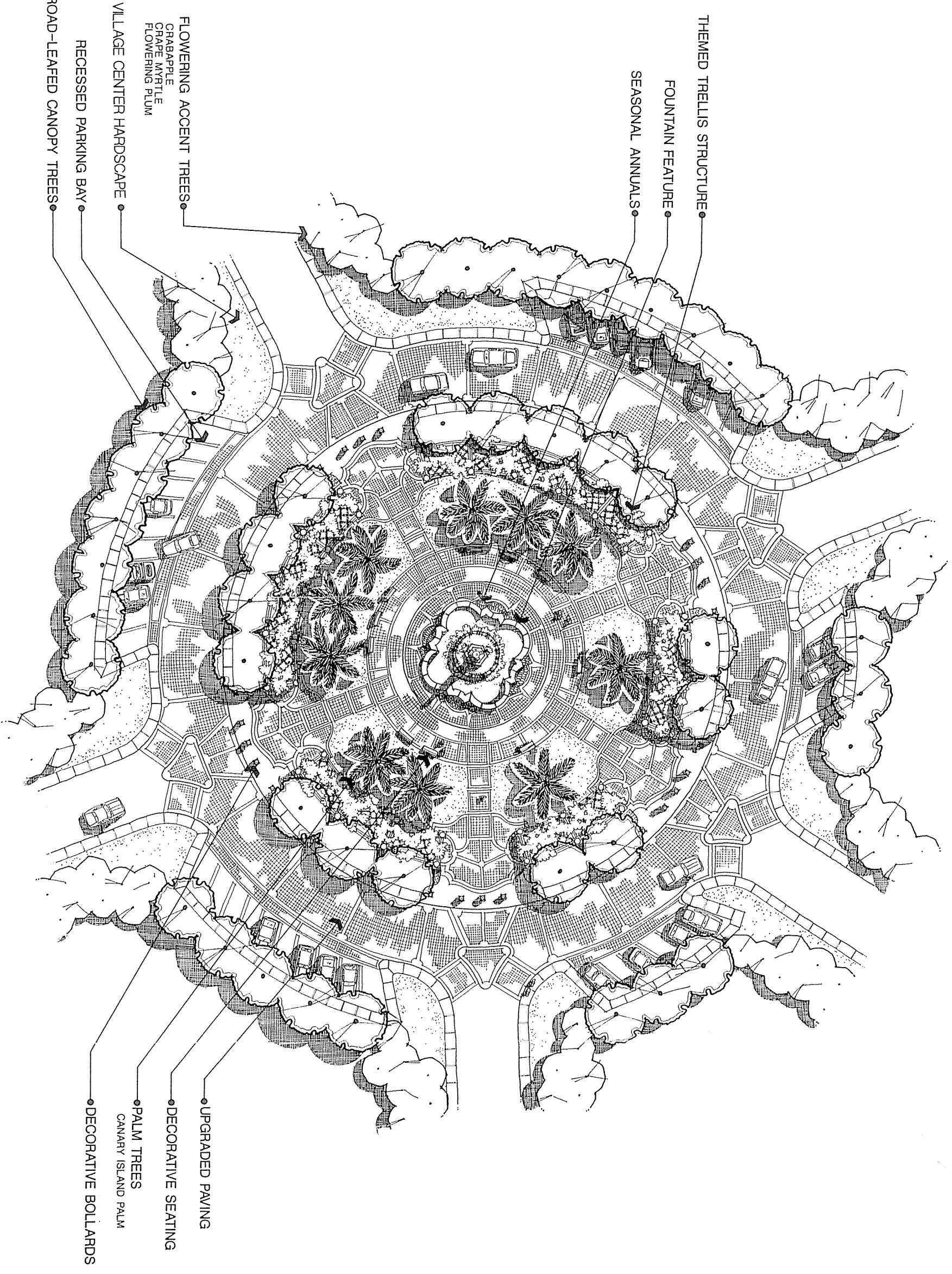


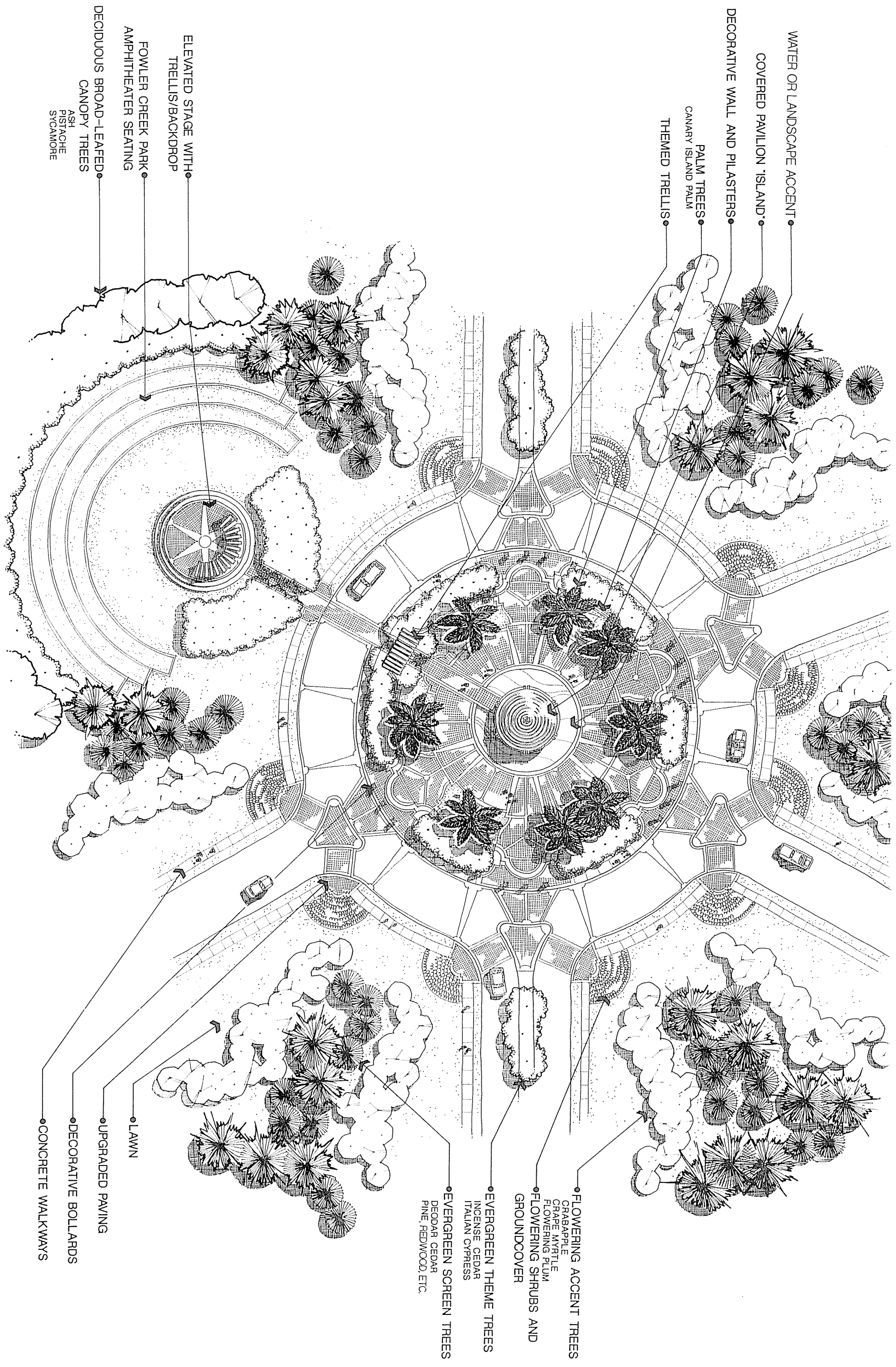
## "H" FOWLER ROAD / RADIAL STREET INTERSECTION





DECIDUOUS BROAD-LEAFED CANOPY TREES  
ASH  
PISTACHE  
SYCAMORE



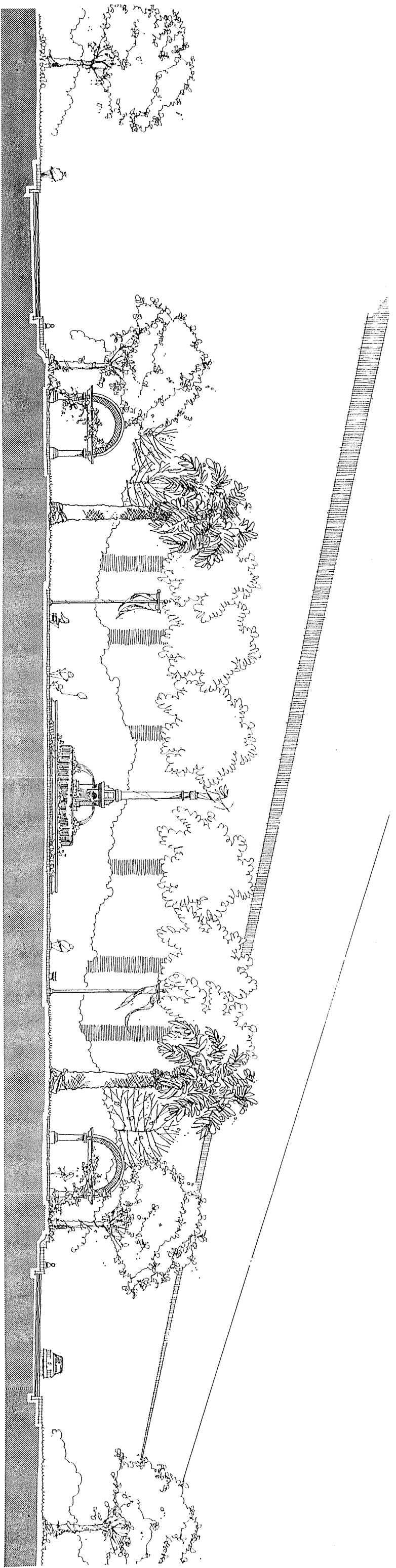


"J" FOWLER CREEK PARK ROTARY

CITY OF SAN JOSE

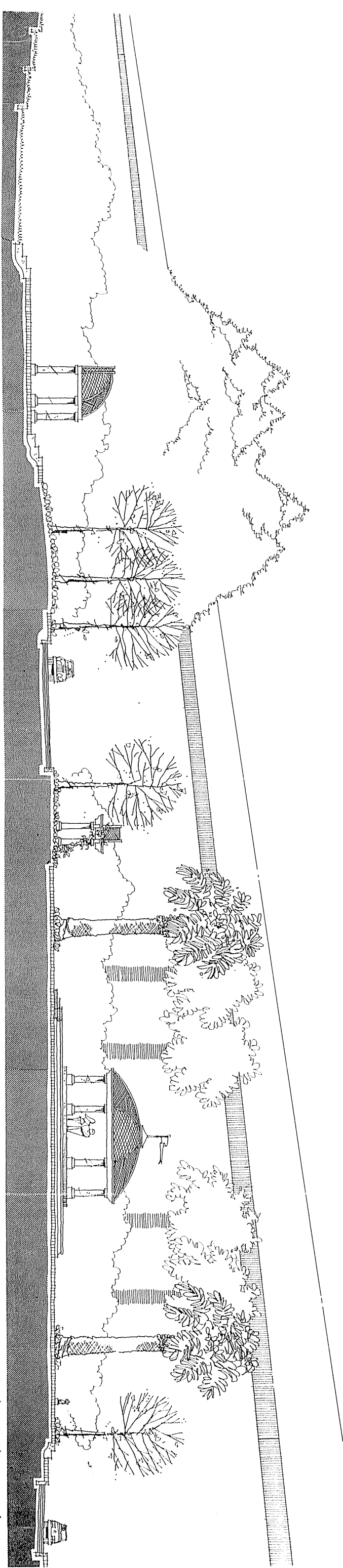
EVERGREEN SPECIFIC PLAN





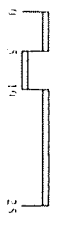
- ▲ ROTARY STREET
- ▲ DECORATIVE BOLLARD
- ▲ THEMED TRELLIS STRUCTURE
- ▲ PALM TREE
- ▲ DECORATIVE SEATING
- ▲ FOUNTAIN FEATURE
- ▲ CENTRAL PLAZA
- ▲ DECORATIVE FLAG POLES
- ▲ WALKWAY
- ▲ ROTARY STREET
- ▲ DECIDUOUS BROAD-LEAF CANOPY TREE

VILLAGE CENTER ROTARY

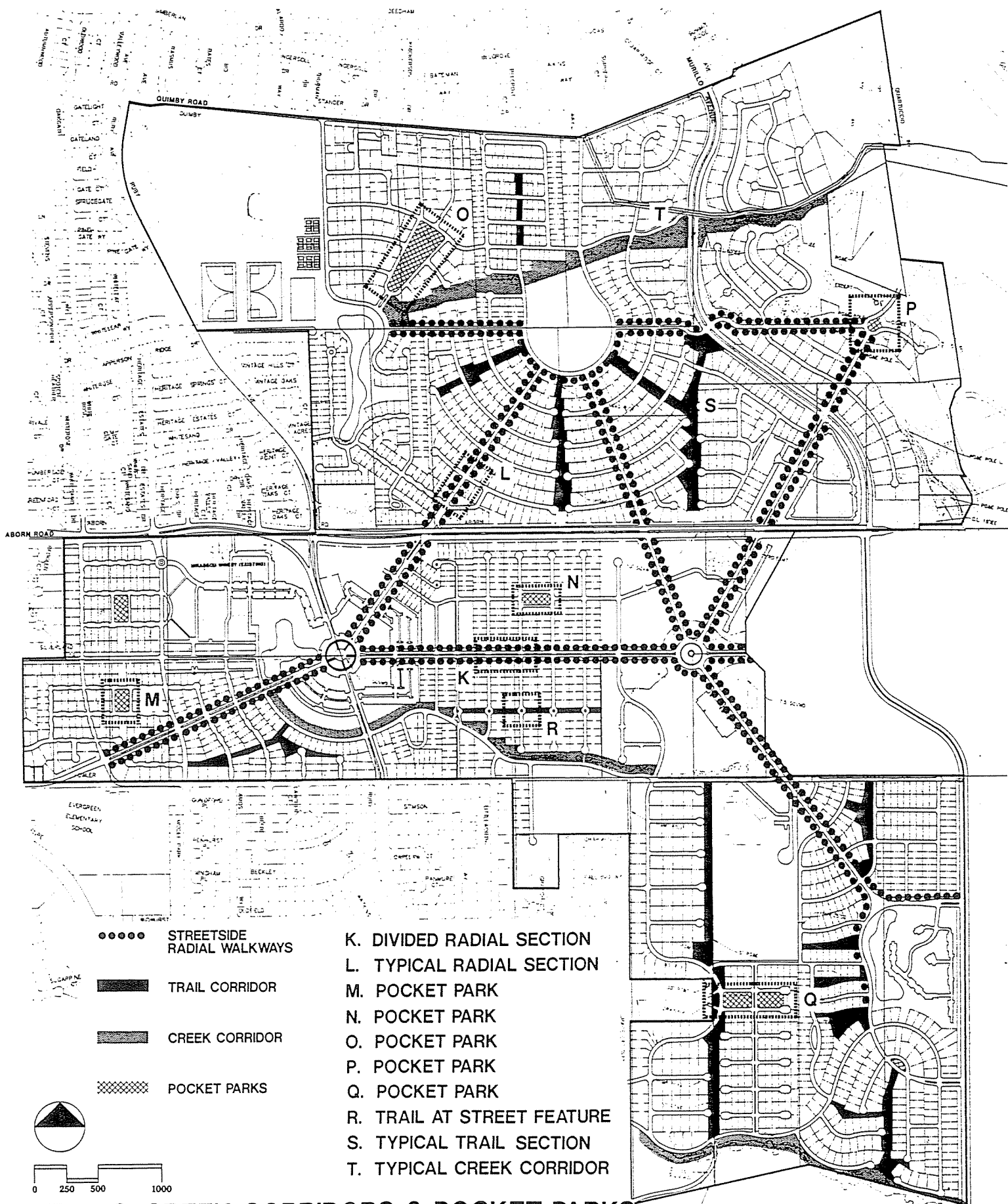


- ▲ AMPHITHEATRE SEATING
- ▲ ELEVATED STAGE
- ▲ TRELLIS BACKDROP
- ▲ EVERGREEN THEME TREE
- ▲ ROTARY STREET
- ▲ EVERGREEN THEME TREE
- ▲ THEMED TRELLIS STRUCTURE
- ▲ PALM TREE
- ▲ COVERED PAVILION ISLAND
- ▲ CENTRAL PLAZA
- ▲ DECORATIVE BOLLARD
- ▲ WALKWAY
- ▲ ROTARY STREET

FOWLER CREEK PARK ROTARY



ROTARY SECTIONS



## TRAILS, CREEK CORRIDORS & POCKET PARKS

While technically a feature of the Fowler Creek Park, the amphitheater has been designated as an integral element of the rotary periphery. Canary Island palms, turfed areas, and low decorative walls encircle the pavilion, while flowering accent trees frame the street corners and rotary edges. As in the Village Center rotary design, decorative bollards and upgraded paving utilized within the park, the rotary street, and crosswalks add to the character of the area. Open space areas at the periphery of the rotary are landscaped with pockets of flowering shrubs, extensive turf areas, Evergreen theme and flowering accent trees, all call attention to the park center.

### **Radial Collector Streets**

The radial collector street sections (Exhibit 32) are designed specifically to create open vistas to the focal points of the two rotaries and the northern elementary school site. Two types of radial collectors are proposed; radial collectors with and without medians.

#### **Radial Collectors With Medians:**

The radial collectors with medians will provide a broad boulevard entrance to the Village Center from Fowler Road and then connect the Village Center rotary with the Fowler Creek Park rotary to the east. The radial collectors with a median will have broad thirty foot (30') parkways from the curbs and provide a tree-lined route.

#### **Radial Collectors Without Medians:**

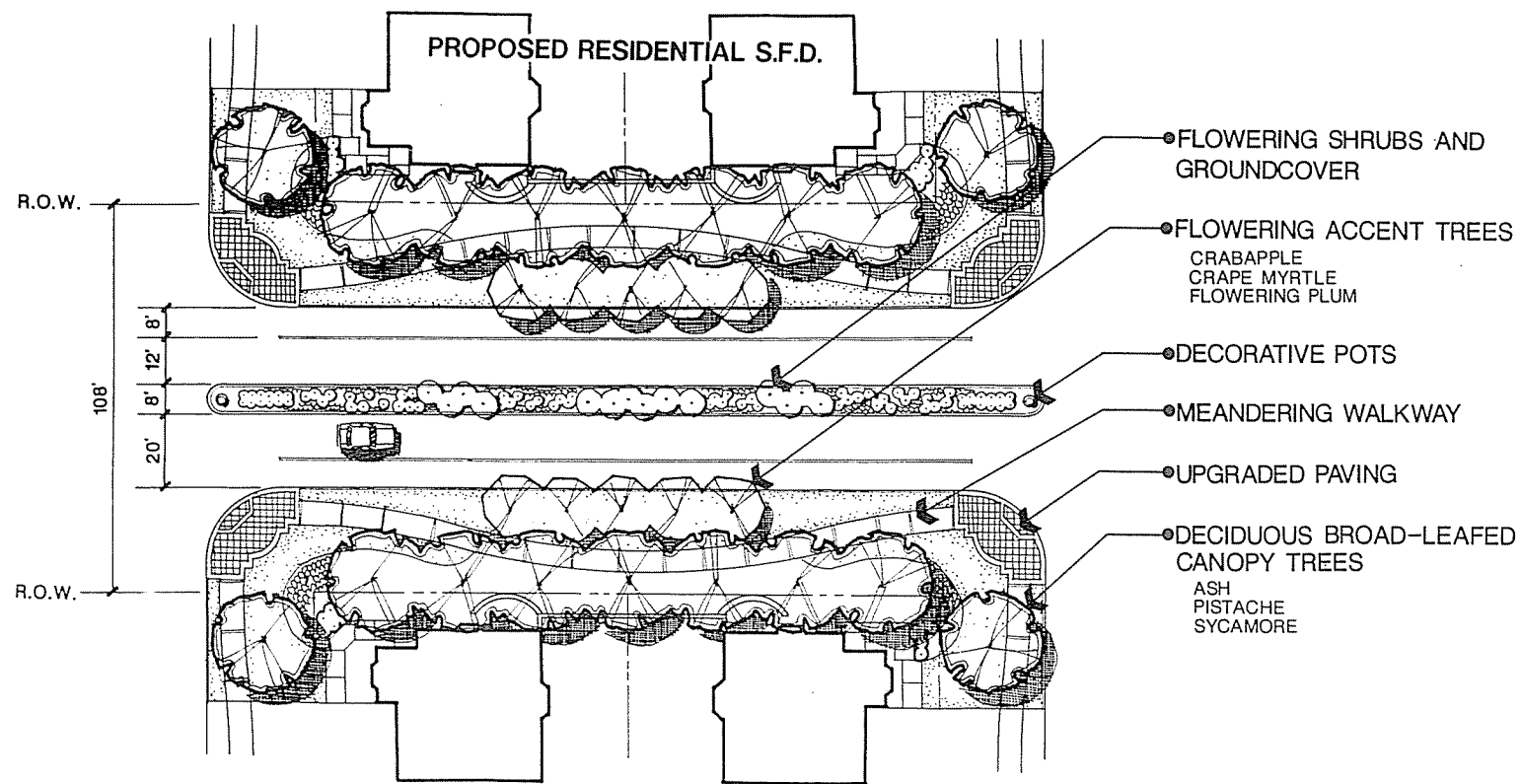
These radial collectors, although slightly less in width than the radial collectors with medians, still provide a broad tree-lined vista with a thirty foot (30') landscaped parkway crossing Aborn Road and connecting the Village Center rotary to the northern school site and the school site to the Fowler Creek Park rotary.

### **Neighborhood Streets**

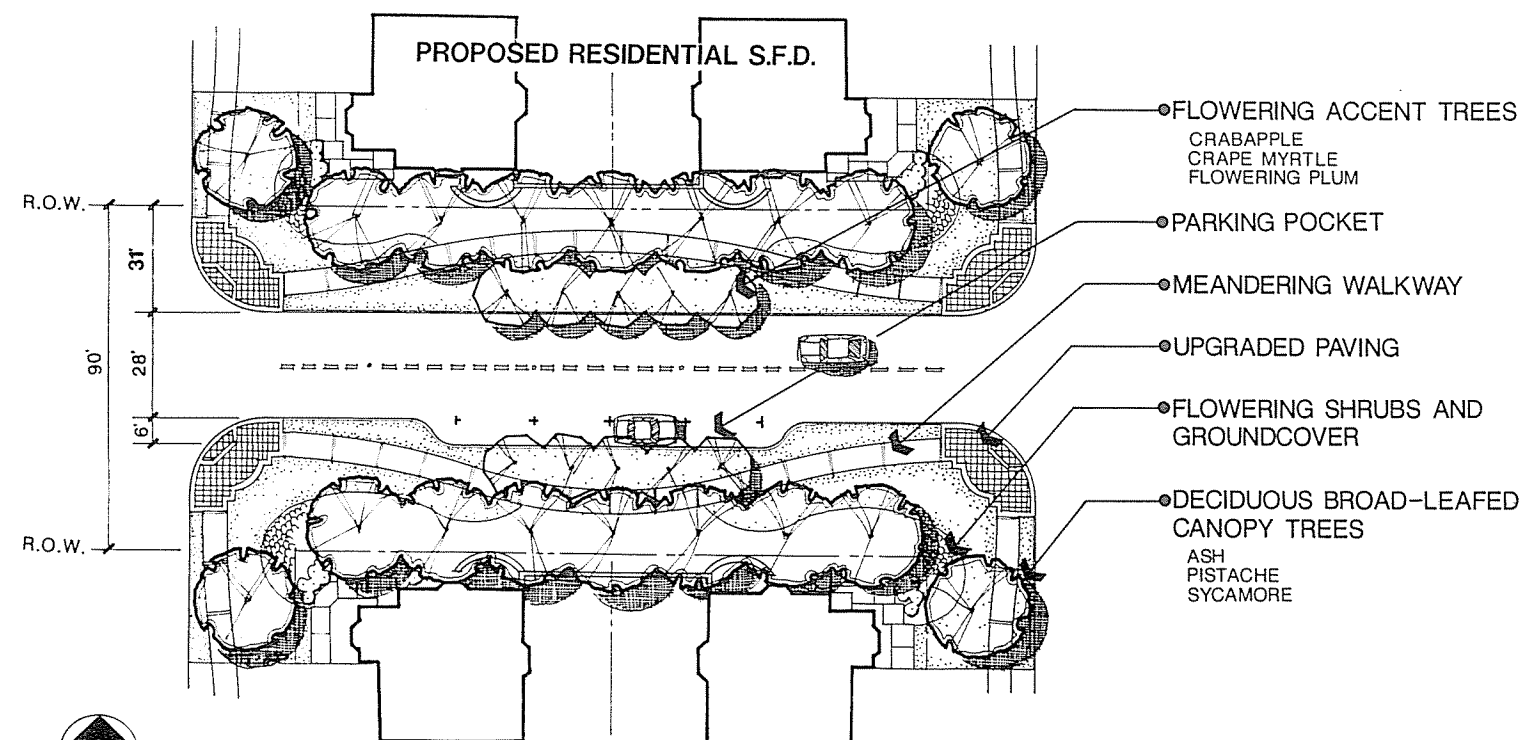
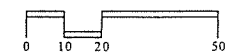
All neighborhood streets shall incorporate a street tree planting and maintenance program (through a Lighting and Landscaping District) of 15 gallon minimum (or base root equivalent) sycamore trees. Tree spacing should be either forty feet (40') along street and thirty-six feet (36') across from each other, or fifty feet (50') along street and thirty feet (30') across from each other.

### **POCKET PARKS**

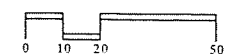
Scattered throughout the community are pocket parks which provide neighborhood-level recreational opportunities in the individual developments.



"K" RADIAL WITH MEDIAN PLAN

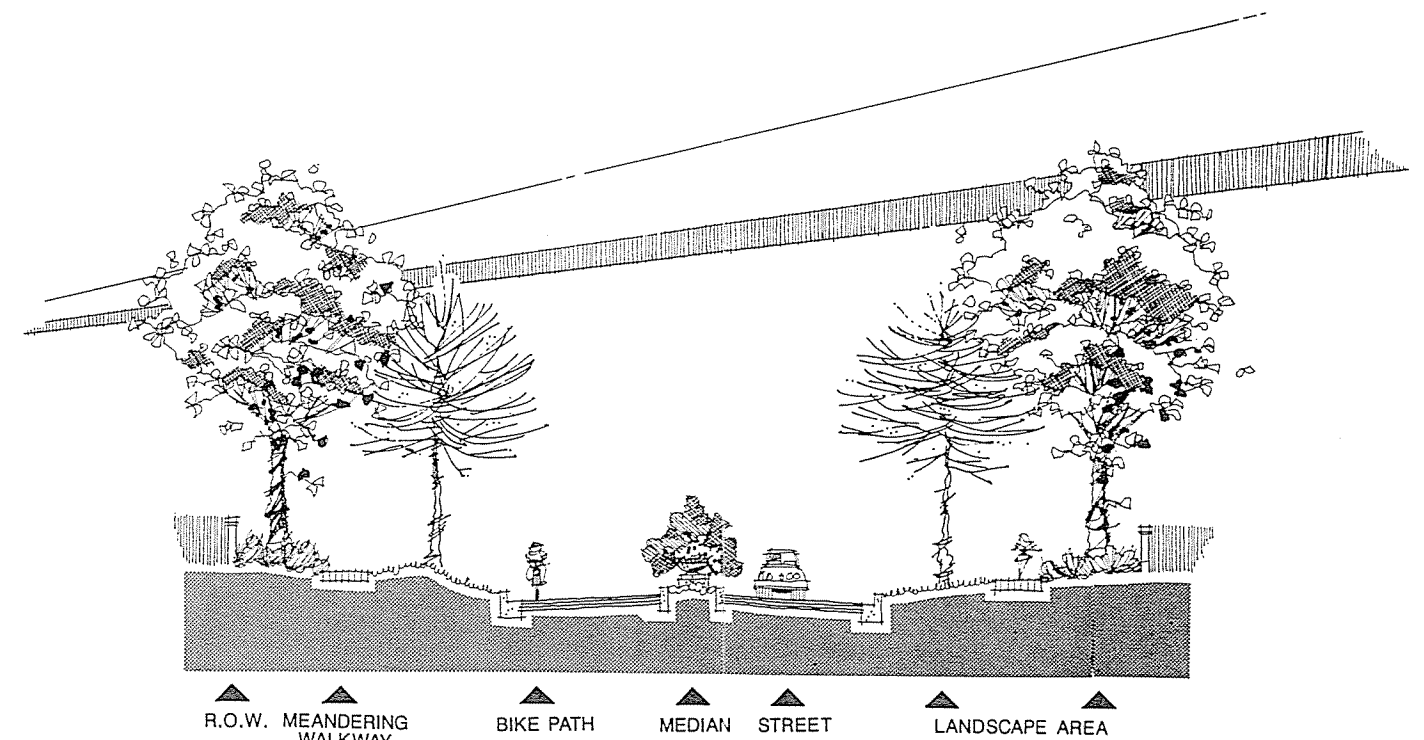


"L" RADIAL WITH PARKING POCKET PLAN

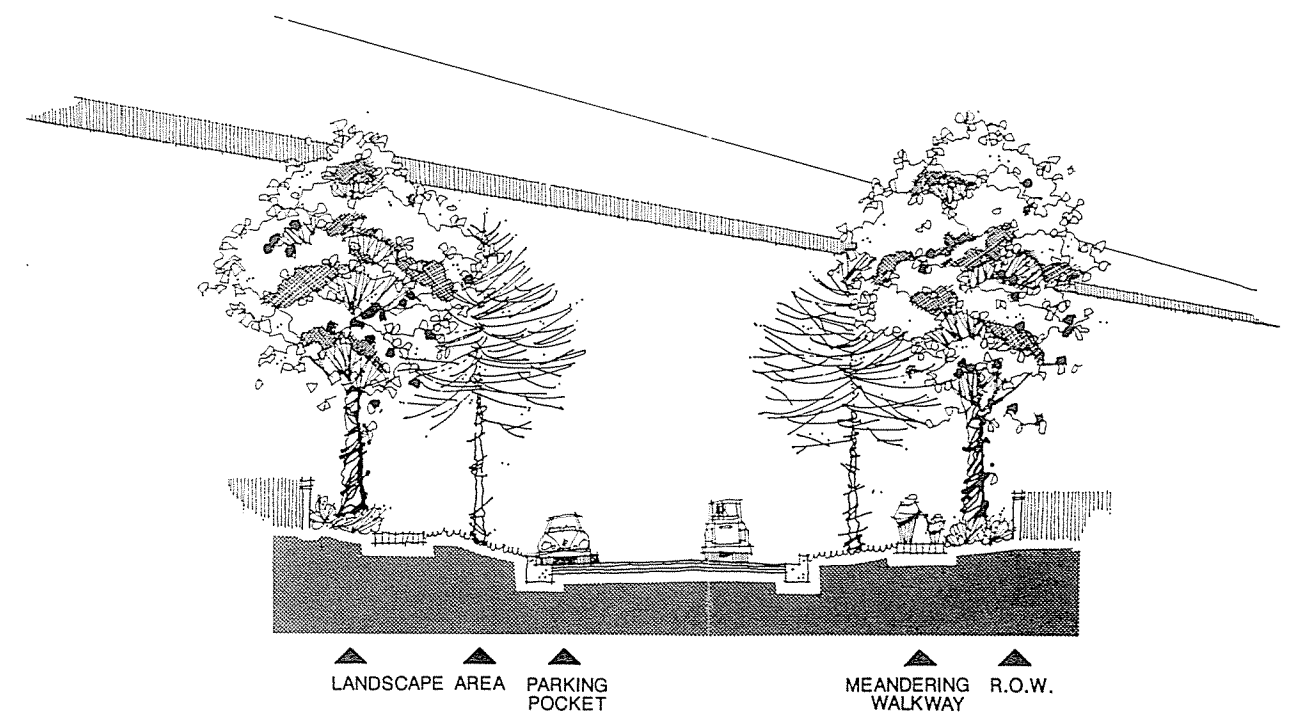
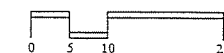


## TYPICAL RADIAL SECTIONS

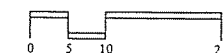
CITY OF SAN JOSE  
EVERGREEN SPECIFIC PLAN



RADIAL WITH MEDIAN SECTION



RADIAL WITH PARKING POCKET SECTION



### **Pocket Park "M"**

The type of pocket park as shown in Exhibit 33 is an optimum location for a tot lot due to its close proximity to the Evergreen Elementary School and its central location in a single family neighborhood. The pocket park also serves as a destination point at the center of one of the residential trail systems. Featured within the tot lot are decorative benches, a generous play area with play elements intended to cover a broad range of children's skill levels, and an architecturally themed trellis structure. Shady broad-leafed canopy trees and flowering accent trees highlight portions of the park and are reflected again at the street corners and trail heads surrounding the space. Rolling lawn areas border the the park and street corners, and the use of decorative bollards, combined with generous areas of upgraded paving at crosswalks and trail connections aid in defining the park as a pedestrian-oriented space.

### **Pocket Park "N"**

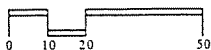
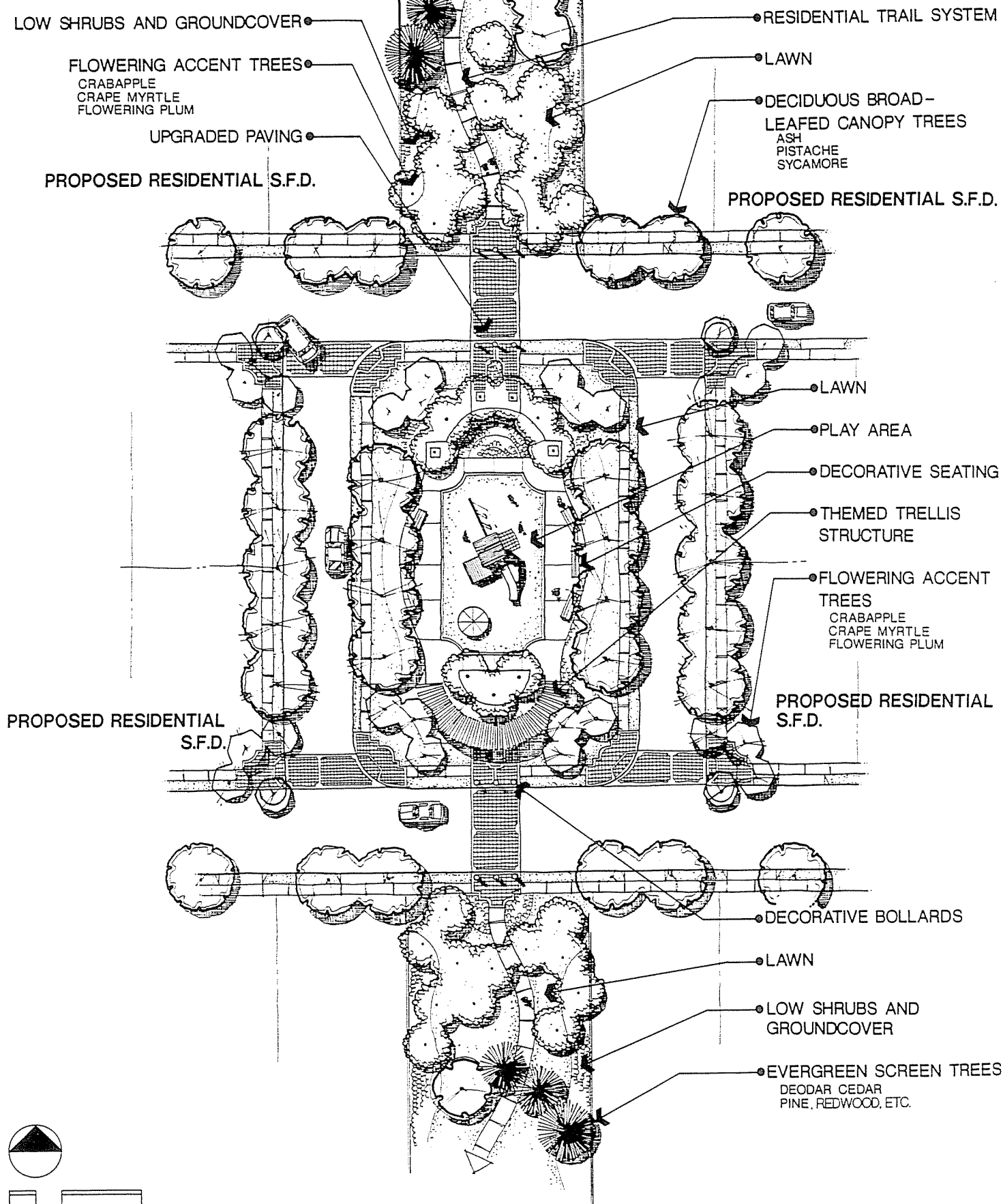
The size and location of this park (Exhibit 34) warrant a bit more attention than most pocket parks within the Evergreen Specific Plan. The park features a themed trellis and a traditional fountain or planter which can serve as a gathering spot. The fountain or planter and trellis have been offset to provide generous room for lawn activities and active play space. Flowering shrubs and accent trees draw attention to focal points and street corners where broad-leafed canopy trees line the streets. Upgraded paving at crosswalks and special site features add richness to the park and define the area as a pedestrian environment.

### **Pocket Park "O"**

As one of the largest open space areas within the Evergreen Specific Plan area, the triangular pocket park (Exhibit 35) offers ample room for both passive and active community uses. In addition to its significant size, the park acts as a link to the Quimby Creek trail system and a dramatic visual corridor to the adjacent lake. While comprised primarily of turf and trees, the park's amenities include a fountain or planter, decorative site furniture, upgraded paving, themed bollards, and an architecturally themed trellis. All of these amenities are intended to maximize public use. Tree massings are intended to flank the park edges and walkways, and to accentuate views toward the lake and special elements within the park. Centered at the intersection of the park axis and the east-west street is a fountain in the lake.

### **Pocket Park "P"**

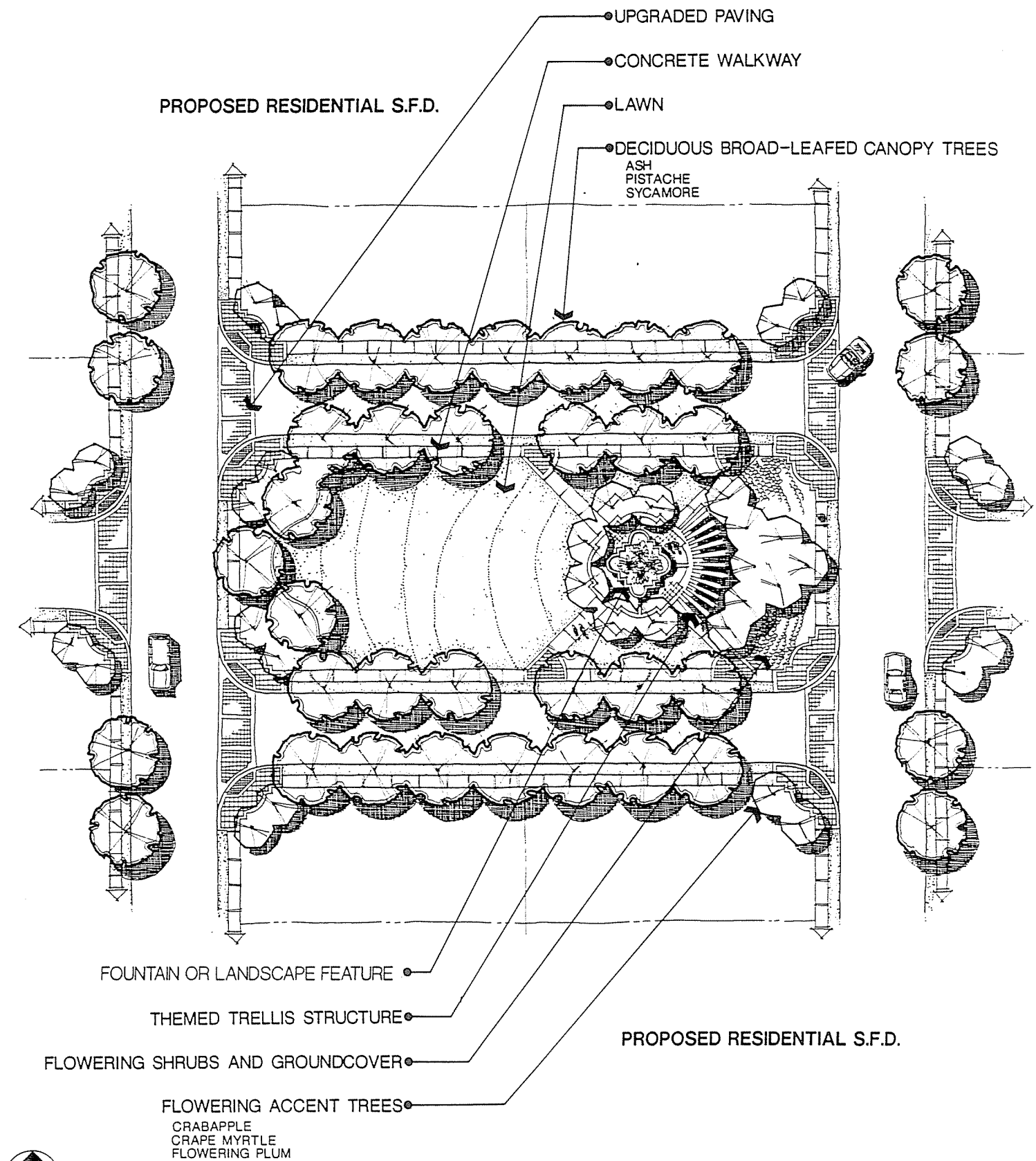
In keeping with the concept of establishing a focal monument at the intersections of major radial streets, the "Vista" park (Exhibit 36) capitalizes on the higher elevation of this location and the dramatic views it affords across the South Bay. The mini-park combines a trellis nestled among theme trees, a traditional style fountain or planter, decorative low walls, benches, upgraded paving, and flowering accent



## "M" POCKET PARK

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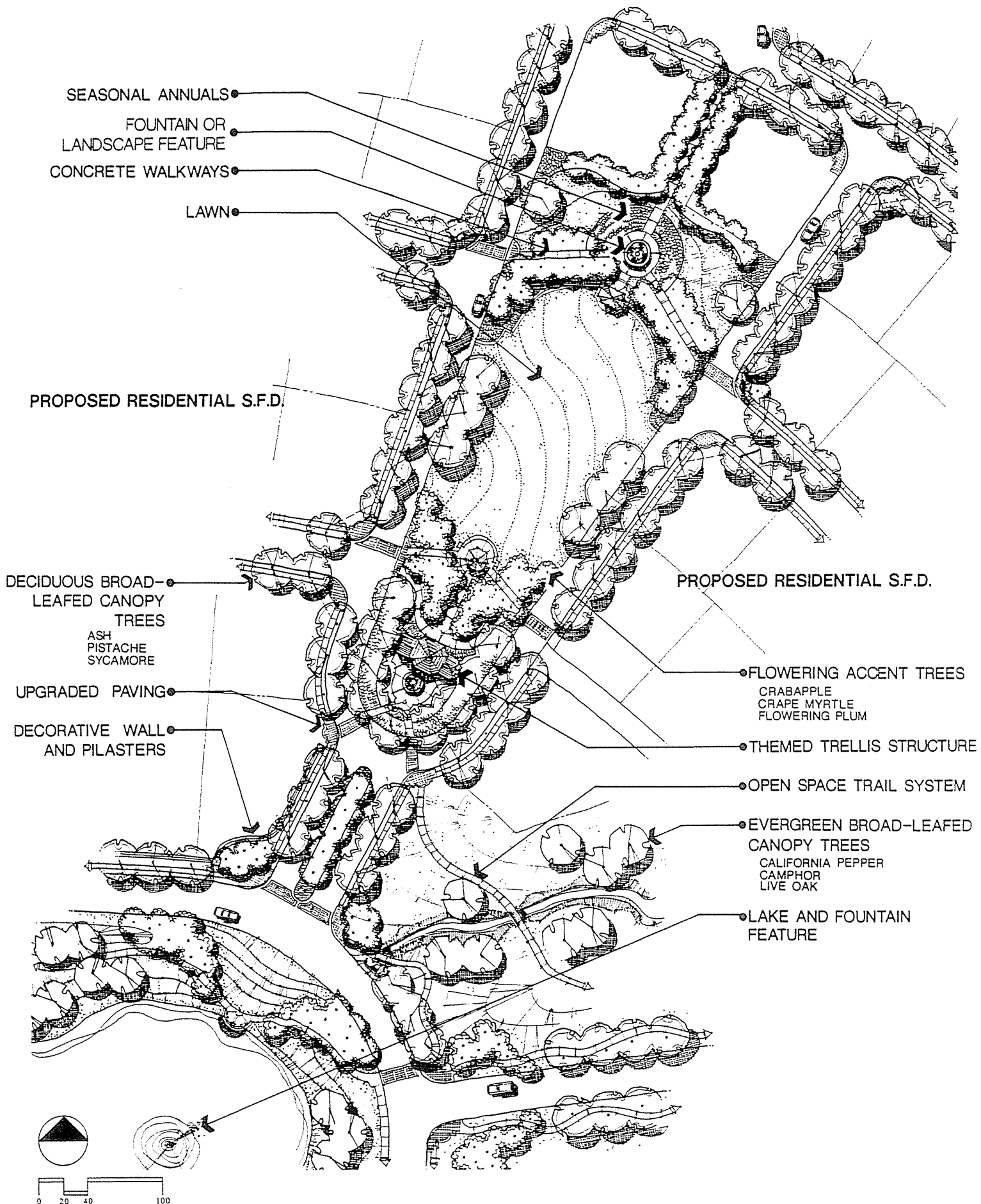


## "N" POCKET PARK

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EXHIBIT

34



## "O" POCKET PARK

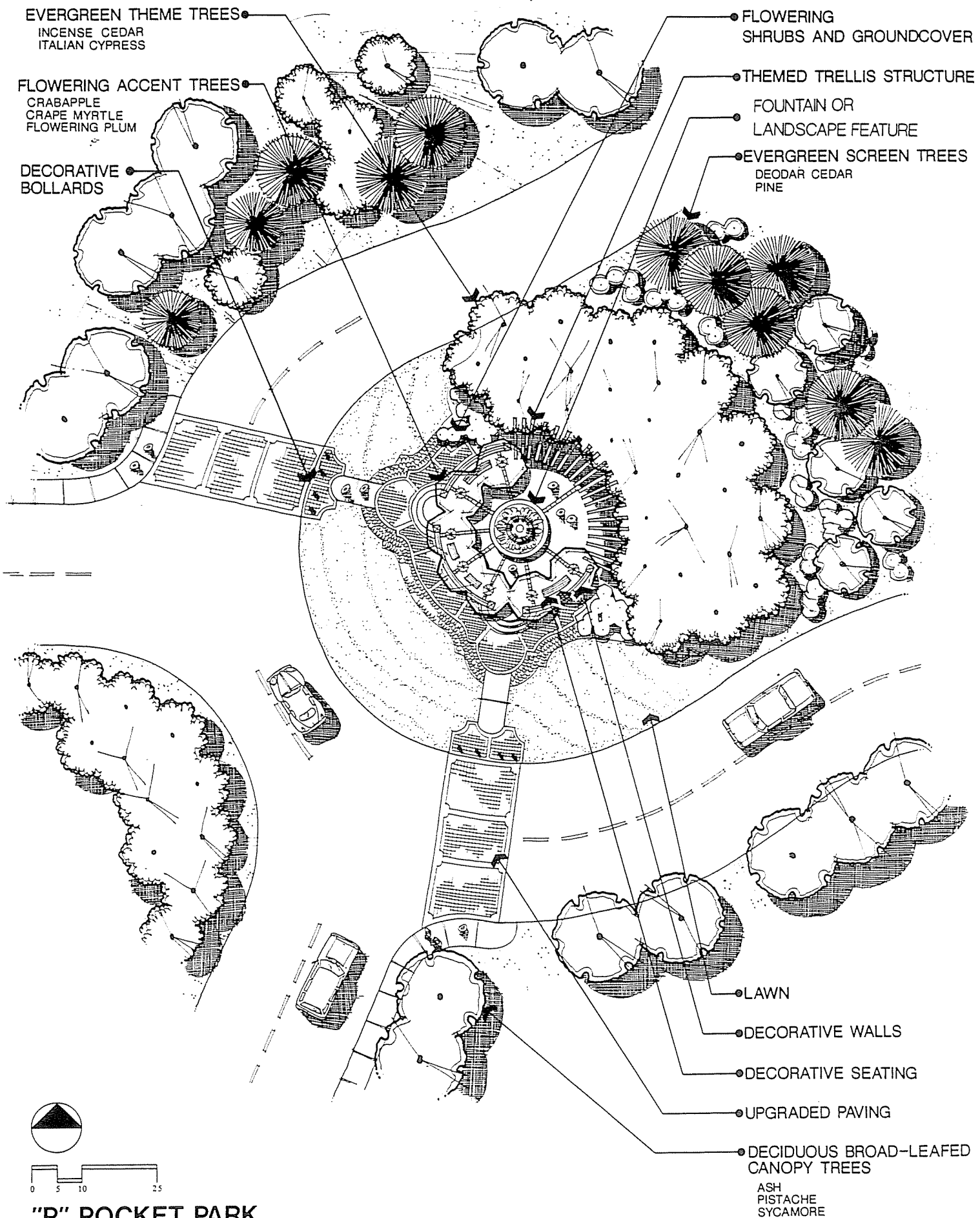
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EXHIBIT

35





trees to create an intimate setting from which pedestrians, cyclists, and joggers can enjoy the distant views.

### **Pocket Park "Q"**

As a prominent feature within the Evergreen Specific Plan, the Delta Road park (Exhibit 37) serves several recreational and landscape functions. The landscape within the park is intended to serve as a soft, gentle transition for the terminus of Delta Road as it flares to the north and south. Masses of flowering accent trees highlight a decorative fountain or planter and the pedestrian trail crossings at the street's end. Deciduous broad-leaf canopy trees accentuate the curvature of the streets and surround the open space area with shade.

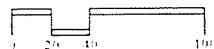
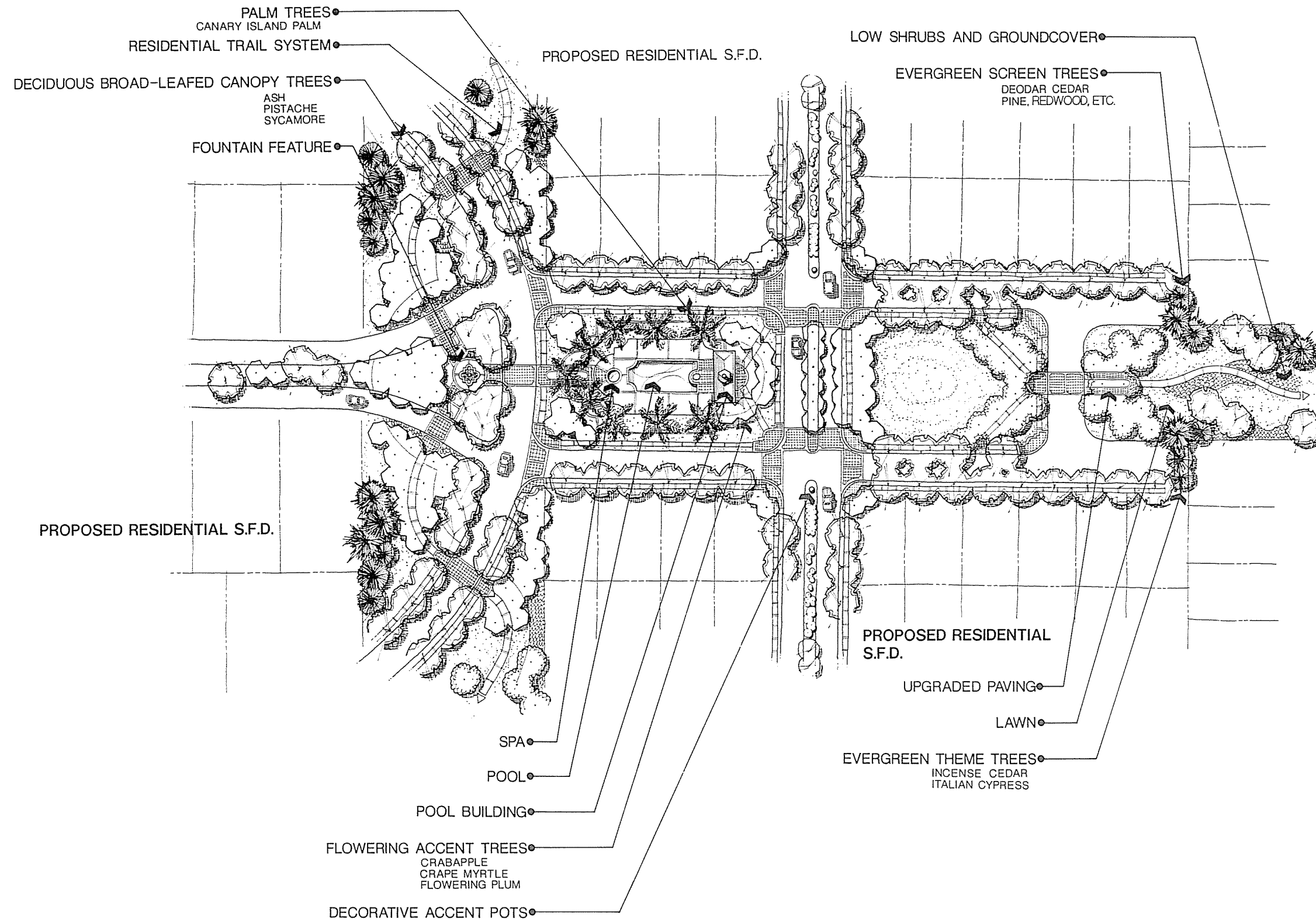
As a highly visible, integral feature of the adjacent development, a private recreation center complete with accent palm trees, swimming pool, and spa may be incorporated within a portion of the open space. The remainder of the park space has been devoted to providing trees and generous areas of turf for both active and passive use, in addition to providing a direct connection to the residential trail systems to the north, south, and east. As in all other significant spaces within the Evergreen Specific Plan, the consistent use of upgraded paving at crosswalks, decorative bollards, and colorful themed plant materials adds to the sense of community and intimacy at the pedestrian level.

### **Street Feature "R"**

While primarily serving a basic traffic feature, the mid-street cul-de-sac (Exhibit 38) provides an excellent opportunity to create a distinctive residential focal point. A central planter surrounded by upgraded paving within the street, sidewalk, and trail heads create a micro "destination" point for vehicles and pedestrians alike. The centerpiece of the cul-de-sac is accentuated with a multi-trunked specimen tree placed within a decorative raised planter. Themed bollards and flowering accent trees at the trail heads complete the detail reflected at other significant locations with the Specific Plan area.

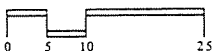
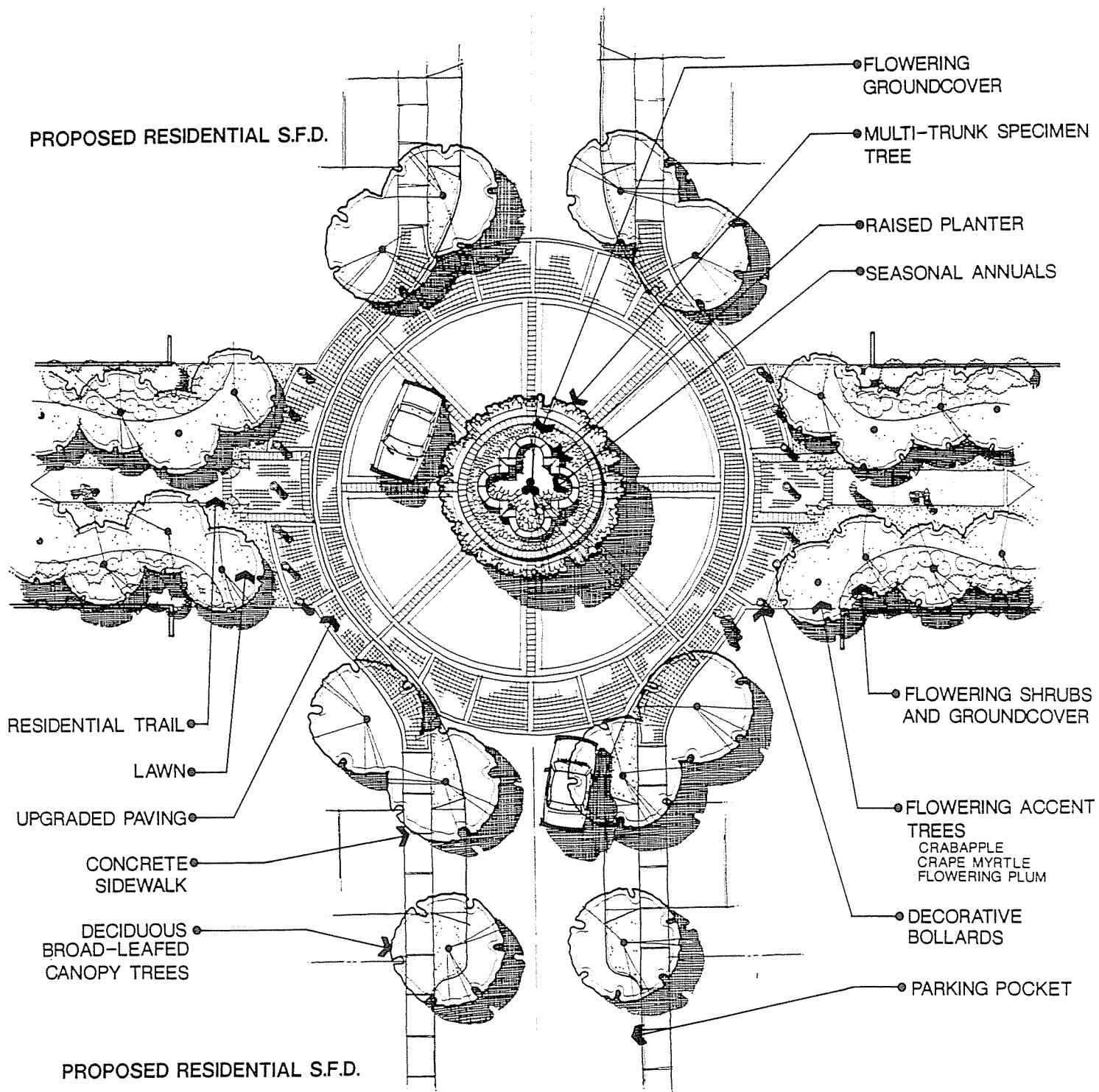
## **TRAILS AND CREEK CORRIDORS**

Another aspect to the provision of community amenities within the Evergreen Specific Plan area is the development of a pedestrian/bicycle trail system. The community should have the ability to move through the Specific Plan area and not be forced to use a car. The Specific Plan has incorporated a system of trails throughout the project site which provides connections to the schools and parks through the project area without forcing children to cross a number of streets.



## "Q" POCKET PARK

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EVERGREEN SPECIFIC PLAN



## "R" TRAIL AT STREET FEATURE

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### **Internal Trail Systems**

The Evergreen Specific Plan has provided a series of internal trail systems (Exhibit 39) separated from the street system, which allows movement throughout the community without having to cross major streets. As a link throughout the community, the internal trail system is intended to be more of a pedestrian corridor than recreational open space. As such, the landscape concepts for the trail system are based on issues such as safety, water conservation, and ease of maintenance.

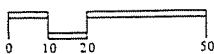
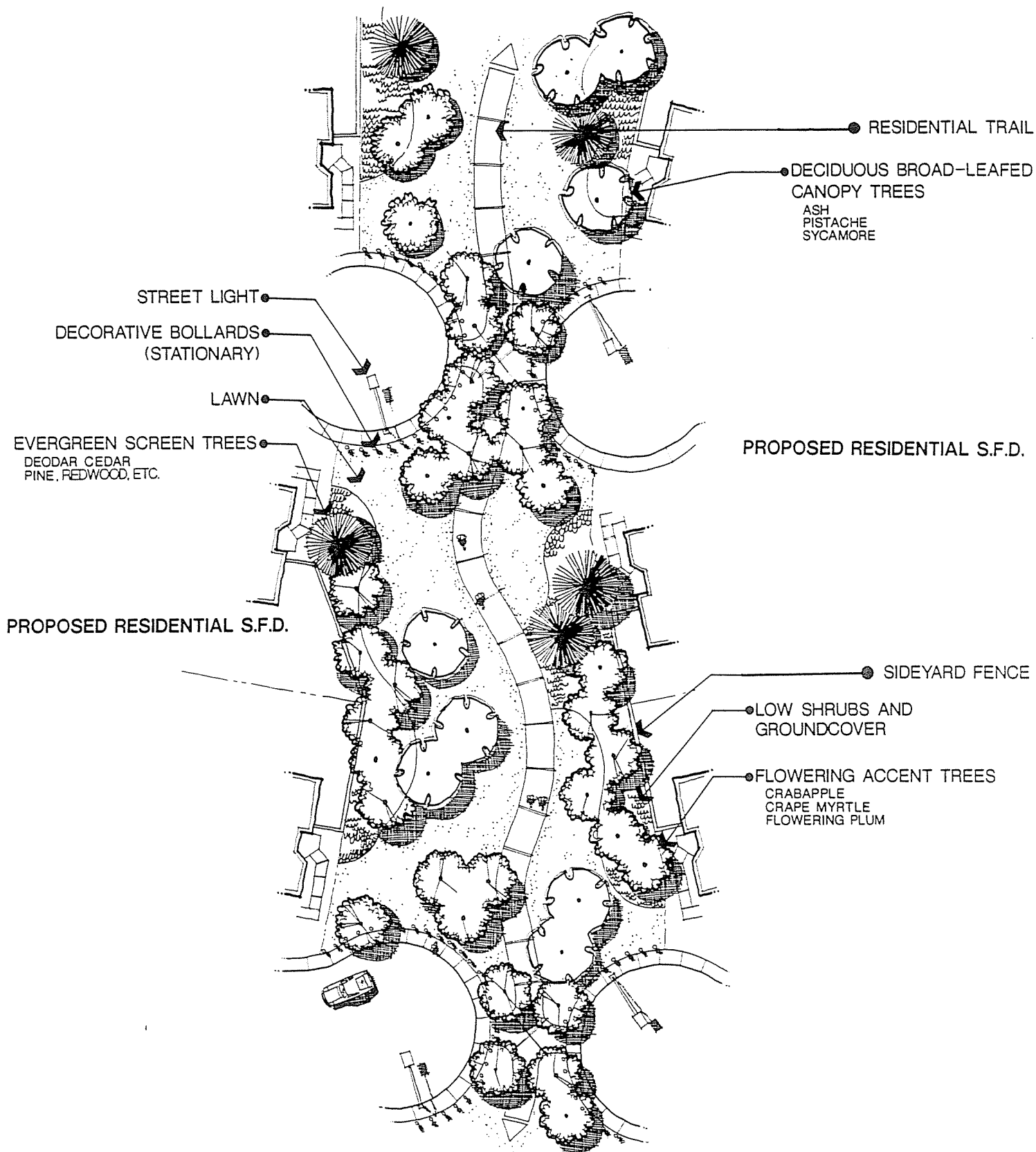
As a deterrent to crime, landscape plantings will be designed in such a way as to minimize or eliminate potential hiding places among trees, shrubs, and fences. Thorny "anti-personnel" shrub plantings can be introduced in and around the fences to discourage people from hiding in the vegetation. Tree species can also be selected for reduced size and increased openness. The heads of the cul-de-sac should be extended into the open space trails to allow more visibility into the trail areas. While lawn areas comprise a significant portion of the trail system, water conservation and landscape maintenance issues can be addressed through the use of drip and low precipitation rate irrigation systems in conjunction with a planting palette comprised of attractive, low-maintenance, drought-tolerant species.

### **Creek Trail Systems**

The project site is crossed by both Quimby and Fowler Creeks, which will remain as natural creeks where there is existing riparian habitat (Exhibit 40). Parallel to the creeks will be a trail system which follows Quimby and Fowler Creeks up to the foothills. The trail system which parallels Fowler Creek terminates at Fowler Creek Park, with the option of a future extension to the foothills beyond. Aside from the introduction of a trail link and supplemental landscaping, few modifications are proposed within the existing creek corridors. The lower portions of the creek will require modification and enhancement to channelize run-off towards the proposed lake features. Landscape plantings shall be limited to trees and shrubs indigenous to riparian environments and those which can provide habitat and flourish with limited water resources. Site furniture and trail lighting would be inappropriate in this setting.

### **LAKES**

The aesthetic value of water features (Exhibit 41) are apparent to anyone who has had the opportunity to either live or work around them. However, the water features within the Evergreen Specific Plan serve a dual purpose. Due to the nature of the storm drainage system that serves the larger Evergreen area, the Specific Plan area must detain some degree of the storm water run-off during peak periods of rain fall. Two of the water features lie at the terminus of Quimby and Fowler Creeks. These features, although serving as year-round water features, also serve as storm water retention facilities in peak flows. A third feature lies within the multi-family residential project along Murillo Avenue. All of the lakes are surrounded in part by public open space and will have trail connections



## "S" TYPICAL TRAIL SECTION

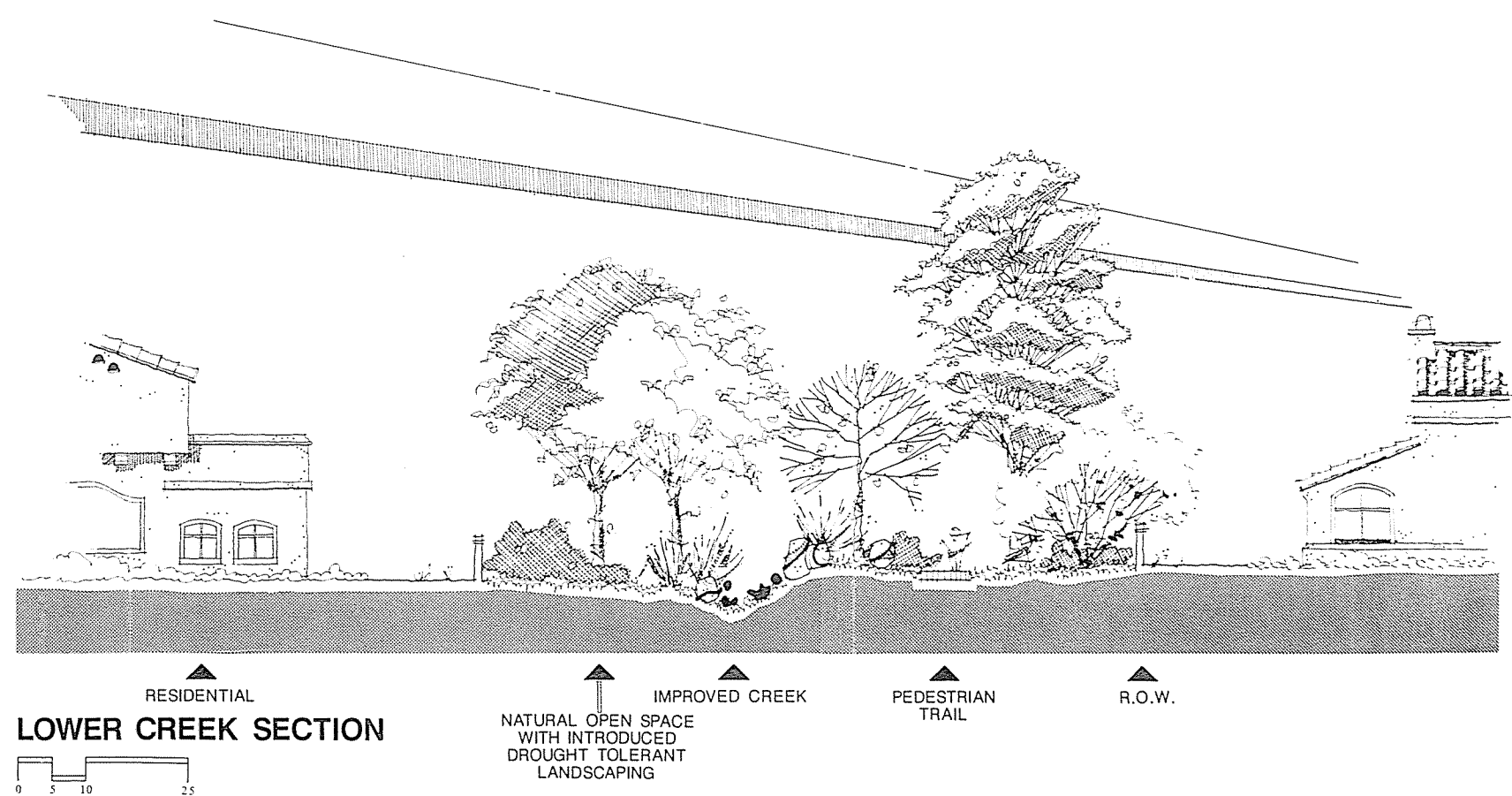
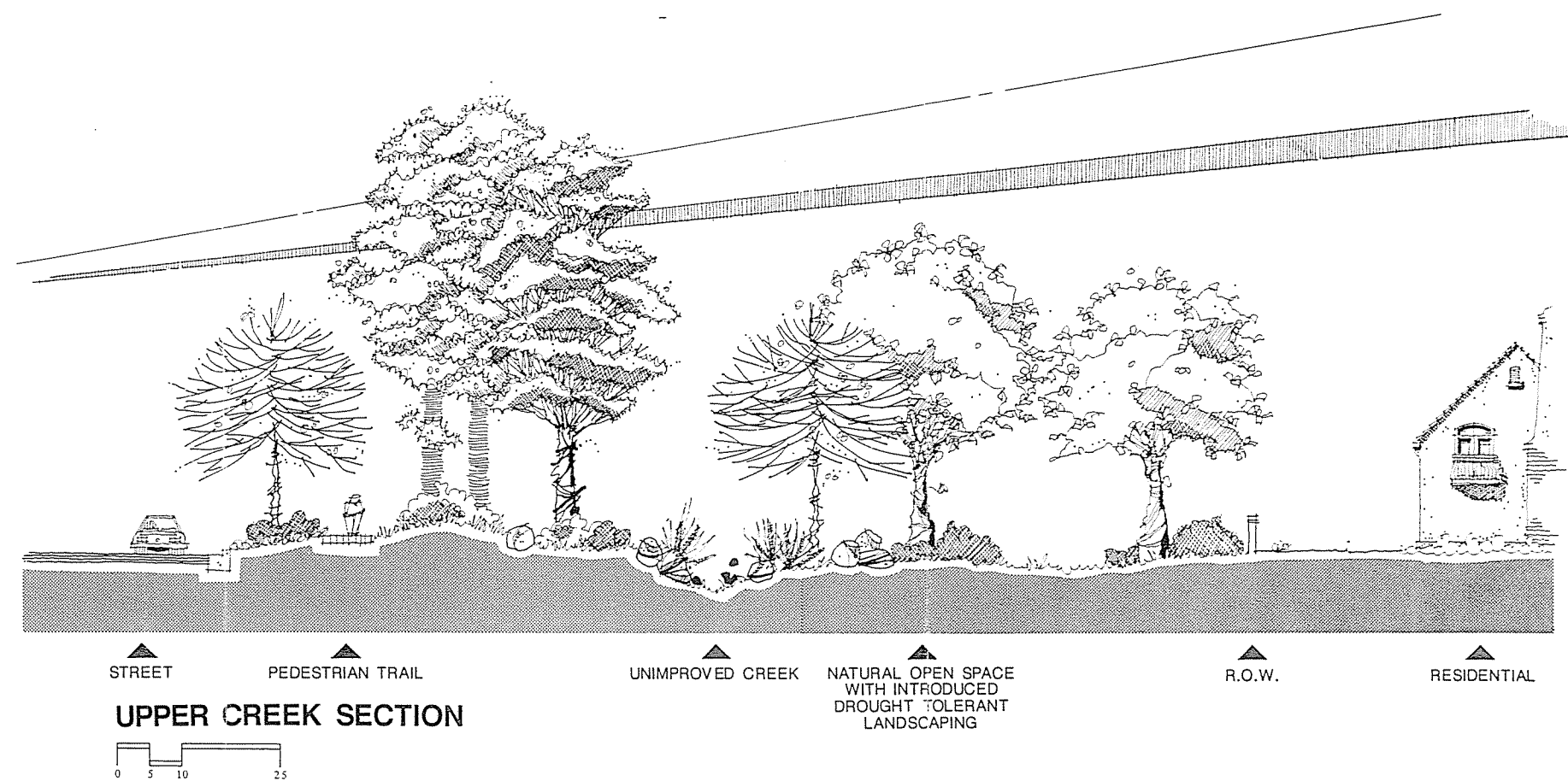
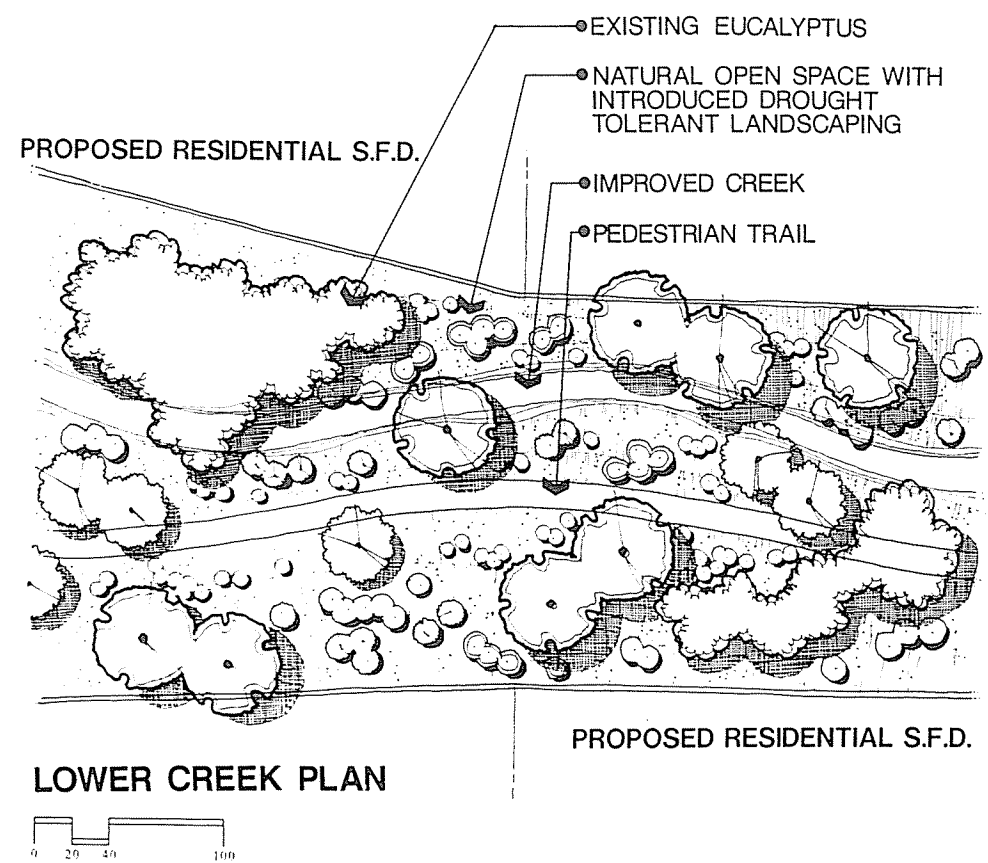
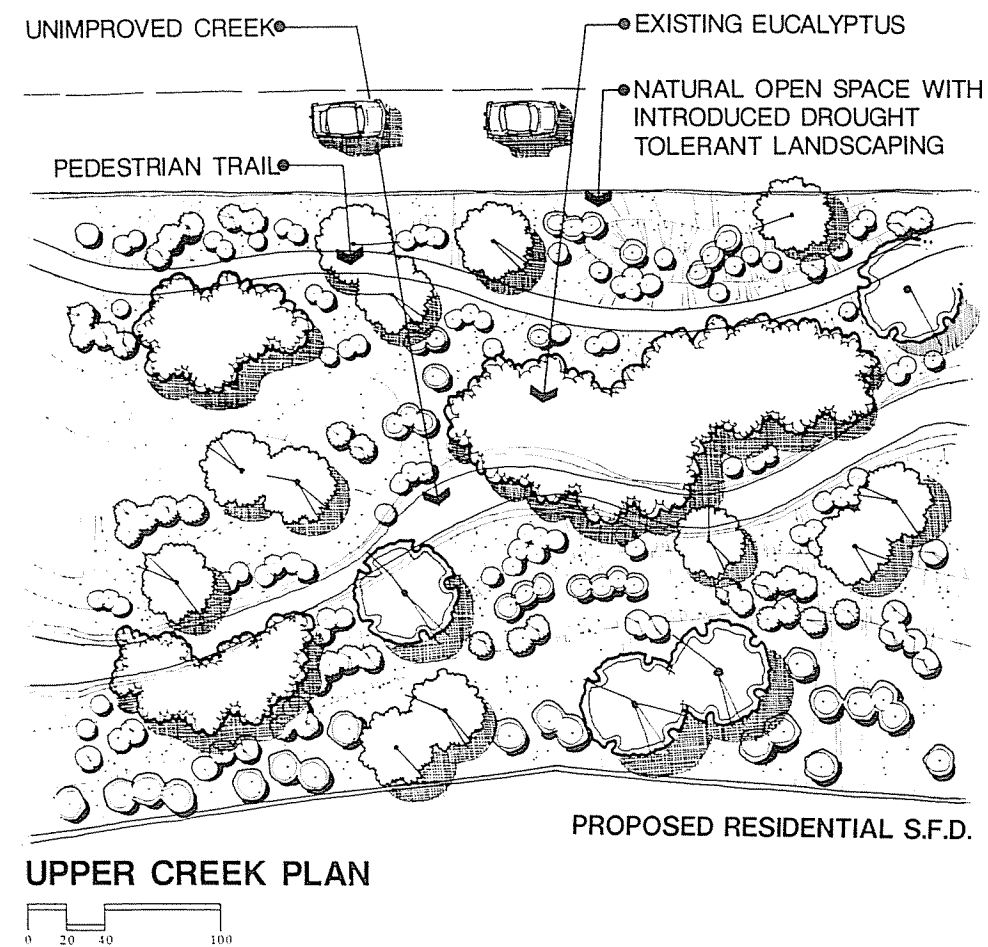
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EXHIBIT

39





## "T" TYPICAL CREEK CORRIDOR

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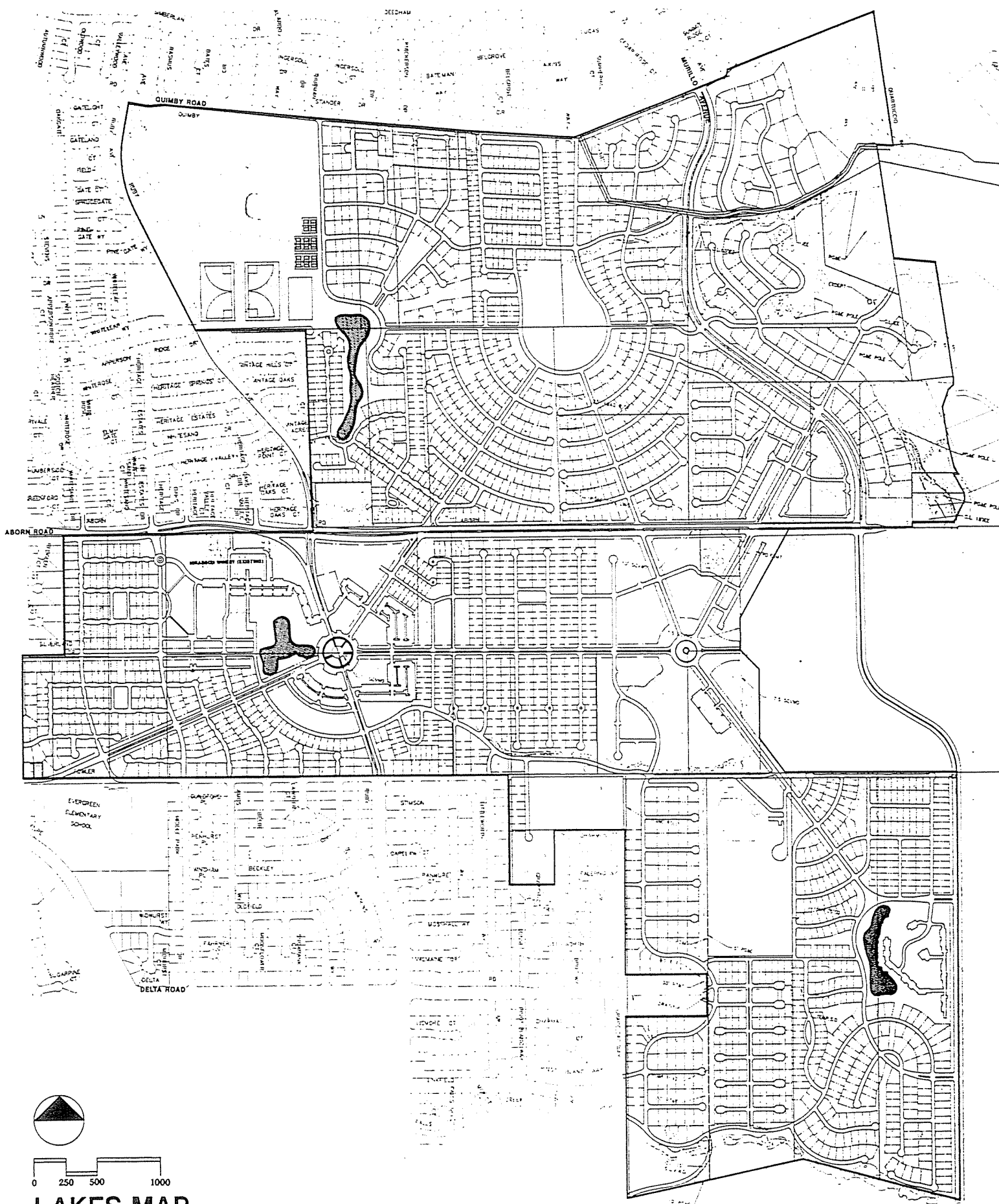
throughout the community and beyond to the Silver/Thompson Creek trail system to the west and the regional foothill trail systems to the east. Reclaimed water will supply the lakes and the bottoms will be lined to limit water loss (Exhibit 42).

The landscaping surrounding the lakes is intended to accentuate these open space areas visually as well as to maximize the recreational opportunities within the community. Along with small clusters of oaks and flowering accent trees, contoured turf areas dotted with water oriented trees (such as willows, alders, and sycamores) comprise the majority of the open space. Shrubs used in landscape buffer zones would be drought-tolerant species similar to those used along the residential trail system, while shrubs in the highly visible areas could include more ornamental species. For a "finished" appearance, and for erosion control purposes, the lake perimeter could be constructed with a concrete edge.

### **HERITAGE VINEYARD**

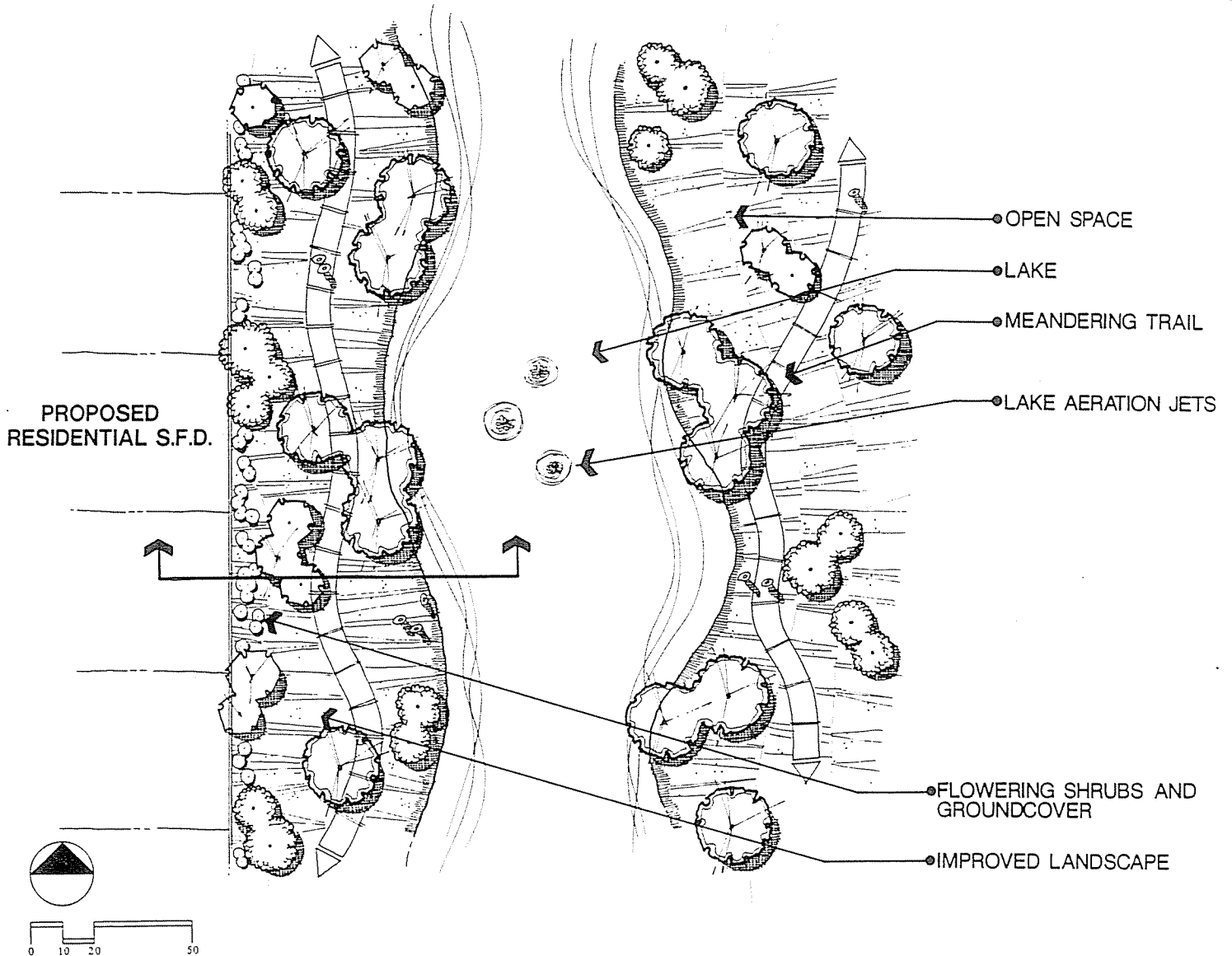
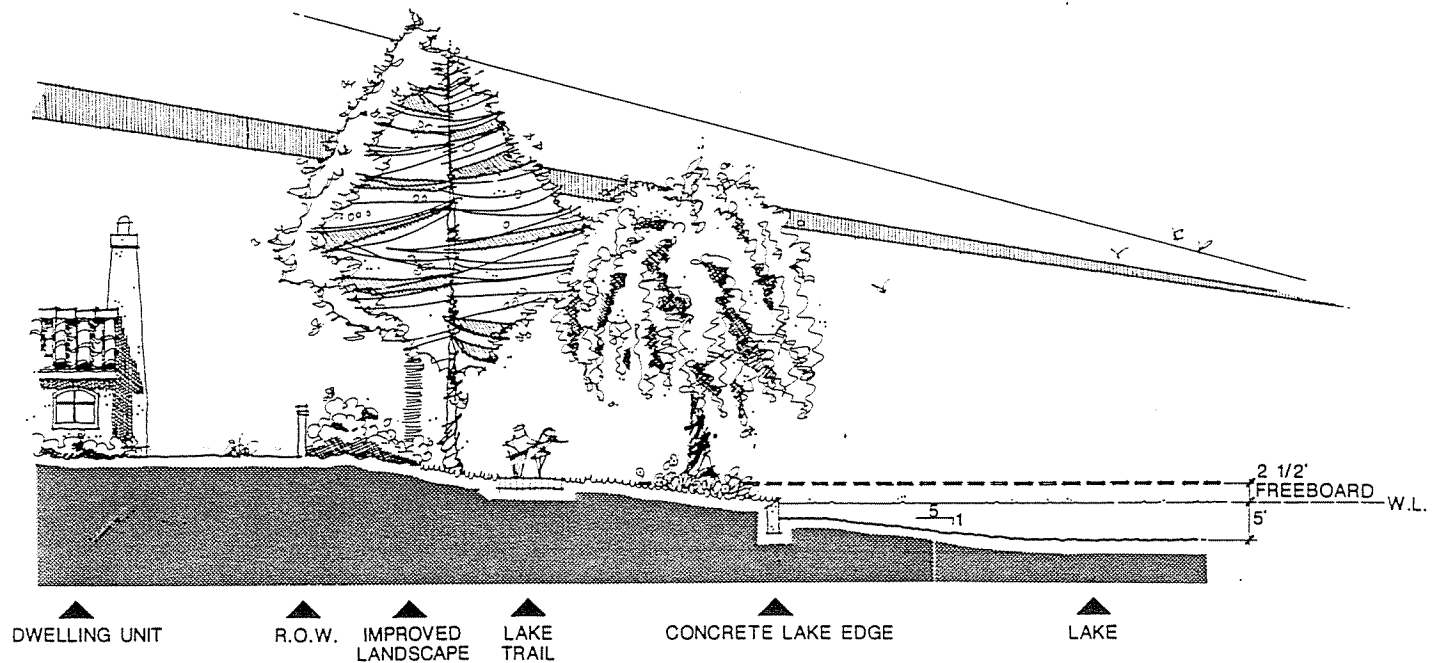
In an effort to preserve an important historic and visual feature, the Evergreen Specific Plan will acquire and maintain a portion of the Mirassou vineyards that lie along Aborn Road (Exhibit 43). The vineyards provide a tie to Evergreen's past and leads to the Village Center rotary (Exhibit 44). The streetscape improvements adjacent to Aborn Road and Ruby Avenue will incorporate a meandering sidewalk. Instead of turf, flowering groundcover will be used along the street to provide a more subtle foreground planting to the vineyards. Informal groupings of flowering accent trees are intended to highlight intersections and focal points.

oOo



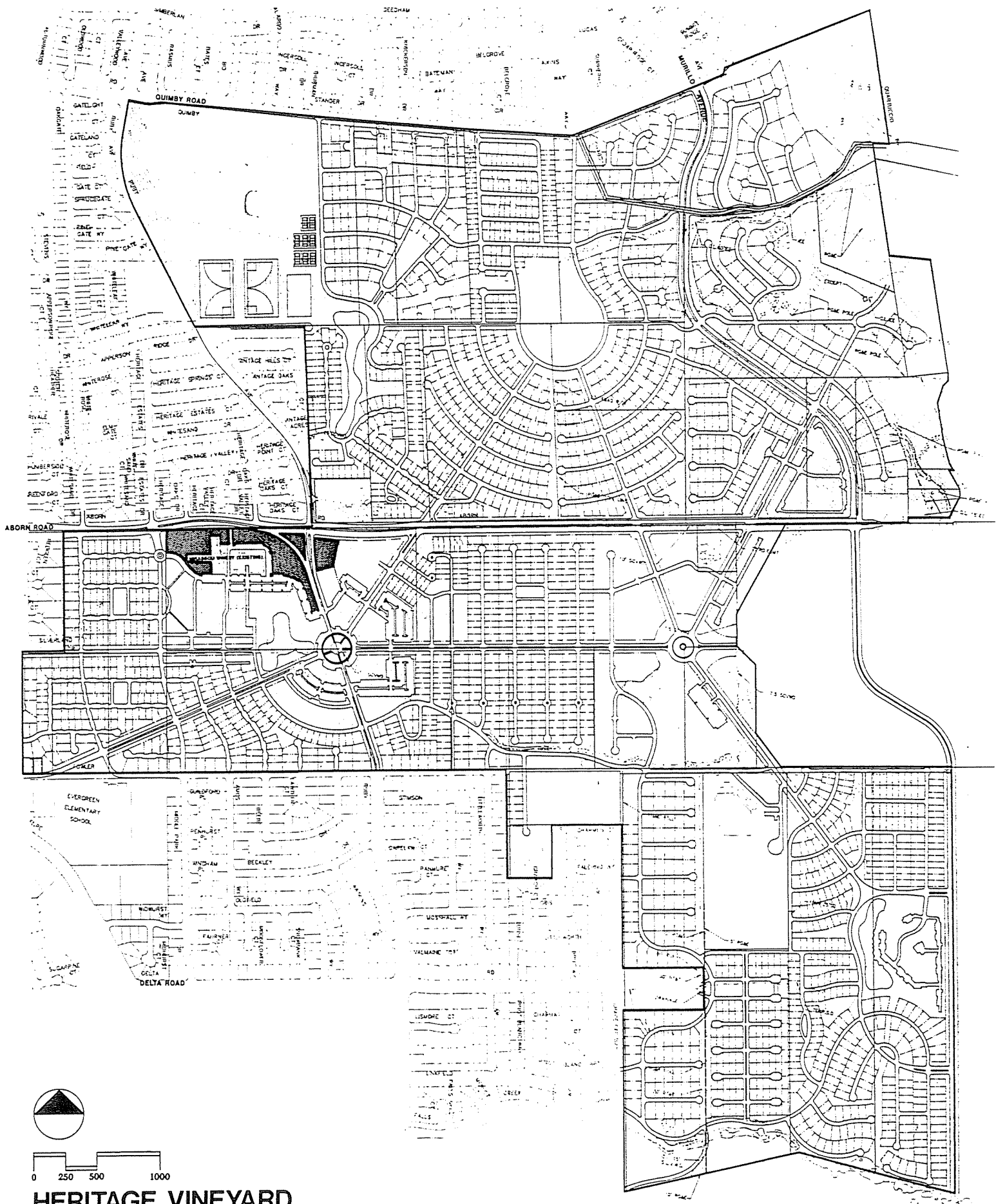
# **LAKES MAP**

**CITY OF SAN JOSE  
EVERGREEN SPECIFIC PLAN**



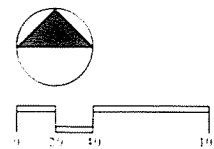
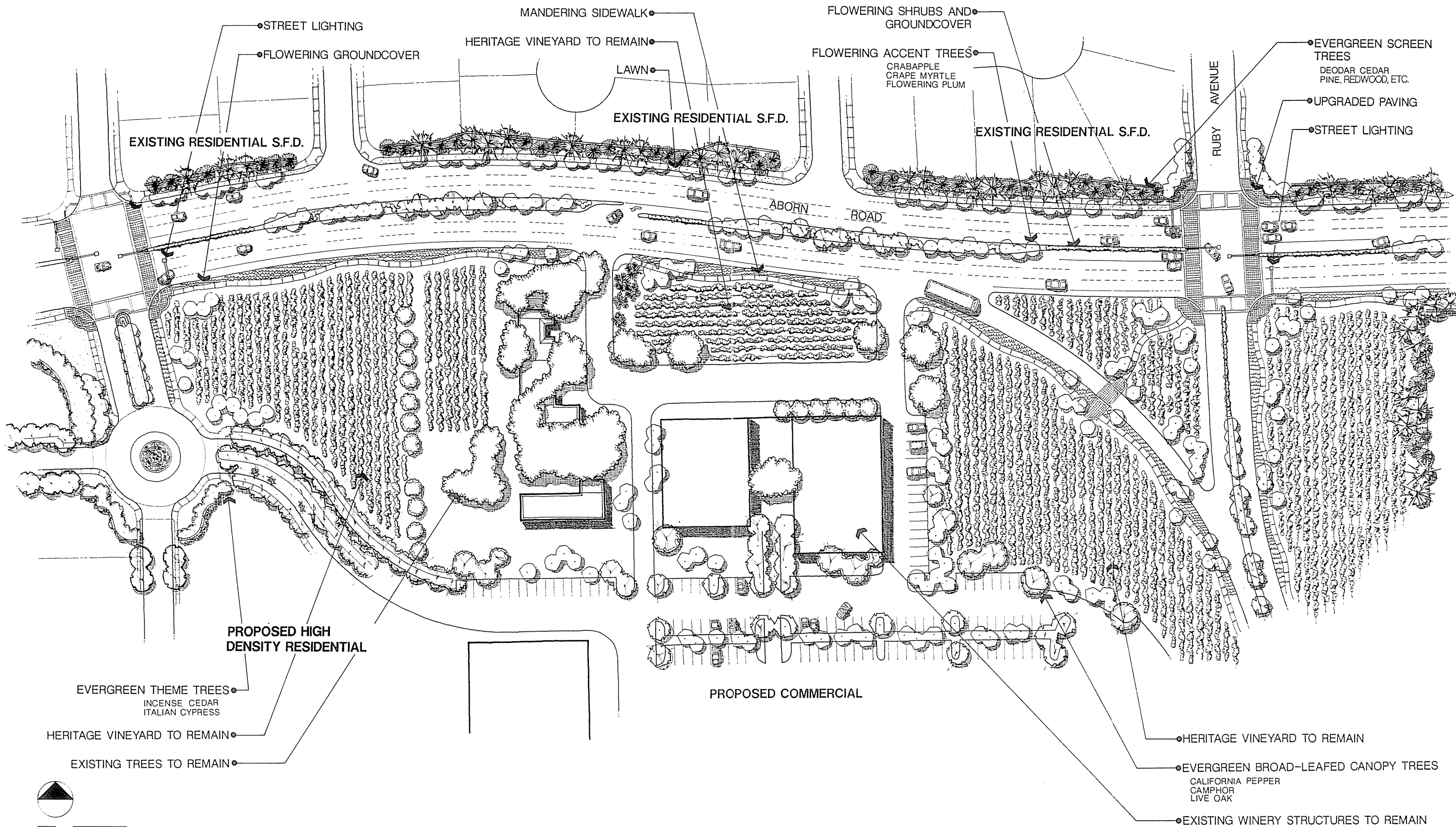
## LAKE PLAN AND SECTION

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# HERITAGE VINEYARD

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**HERITAGE VINEYARD PLAN**  
**CITY OF SAN JOSE**  
**EVERGREEN SPECIFIC PLAN**

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## Private Development



## Chapter 8 - Private Development

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**T**he Evergreen Specific Plan area is made up primarily of private development elements. These elements consist of the Village Center commercial area and the residential neighborhoods, which account for 80% of the project area.

For the area included in the Evergreen Specific Plan, the Planned Development Zoning is the major regulatory tool which works with and implements the Specific Plan. The Planned Development Zoning (PDCC 91-3-27) regulates uses and development standards in conformance with the Specific Plan and as provided by the City of San Jose Zoning Ordinance (Title 20).

The Specific Plan provides the overall policy framework, some of it very detailed, for development in the plan area. It establishes the form and character of future development through policies and illustrations which go beyond the typical scope of zoning but represent the thoughtful intent of the City of San Jose.

In addition to the Specific Plan and the PD Zoning, development in the plan area will also be subject to San Jose's Residential Design Guidelines and Commercial Design Guidelines for any elements which are not expressly addressed by the Specific Plan or the PD Zone. These three levels of control work as a package with the Specific Plan providing the intent, zoning the regulations, and design guidelines the development standards not addressed in the other two. Where the Specific Plan differs from City guidelines, the Specific Plan shall govern.

### EVERGREEN VILLAGE CENTER

#### Why A Village Center?

San Jose is the third largest city in California. It is a dynamic center of high technology with a vibrant economy and a revitalizing urban center. Many of its residential areas have grown during the postwar years of tract home sprawl, strip convenience centers, and regional malls. To a certain degree, much of this development lacks the focus and definition of the kind of neighborhood districts that comprise our best older cities. Beacon Hill in Boston, Nob Hill or Noe Valley in San Francisco, and Ballard in Seattle are all well known integrated neighborhood districts with small retail cores, a mix of housing, and their own civic buildings and identity. The Evergreen Specific Plan affords the opportunity to create this kind of neighborhood district. The Village Center is the retail, activity, circulation, and visual hub of this area.



## **Village Center Form and Character**

Keying off the Mirassou Winery and their heritage vineyards which form the Village Center's entry, the architecture is intended to establish the overall Evergreen identity of freely interpreted European and historical American architecture. The rotary and radial street pattern create a central village plaza to be encircled by shops. To assure that this form is reinforced, building fronts should conform to a curved or segmented circular facade set fifteen feet (15') back from the parking bay curb. Facades should divide vertically into plus or minus twenty-five foot ( $\pm 25'$ ) bays extending through the two- or three-story rotary-fronting facade.

Limited time parking is available at storefronts on the rotary while the bulk of the parking (unrestricted) is behind the buildings. Store frontage will help to animate "street life" by creating outdoor sales and displays, sidewalk cafes, and playful and effective, as well as tasteful, signage. Cart vendors, street trees, public seating, a transit stop, and events in the central plaza can all add to the Village Center's vibrancy.

## **Uses**

The Specific Plan's goal for the Village Center is that it house a variety of locally-serving and specialty-draw users. Theater, health club, salon, restaurants and cafes, shops, a small market, and small community-serving professional offices are all considered appropriate.

## **Shared Parking**

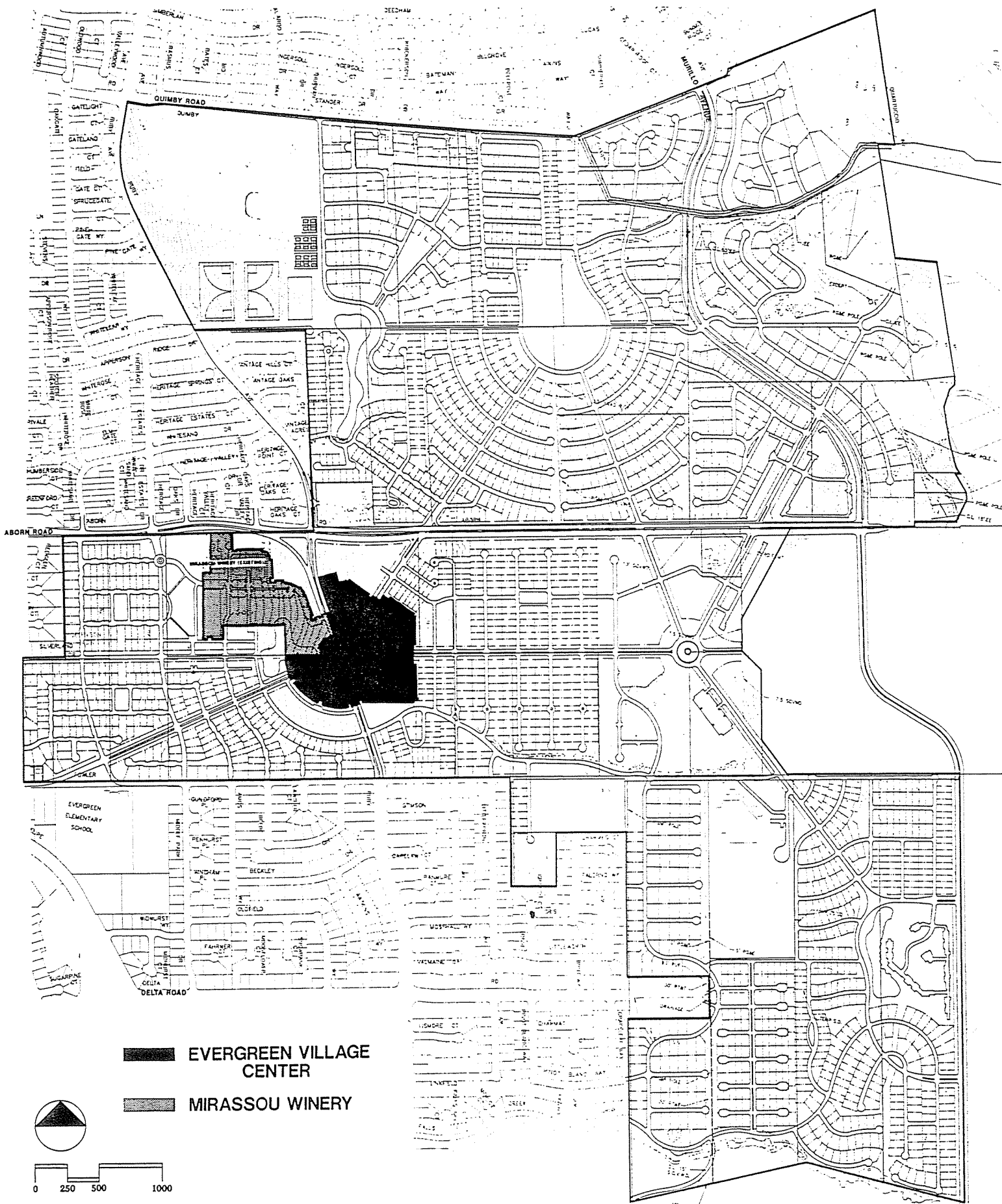
Where particular uses have predictable time cycle parking demands, shared parking is endorsed by this Specific Plan as a way to reduce total parking lot area. The Village Center Plan (Exhibit 45) and Village Center Design Concepts (Exhibits 46a, 46b, and 47) represent an example only. More detailed analysis will ultimately produce a final plan, which will be reviewed and approved through the Planned Development permit process.

## **Concept**

The heritage vineyard (Exhibit 44) and local fire station (Exhibit 16) establish a gateway to the Village Center.

The plaza-fronting side of the market displays wares in an open-air arcade, and the health club, related salon, barber, athletic equipment, and health food stores are presented as individual store fronts (Exhibit 47).

Twin towers, where Ruby Avenue exits the rotary to the south, "close" the circle and link the theater and restaurant to the east with a traditional vertically-divided, shop-over-storefront facade to the west (Exhibit 47).



## EVERGREEN VILLAGE CENTER

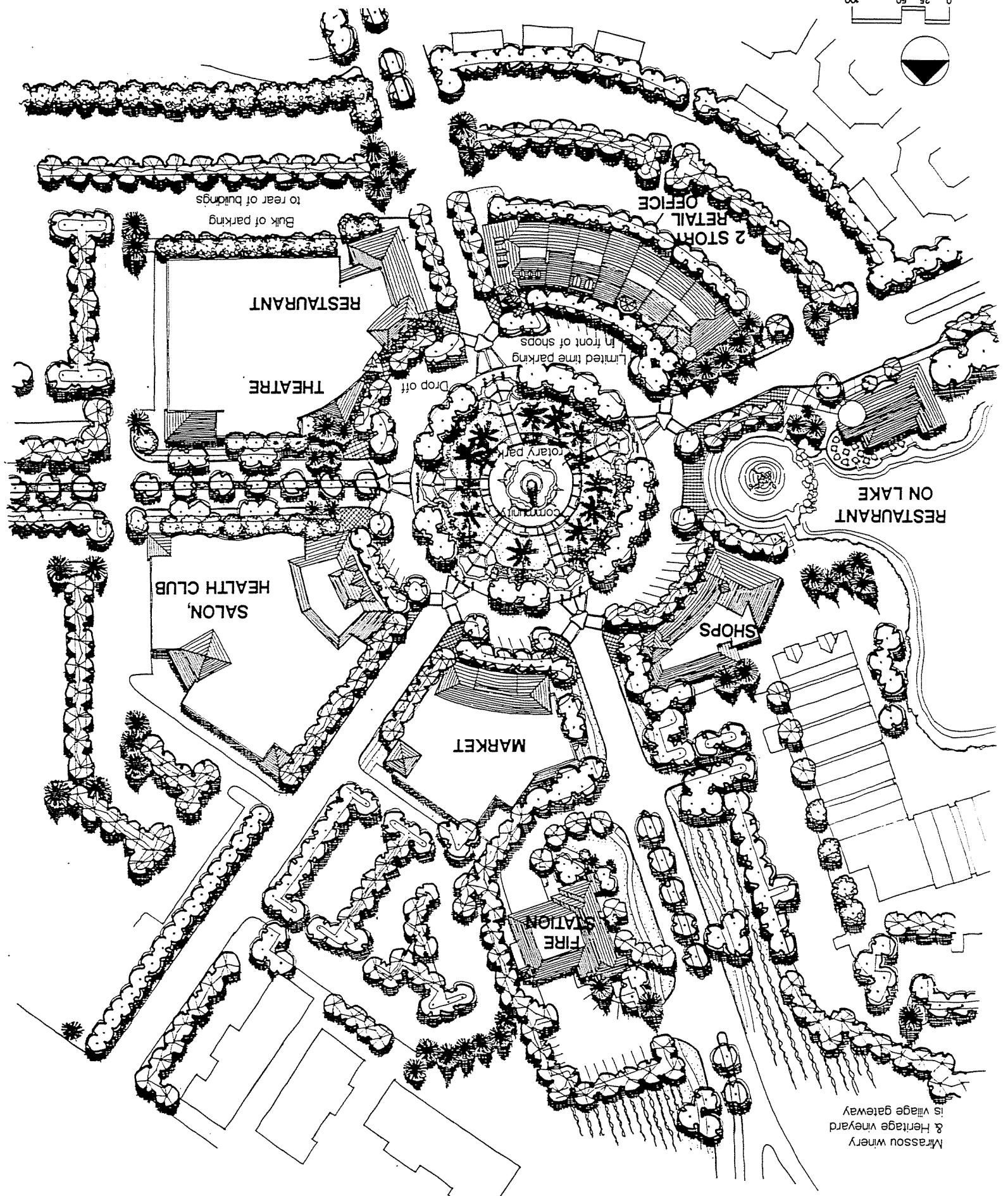
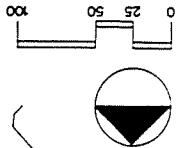
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EXHIBIT

45

VILLAGE CENTER CONCEPT PLAN



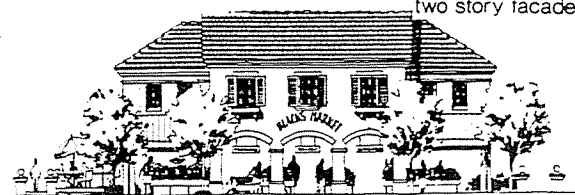
Mirassou winery  
& Heritage vineyard  
is Village gateway





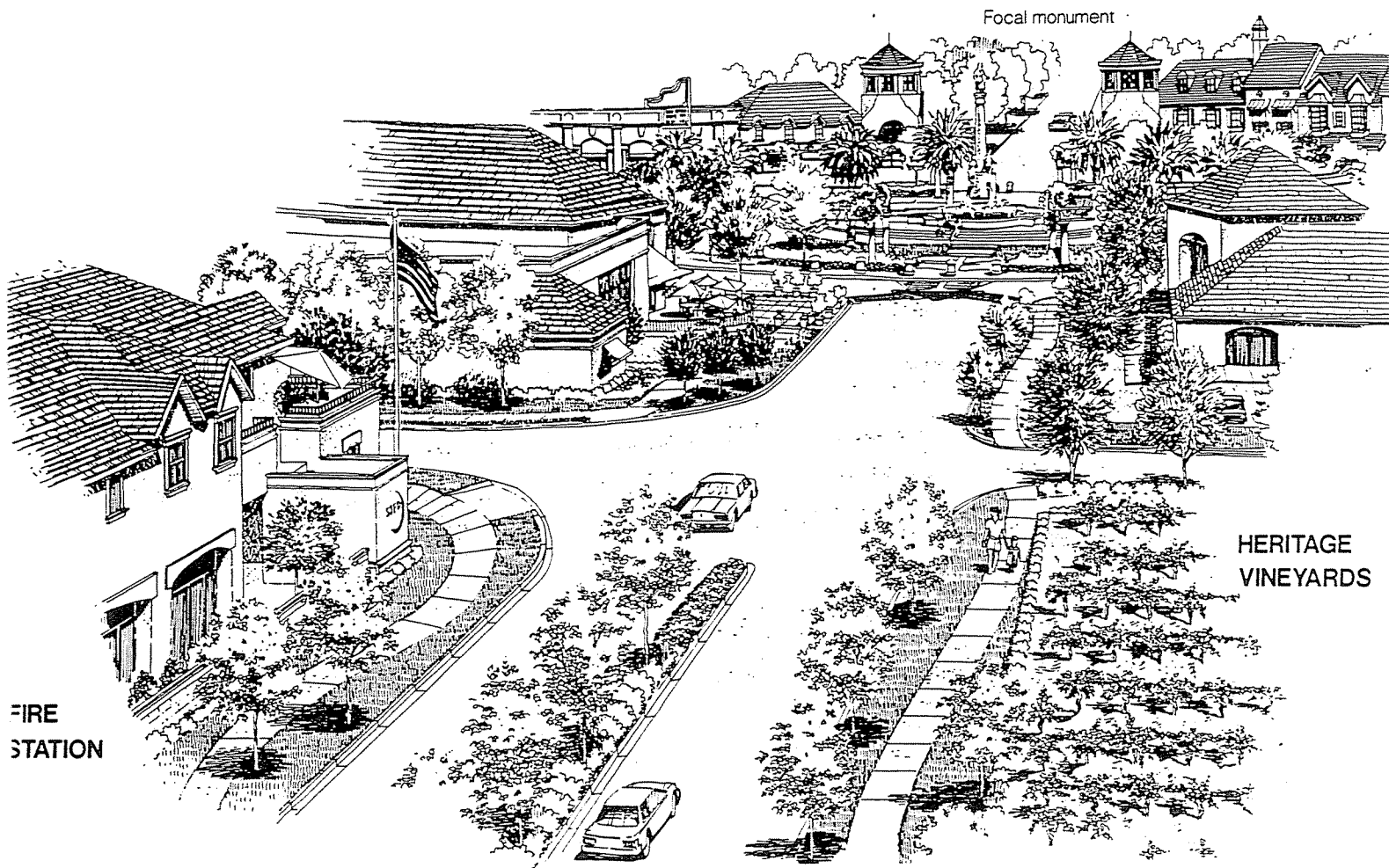
Offices, health care  
above

Health, beauty, grooming & sports equipment,  
small shops as part of Health Club Building



Market administration  
above to create  
two story facade

"Open air" front to village market



Focal monument

FIRE  
STATION

HERITAGE  
VINEYARDS



THEATRE MARQUE

Twin towers "close" circle



Vertical division of facade

## VILLAGE CENTER DESIGN CONCEPTS

CITY OF SAN JOSE  
EVERGREEN SPECIFIC PLAN

## **Development Criteria**

The Evergreen Village Center most closely resembles a "Sidewalk Strip" as described in the *Commercial Design Guidelines* published by the City of San Jose. Where this Specific Plan does not illustrate or describe particular development criteria, then the City guidelines shall govern the development. Where this Specific Plan differs from City Guidelines, the Specific Plan shall govern.

## **Permitted Uses**

- A. Movie theatres
- B. Financial institutions
- C. Business offices and agencies
- D. Catering
- E. Dry cleaning establishments as per Section 20.28.250(F) of Title 20.
- F. Dressmaking shops (no factories)
- G. Rental libraries
- H. Locksmiths
- I. News and magazine stands
- J. Personal services shops
- K. Professional office
- L. Tailor shop
- M. Studios except those listed under Subsection B of Section 20.28.260 of Title 20.
- N. Public eating establishments as per Section 20.28.260(Q) of Title 20.
- O. Museums, libraries, or community centers publicly operated.
- P. Outdoor sale of Halloween pumpkins between October 1 and November 5 inclusive, and Christmas trees between November 15 and December 30 inclusive.

- Q. Short-term uses with an Administrative Permit in accordance with the provisions of Subsection A of Section 20.08.910 of Title 20.
- R. Collectibles shops
- S. Gymnasiums and health clubs
- T. Laundromats
- U. Bars
- V. Any other retail businesses or retail commercial enterprises which are designed and planned to serve primarily the immediate neighborhood and which are not more detrimental to the welfare of the neighborhood than any use listed above, except (i) secondhand stores and (ii) any business or commercial enterprise which, in whole or in part, affords or provides active or passive entertainment on the premises for its business invitees or patrons, provided that upon issuance of and in compliance with a Planned Development Permit as provided in Part 4 of Chapter 20.44 of Title 20, active or passive entertainment on the premises for its business invitees or patrons may be provided as an incident for and in conjunction with (but not independently of) a business or commercial enterprise conducted under the provisions of the preceding subdivisions of this Section.

### **Special Uses**

Special uses within the Village Center commercial district shall be as set forth in Section 20.28.255 of Title 20, but are subject to the Planned Development permit process.

### **Conditional Uses (Subject to Special Considerations of Compatibility and Impacts in the Planned Development Process)**

- A. Studios whose visitors, customers, or business invitees paint, draw, sculpt or photograph persons provided as models by the studio or any other person.
- B. Medical clinics
- C. Night clubs
- D. Public utility facilities, excluding corporation yards, storage or repair yards and warehouses.
- E. Off-site and alternating use parking arrangements in accordance with the provisions of Chapter 20.12 of Title 20.

- F. Short-term uses in accordance with the provisions of Subsection B of 20.08.910 of Title 20.
- G. Bed and breakfast inns
- H. Amusement game devices, up to nineteen (19) per business establishment, in accordance with the provisions of Subsection B of Section 20.08.500 of Title 20.
- I. Live/work lofts
- J. Retail sales of alcoholic beverages for off-site consumption.

#### **Temporary and Interim Uses**

The following uses may be permitted within the Village Center as a temporary or interim use. These uses will require a Planned Development permit, which will establish the timeframe for these activities:

- A. Nursery
- B. Produce Market
- C. Auction Gallery
- D. Flea Market
- E. Craft and Art Fairs

Sidewalk vending is subject to Part 6 of Chapter 8, of Title 20.

#### **Sidewalk Cafes**

Sidewalk cafes are permitted in the public right-of-way in accordance with Section 20.52.450 of Title 20, Conditions and Restrictions, and subject to a Planned Development permit.



## **Development Regulations**

Minimum lot or parcel size:

6000 Sq. Ft. with on-site parking

**or**

1200 Sq. Ft. with an off-site parking agreement

Maximum number of stories: Three (3)

Height:

A minimum height of twenty feet (20') at rotary facade, a maximum height at radial street setback of twenty-five feet (25') thence increasing at a slope of 1:1 away from the street to a maximum overall height of forty-five feet (45') or the maximum height allowed by General Plan policy as illustrated in Exhibit 46b.

Setbacks:

Setbacks are measured from back of curb or property lines and illustrated in Exhibit 46b. Buildings must sit at least six inches (6") inside their property line.

## **Signs**

Specific Village Center signage design and individual enterprise signage and storefront criteria shall be part of the PD design submittal.

*Encouraged:* Raised "Halo" letters on building face, pedestrian oriented blade signs, sculptural cantilevered signs, non-internally lit signs with lighting from a secondary source, neon signs backed by building face or storefront.

*Discouraged:* Cabinet or "can" internally lit signs with the exception of theatre marquee, plastic-faced, internally-lit channel letters.

*Prohibited:* Free standing signs above six feet (6'), animated signs using moving parts, flashing or sequencing lights.

## **Architectural Character**

The architectural character of the Village Center shall conform to the Community Design Strategy of the Evergreen Specific Plan. Conforming architectural examples of this character appear in Exhibit 47 of the Evergreen Specific Plan.

Key to establishing this character is the treatment of the rotary facades. To reinforce the circular rotary and create a contained space with the surrounding buildings, these facades must follow a strict criteria.

Rotary facades shall be built within  $\pm$  two feet (2') of a property line set fifteen feet (15') behind the back of curb.

Rotary facades shall either be curved to follow this property line or consist of segmented radial chords of no more than thirty feet (30').

Rotary facades shall be vertically divided reflecting a traditional town plat of individual commercial lots.

Rotary facades shall have a minimum wall plate height of twenty feet (20').

Awnings, arcades, trellises, and other pedestrian protection may project ten (10) feet beyond the property line. These shall not interrupt the vertical facade divisions.

### **Off-Street Parking**

The minimum number of off-street parking spaces required for uses shall be provided as per Section 20.28.380 of Title 20 and Chapter 20.12. Shared parking in accordance with Section 20.12.100 of Title 20 is encouraged to reduce the total parking area required at the Village Center.

### **Hours of Operation**

No retail commercial establishment shall be open between the hours of 12:00 midnight and 6:00 a.m. in the Village Center except with a Planned Development permit expressly permitting such hours of operation.

### **Mirassou Winery**

The existing Mirassou Winery operations include limited growing, processing, warehousing, and visitor facilities. This General Development Plan and the Evergreen Planned Residential Community Specific Plan anticipate the continuation of these operations as well as the inclusion of retail activity at the Village Center and the expansion of visitor facilities to house other craft and traditional small processing industries. These facilities may include live/work lofts.

Where a change in existing use is deemed to constitute an intensification of use, additional focused environmental impact studies and mitigation measures may be required.

## **RESIDENTIAL DEVELOPMENT**

### **MULTI-FAMILY RESIDENTIAL**

#### **Concept**

Multi-family designations in the Evergreen Specific Plan serve to add variety to the overall housing mix and provides for more affordable and household make-up alternatives to the dominant traditional detached family houses. These areas are sited either in close proximity to the Village Center or campus industrial employment.

Planning Area "A" (Mirassou) is adjacent to a winery and warehousing operation and buffers these uses from single family. Planning Area "B" fronts the realigned Fowler Creek and serves as a buffer between single family and the Village Center. Planning Area "C" is close to campus industrial and serves as a focal terminus to a southeast radial extending from Fowler Creek Park. This site is also integrated with the southernmost lake/retention area (Exhibit 48).

#### **Permitted Uses**

Primary use:

- A. Attached multiple family dwellings.
- B. Public utility facilities primarily to serve the Specific Plan area residents.

Secondary uses:

- A. Accessory buildings as provided for in Section 20.24.260(6) of Title 20.
- B. Private recreational, meeting, and maintenance facilities
- C. Day care facilities

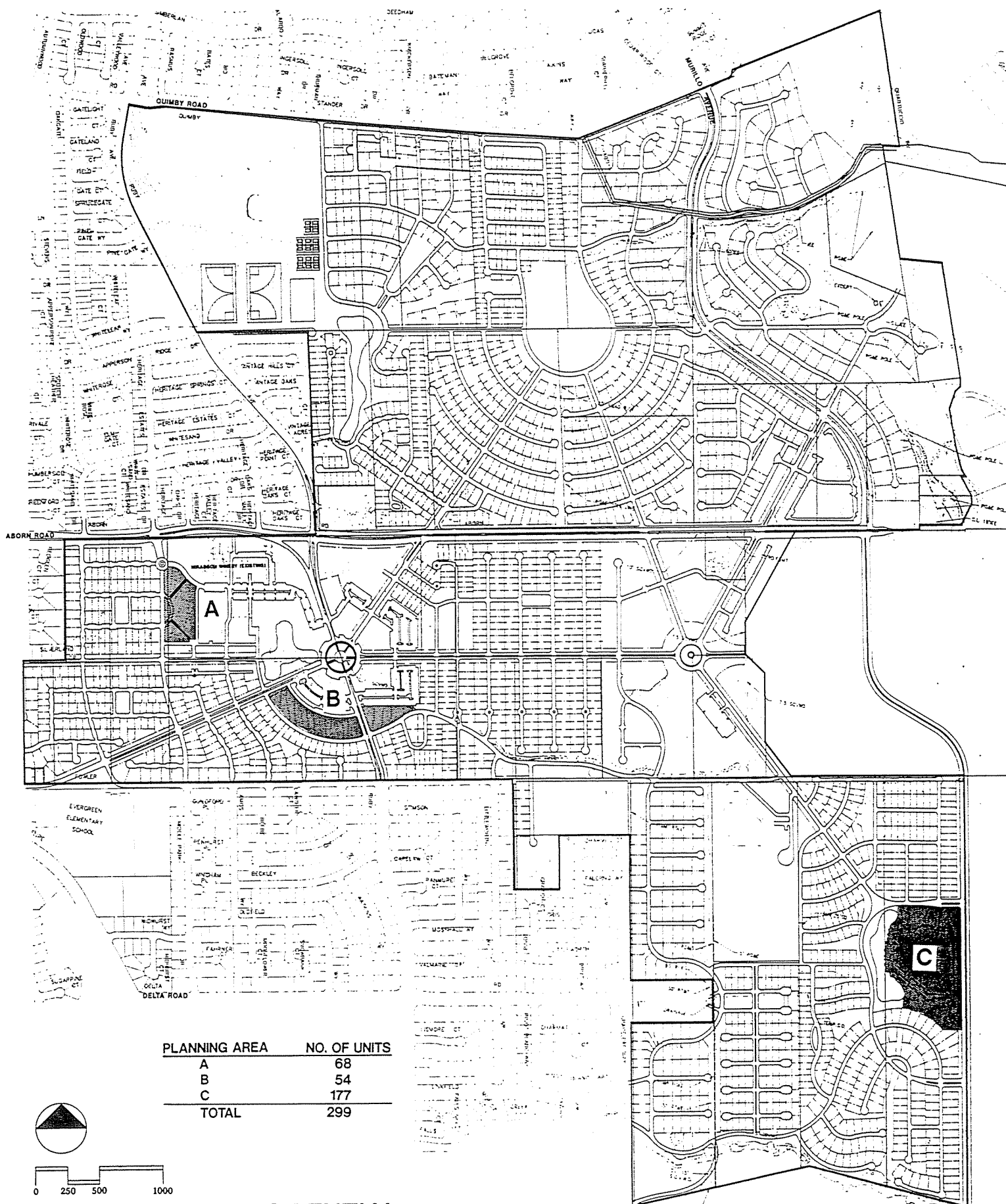
#### **Development Standards**

Building Height Plane:

Within one-hundred feet (100') of property boundaries adjacent to single family: Thirty feet (30')

General: Forty-five feet (45')

Building elements up to five percent of the building footprint maybe as high as permitted by the General Plan.



**HIGH DENSITY RESIDENTIAL** (12-25 DWELLING UNITS/ACRE)

**CITY OF SAN JOSE**  
**EVERGREEN SPECIFIC PLAN**

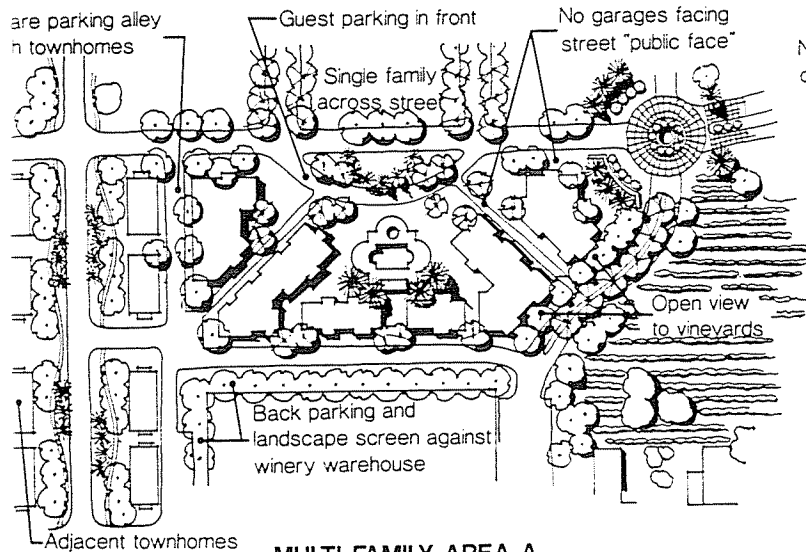


### OPEN PUBLIC FACE

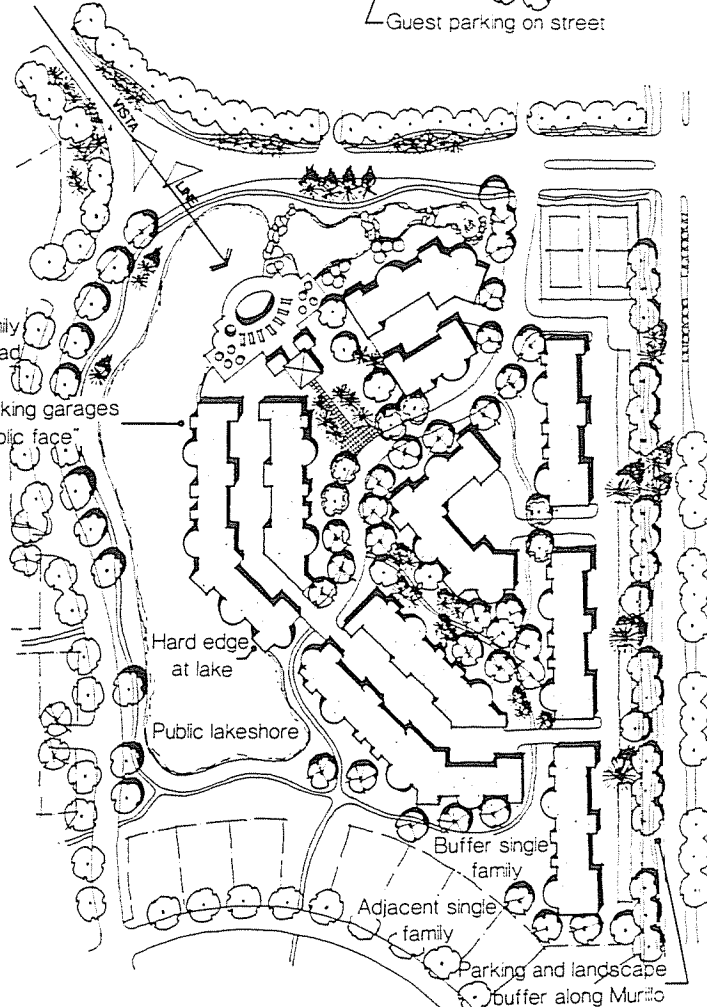
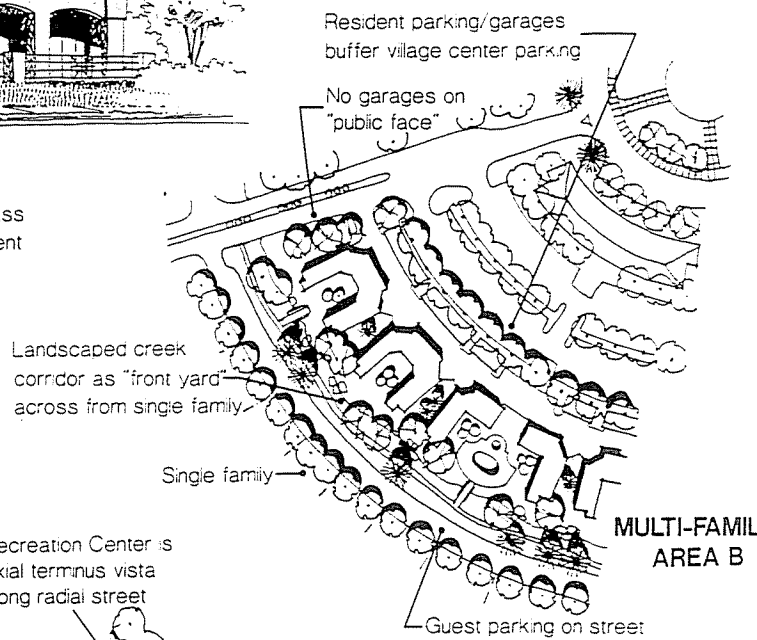
Without parking, building form, mass and streetside landscaping present a good neighbor "public face"



### BUILD OVER COURTYARD ALLEY GARAGES



MULTI-FAMILY AREA A



MULTI-FAMILY AREA C

## HIGH DENSITY RESIDENTIAL (12-25 DWELLING UNITS/ACRE) DEVELOPMENT CRITERIA

Project design and development shall conform to *Residential Design Guidelines for the City of San Jose* for building types considered garden townhomes, entry court townhomes, cluster housing, or podium cluster housing (Part III, page 20 through 23) with the following exception:

1. Perimeter parking drives and carports.

In the residential guidelines for the City of San Jose these are discouraged because they are barriers and present a poor image (see page 22 of the *Residential Design Guidelines for the City of San Jose*). The use of perimeter drives and carports or garages at the perimeter where abutting an alley with other residential types, is suggested and encouraged, and may be permitted along other perimeters which may benefit from a barrier.

### **Signs**

Signs are allowed in accordance with Section 20.24.350 of Title 20.

### **Parking**

The minimum number of parking spaces shall be as set forth in Section 20.12.215 of Title 20. If adjacent on-street parking opportunities are plentiful, designated on-street parking spaces may be included in "Minimum number of off-street parking spaces required", up to .25 spaces per unit.

### **High Density Residential Landscaping**

Landscaping locations and objectives should conform to the *Residential Design Guidelines for the City of San Jose* and the *Landscape and Irrigation Guidelines for the City of San Jose*. Landscape character should complement the public area as well as the landscape plans included in the "Community Amenities" chapter of the Evergreen Specific Plan.

## TOWNHOUSES

### Concept

Townhomes are sited close to the Village Center, Fowler Creek Park, and the campus industrial area below or west of Murillo Avenue which serves as a buffer to larger lots in the hills to the east. These townhomes are intended to face the radials and neighborhood collectors providing a pedestrian-friendly "urban" scale to these streets as they move closer to the rotary centers.

### Permitted Uses

Primary use:

- A. Attached single family dwellings (i.e. rowhomes, townhomes, and entry-drive townhomes).
- B. Public utility facilities primarily to serve Specific Plan area residents.

Secondary uses:

- A. Accessory buildings as provided for in Section 20.24.260(6) of Title 20.
- B. Private recreation, meeting, and maintenance facilities.
- C. Day care facilities

### Development Standards

Building Height Plane: Thirty feet (30') .

### Setbacks

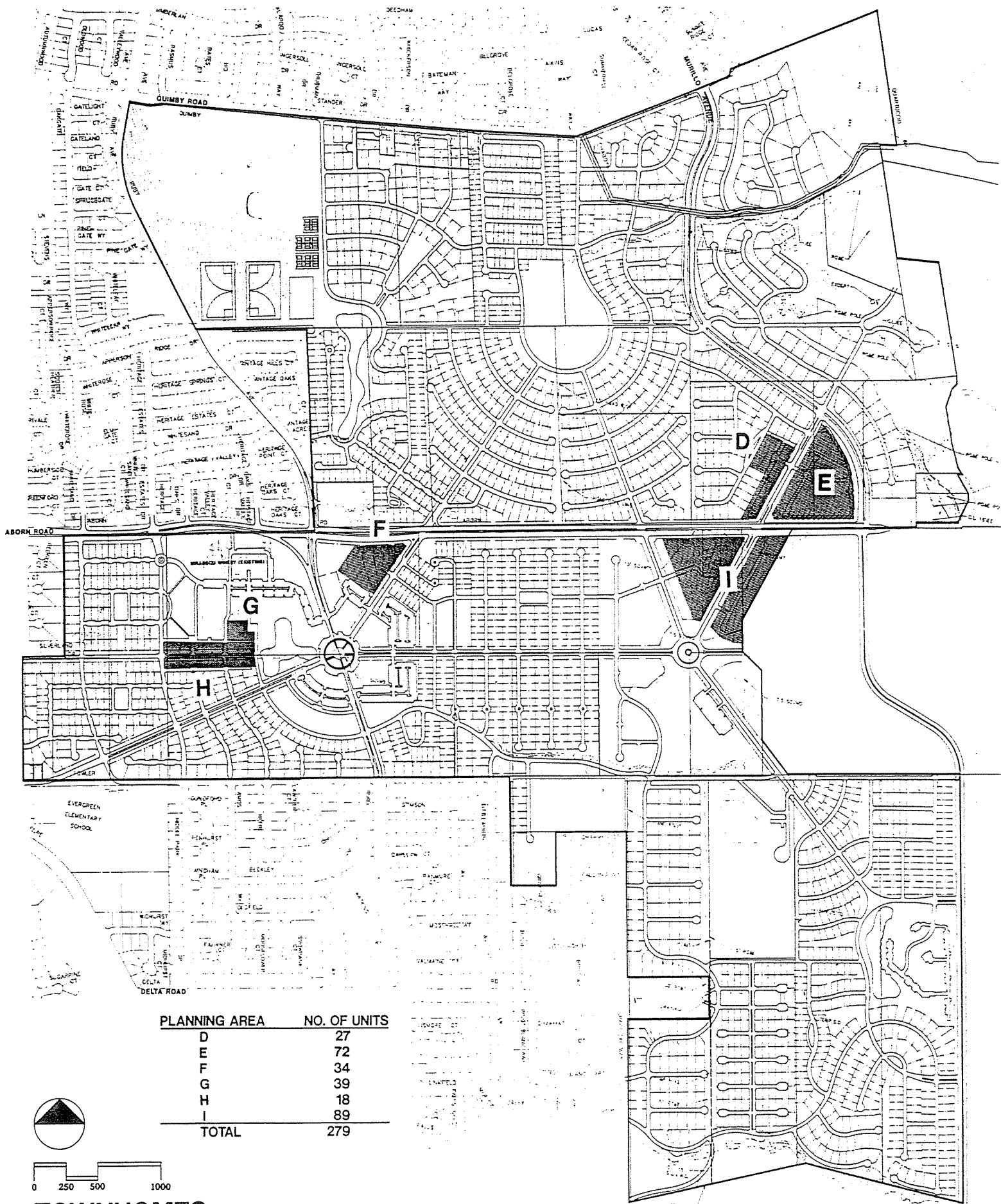
Front Setbacks:

From Streets:	A:2.5/B:8/C:12/D:18/E:12
Front to Front Separations:	30'
Front to Side Separations:	25'

Rear Setbacks:

Rear Setbacks:	R:10/S:20/T:20/U:20
Rear Setbacks to Alley:	0'

Curb cuts on radial collectors and neighborhood collectors are limited to one sixteen foot (16') cut per one-hundred feet (100') of street length or one twenty-six foot (26') curb cut per two-hundred feet (200') of street length. Guest parking for units facing radial or neighborhood collectors shall be provided at a ratio of one space per

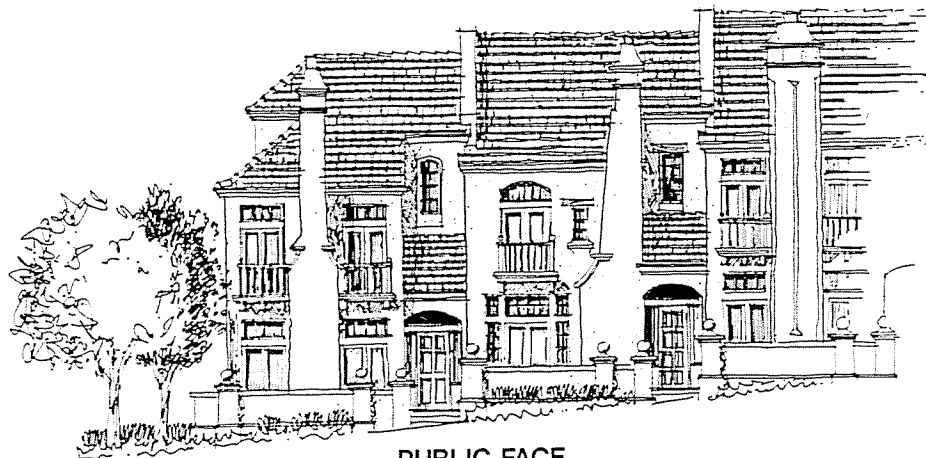
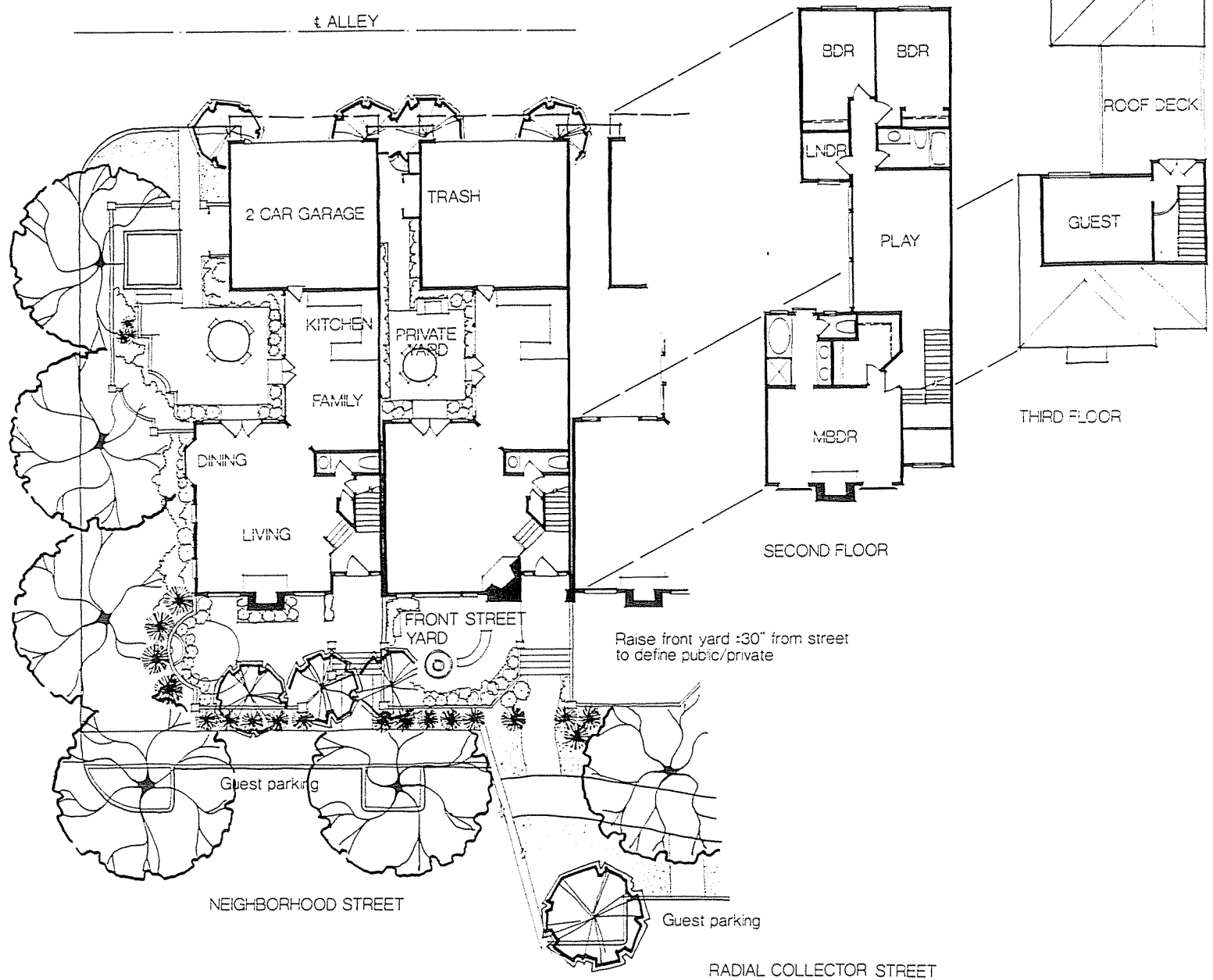


PLANNING AREA	NO. OF UNITS
D	27
E	72
F	34
G	39
H	18
I	89
<b>TOTAL</b>	<b>279</b>

## TOWNHOMES

CITY OF SAN JOSE  
EVERGREEN SPECIFIC PLAN





PUBLIC FACE  
(STREET FACADE)

## TOWNHOME DEVELOPMENT CRITERIA

CITY OF SAN JOSE  
EVERGREEN SPECIFIC PLAN

two units developed as parking bays along the street. Guest parking for "internal" units shall be provided at a ratio of one space per three units and distributed so as to be reasonably convenient to all units. Alleys and rear-facing garages are encouraged, especially for units facing radial and neighborhood collectors. These units should face the street with entry doors and living area windows visible.

These townhomes are subject to *Residential Design Guidelines for the City of San Jose* Section 19, Row Houses except Section 19 A.1 Setbacks, which are described above. Section 19 B.1 and 19 B.2 apply only to units facing radial or neighborhood collectors.

### **Signs**

Signs are allowed in accordance with Section 20.24.350 of Title 20.

### **Off-Street Parking**

The minimum number of off-street parking spaces shall be as set forth in Section 20.12.215 of Title 20 (Two-family dwellings).

### **Townhome Landscaping**

Landscaping locations and objectives should conform to the *Residential and Irrigation Guidelines for the City of San Jose* and the *Landscape and Irrigation Guidelines for the City of San Jose*. Landscape character should complement the public area as well as landscape plans included in the "Community Amenities" chapter of the Evergreen Specific Plan.

## DUPLEXES

### Concept

Duplexes are sited in the Evergreen Specific Plan as an enclave within a detached single family area. In such instances, duplexes allow a substantial increase in density while each two unit building presents the same height and mass of the surrounding single family buildings. Building form, fenestration, entries, and garage configurations that alternate front- and side-facing garages should emphasize this single family appearance rather than a two-unit row house.

### Permitted Uses

#### Primary use:

- A. Paired dwellings or duplexes.
- B. Public utility facilities primarily to serve Specific Plan area residents.

#### Secondary uses:

- A. Accessory buildings as provided for in Section 20.24.260(6) of Title 20.
- B. Private recreation, meeting, and maintenance facilities.
- C. Day care facilities

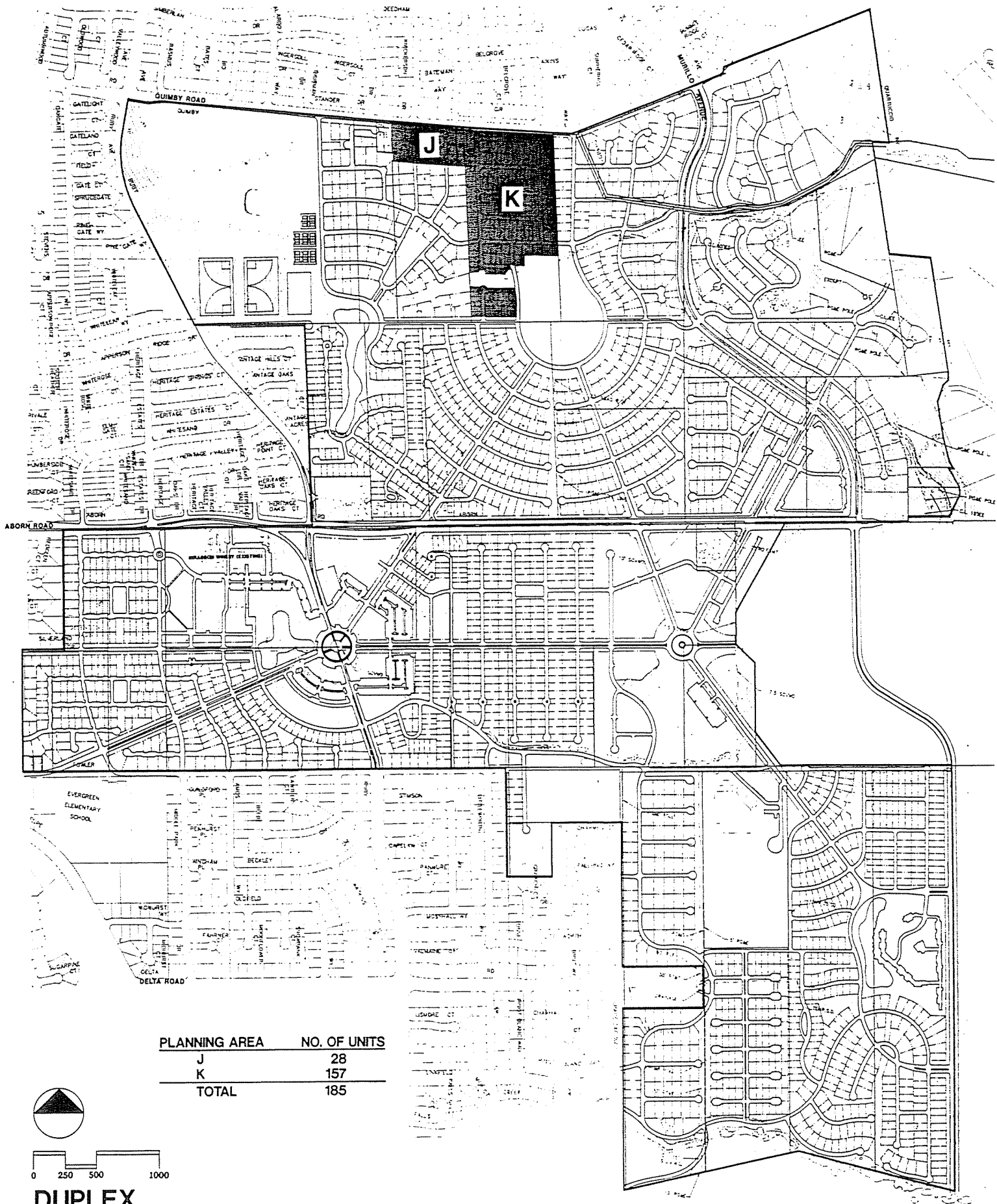
### Development Standards

Building Height Plane: Thirty feet (30').

#### Setbacks:

Front Setbacks:	A:5/B:8/C:15/D:18/E:15
Detached Side Setbacks:	J:4/K:10/L:12/M:8/N:50
Attached Side Setbacks	J:0/K:15/L:12/M:8/N:50
Rear Setbacks:	R:10/S:20/T:20/U:20

Rear setbacks may be modified for specific projects employing innovative siting relationships which maintain a minimum six-hundred (600) square foot "rear yards" in a compact shape and a minimum of eight feet (8') separation between detached buildings.



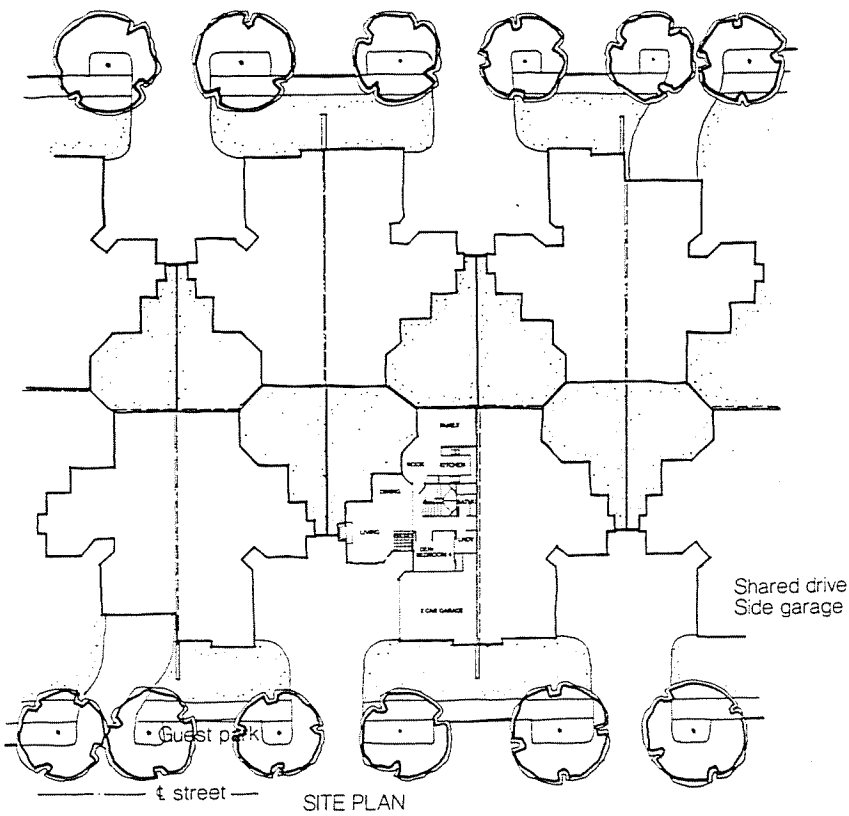
PLANNING AREA	NO. OF UNITS
J	28
K	157
<b>TOTAL</b>	<b>185</b>



0 250 500 1000

**DUPLEX**

**CITY OF SAN JOSE**  
**EVERGREEN SPECIFIC PLAN**



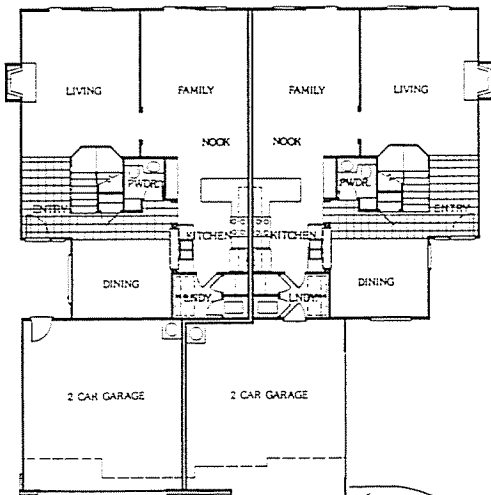
SITE PLAN

## DUPLEX/ZERO LOT LINE HOMES

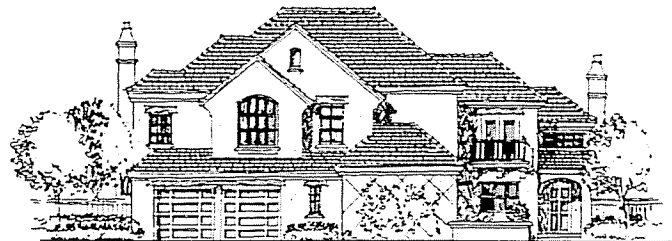
Duplexes can visually appear as larger single-family homes



STREETSCAPE

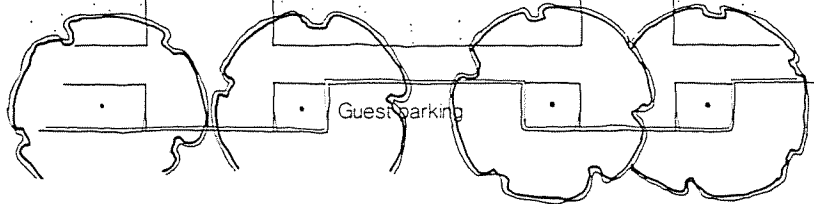


PLAN



ELEVATION

## DUPLEX HOMES



## DUPLEX DEVELOPMENT CRITERIA

**Signs**

Signs are allowed in accordance with Section 20.24.320 of Title 20.

**Off-Street Parking**

The minimum number of off-street parking spaces shall be as set forth in Section 20.12.215 of Title 20 (Two-family dwellings). However, on-street guest parking shall be provided at a ratio of one space for every unit fronting the street. Where units cluster off private drives a ratio of one guest space for every three units should be maintained.

**Duplex/Paired Dwelling Landscaping**

Landscaping locations and objectives should conform to the *Residential Design Guidelines for the City of San Jose* and the *Landscape and Irrigation Guidelines for the City of San Jose*. Landscape character should complement the public area landscape plans included in the "Community Amenities" chapter of the Evergreen Specific Plan.

## CARRIAGE HOMES

### Concept

These homes, sited near the Village Center and Fowler Creek Park offer an alternative housing mix. Single family homes may, on their own lot, include a second unit. This unit may be used for adult members of an extended family household, domestic help, or simply as a rental unit. These secondary units, called carriage homes (a throw back to the days when the stable keeper or other household help lived over the stables or carriage house), can be located above rear-loading, rear lot side-loading, or semi-attached garages.

Alternatives in these areas may include a mix of smaller attached, larger single family detached, and duplex for-sale units.

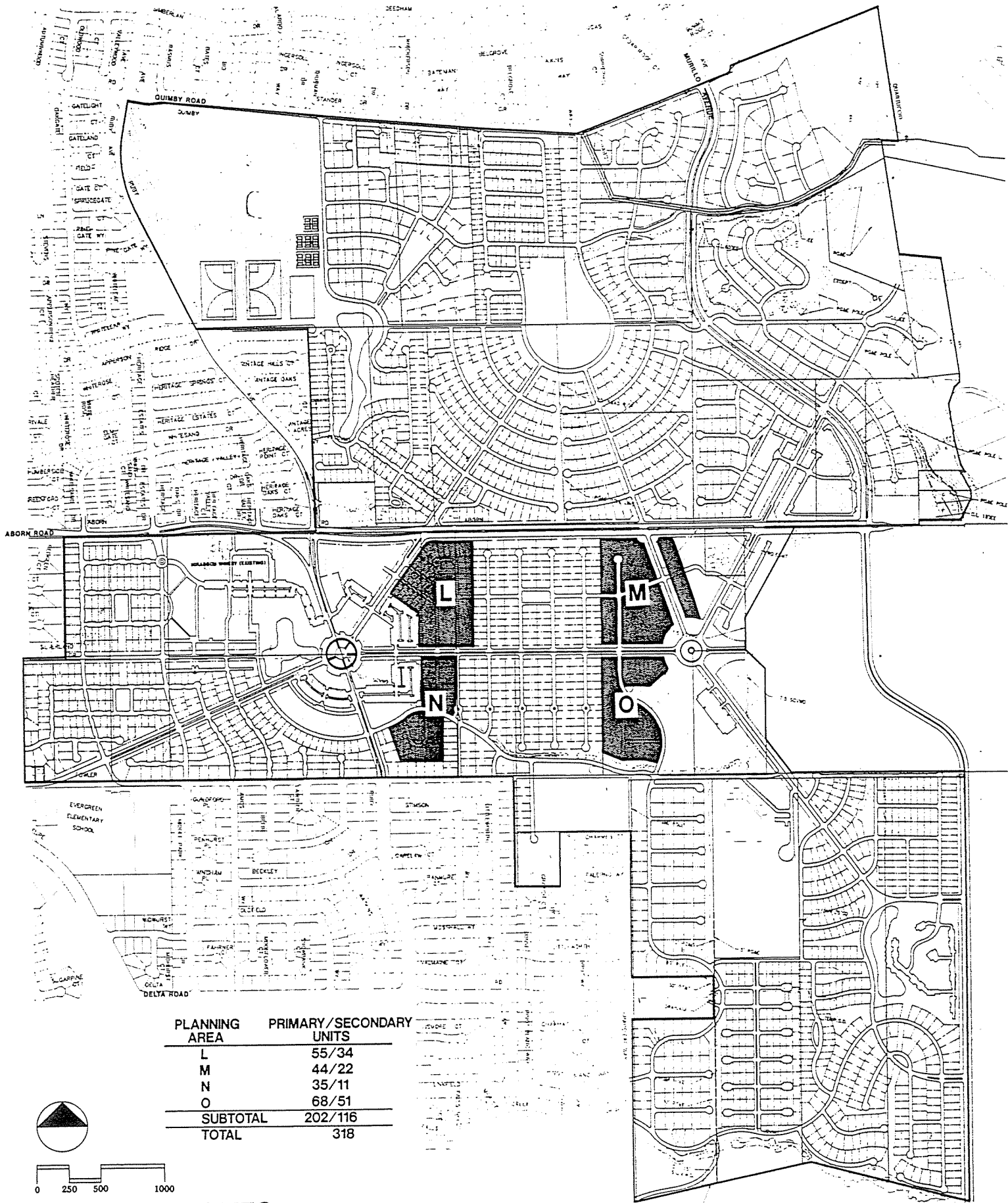
Detached rear-loading or back of lot garages allow the neighborhood streetscape to present a traditional row of home facades where living area windows, second story balconies, entry doors, and front porches create an open, welcoming, and neighborly environment for the pedestrian stroller.

The Evergreen Specific Plan recognizes that this residential mix represents a significant departure from the typical for homebuilders, homebuyers, government agencies, and financial institutions. As such, these criteria intend to provide maximum flexibility to respond to market, finance, and agency needs, and expects that unit design and mix will evolve with new phases. Within this flexibility key goals include: 1) Primary units are generally "for sale" single family detached or attached homes in a 1,200 square foot to 2,600 square foot range; 2) Secondary units are either "for sale" or rental, one and two bedroom units in a 400 to 1,000 square foot range. These may be very small lot single family, or rental either as multi-family rental buildings or individual rental units on primary unit's property; 3) Primary and secondary units should be intermixed throughout these planning areas; and, 4) Overall "feel" should be of a traditional small home neighborhood where front doors, living area windows, and secondary story balconies and window flower boxes (not garages) line pedestrian scale streets, parks, mews, or paths.

### Permitted Uses

Primary uses:

- A. One-family dwellings.
- B. Two-family dwellings.
- C. Paired dwellings or duplexes.



PLANNING AREA	PRIMARY/SECONDARY UNITS
L	55/34
M	44/22
N	35/11
O	68/51
SUBTOTAL	202/116
TOTAL	318

## CARRIAGE HOMES

CITY OF SAN JOSE  
EVERGREEN SPECIFIC PLAN

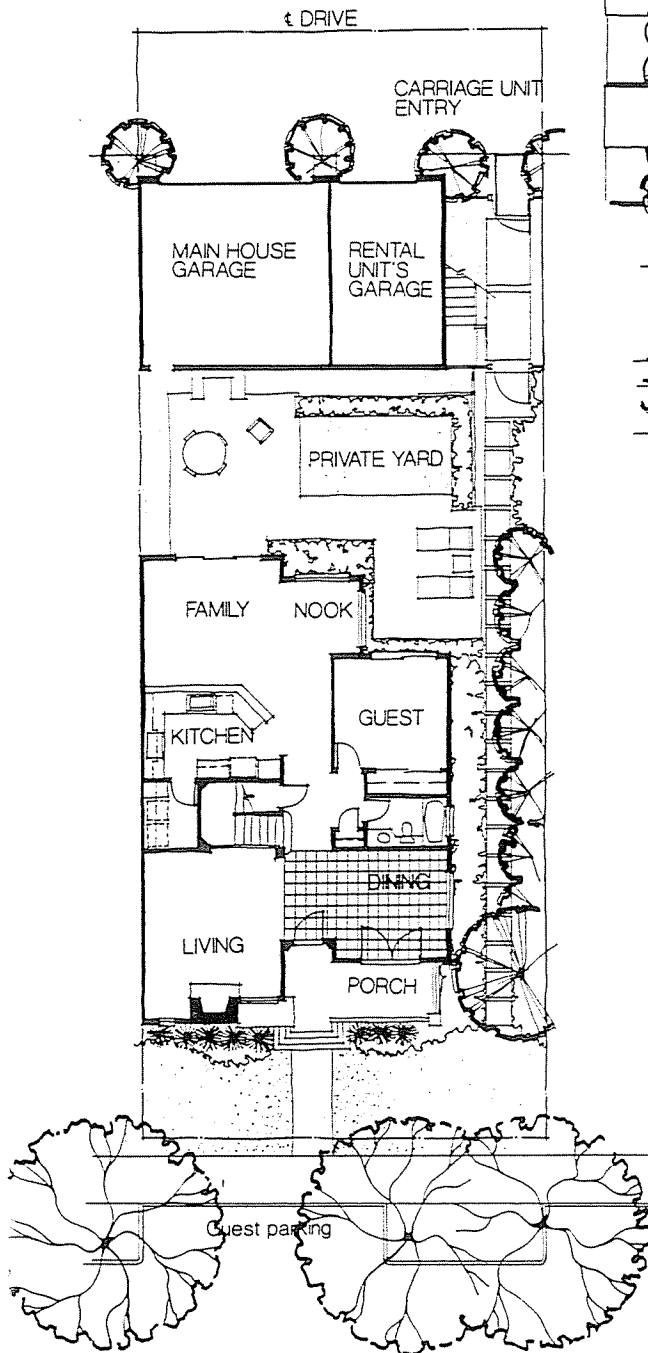


Where no rear access  
duplex homes share drive  
to garage carriage unit

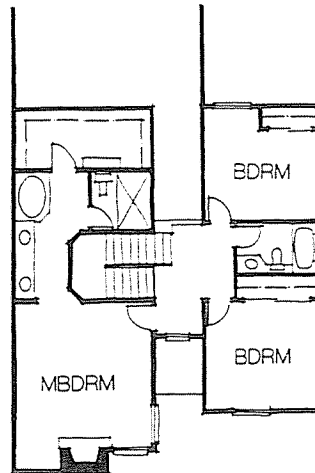
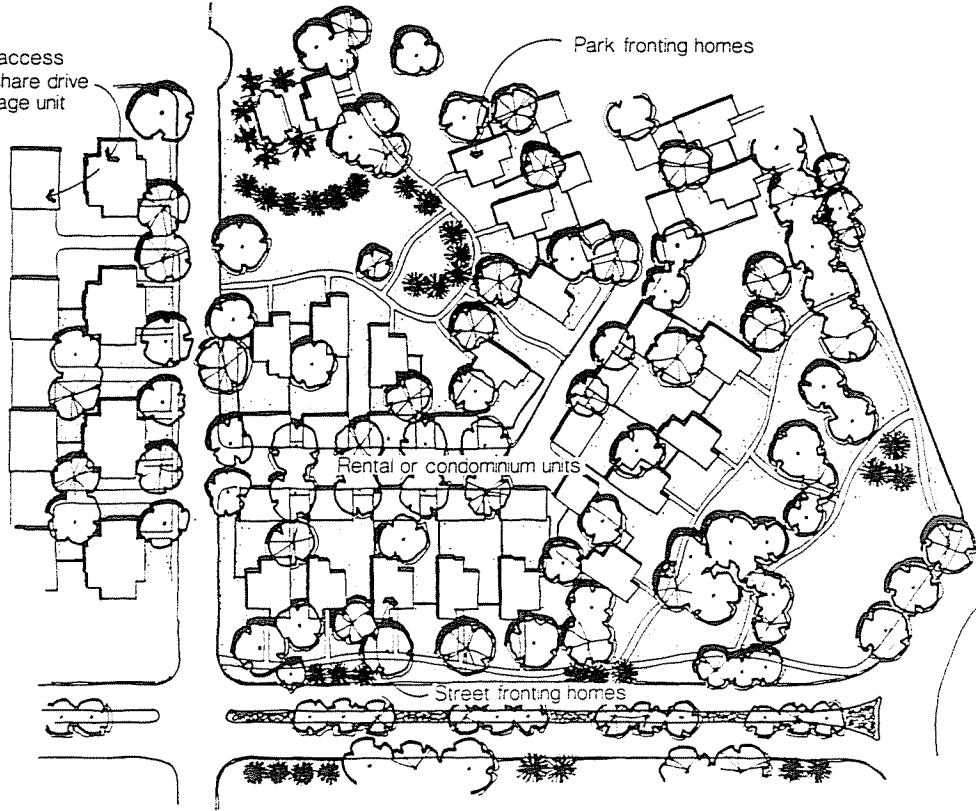
Park fronting homes

Rental or condominium units

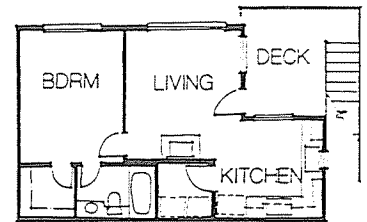
Street fronting homes



TYPICAL PLAN



SECOND FLOOR



RENTAL OR CONDOMINIUM  
UNIT 0/ 3 CAR GARAGE



STREET SCAP

## CARRIAGE HOMES DEVELOPMENT CRITERIA

CITY OF SAN JOSE  
EVERGREEN SPECIFIC PLAN

EXHIBIT

55

D. Public utility facilities primarily to serve Specific Plan area residents.

Secondary uses:

A. Accessory buildings as provided for in Section 20.24.260(6) of Title 20.

B. Private recreation, meeting, and maintenance facilities.

C. Day care facilities

**Development Standards**

Building Height Plane: Thirty feet (30').

**Setbacks**

Primary units or primary with secondary attached units:

Front Setbacks:

(From street or common space) A:1.5/B:16/C:12/D:25/E:12

Side Setbacks: J:4/K:8/L:8/M:7/N:50

J:0 at attached unit common wall

Rear Setbacks: R:10/S:20/T:20/U:20

With attached garages accessed  
from a rear driveway:

R:0/S:0/T:0/U:0

Secondary units detached:

Front setbacks:

Free Standing from Street: A:1.5/B:16/C:12/D:25/E:12

From Private Open Space,  
Private Drives, and All Other  
Property Lines:

Four Feet (4')

Secondary units without garages:

Front Setbacks: A:1.5/B:16/C:12/D:25/E:12

Side Setbacks: J:4/K:8/L:8/M:7/N:16

J:0 at attached unit common wall

Rear-backing to Single Family: Twenty Feet (20')

Rear-backing Private Drive: Zero Feet (0')

Rear-backing to Each Other: Twenty Foot (20') Separation

**Signs**

Signs are allowed in accordance with Section 20.24.320 of Title 20.

**Off-Street Parking**

The minimum number of off-street parking spaces shall be as set forth in Section 20.12.215 of Title 20.

**Carriage Homes Landscaping**

Landscaping locations and objectives should conform to the *Residential Design Guidelines for the City of San Jose* and the *Landscape and Irrigation Guidelines for the City of San Jose*". Landscape character should complement the public area landscape plans included in the "Community Amenities" chapter of the Evergreen Specific Plan.

## 4,000 AND 5,000 SQUARE FOOT LOTS

### **Concept**

These smaller lot, single family neighborhoods are distributed throughout the Evergreen Specific Plan area. To minimize garage door dominance of small lot streetscapes, lots should generally be fifty feet (50') wide at the building setback. Front-facing garages set back from the home facade and side-entry garages are encouraged.

Building form and elevation should emphasize living areas and de-emphasize garage doors. Well articulated second stories pushed out over the garage doors are encouraged.

At corner lots, side elevations are very important and will be expected to have the same level of articulation and detail as front elevations.

These neighborhoods are each laid out with pocket parks or common-linking green belts.

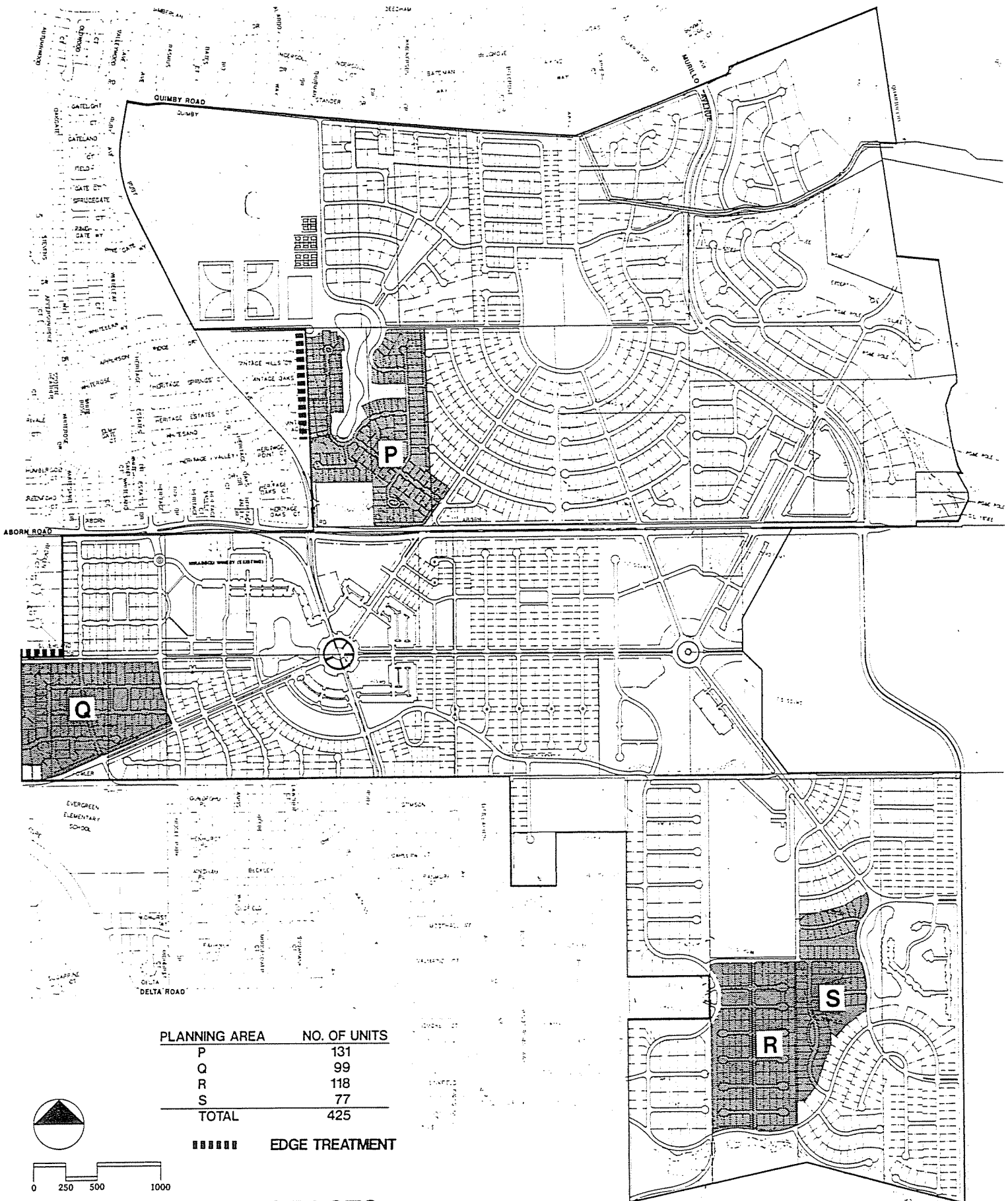
### **Permitted Uses**

Primary use:

- A. One-family dwellings.
- B. Churches. (Churches shall abut a major arterial or major collector and shall take access only from a major collector or radial collector.)
- C. Day care facilities
- D. Private recreation, meeting, and maintenance facilities for the use of surrounding residents.
- E. Public utility facilities primarily to serve Specific Plan area residents.

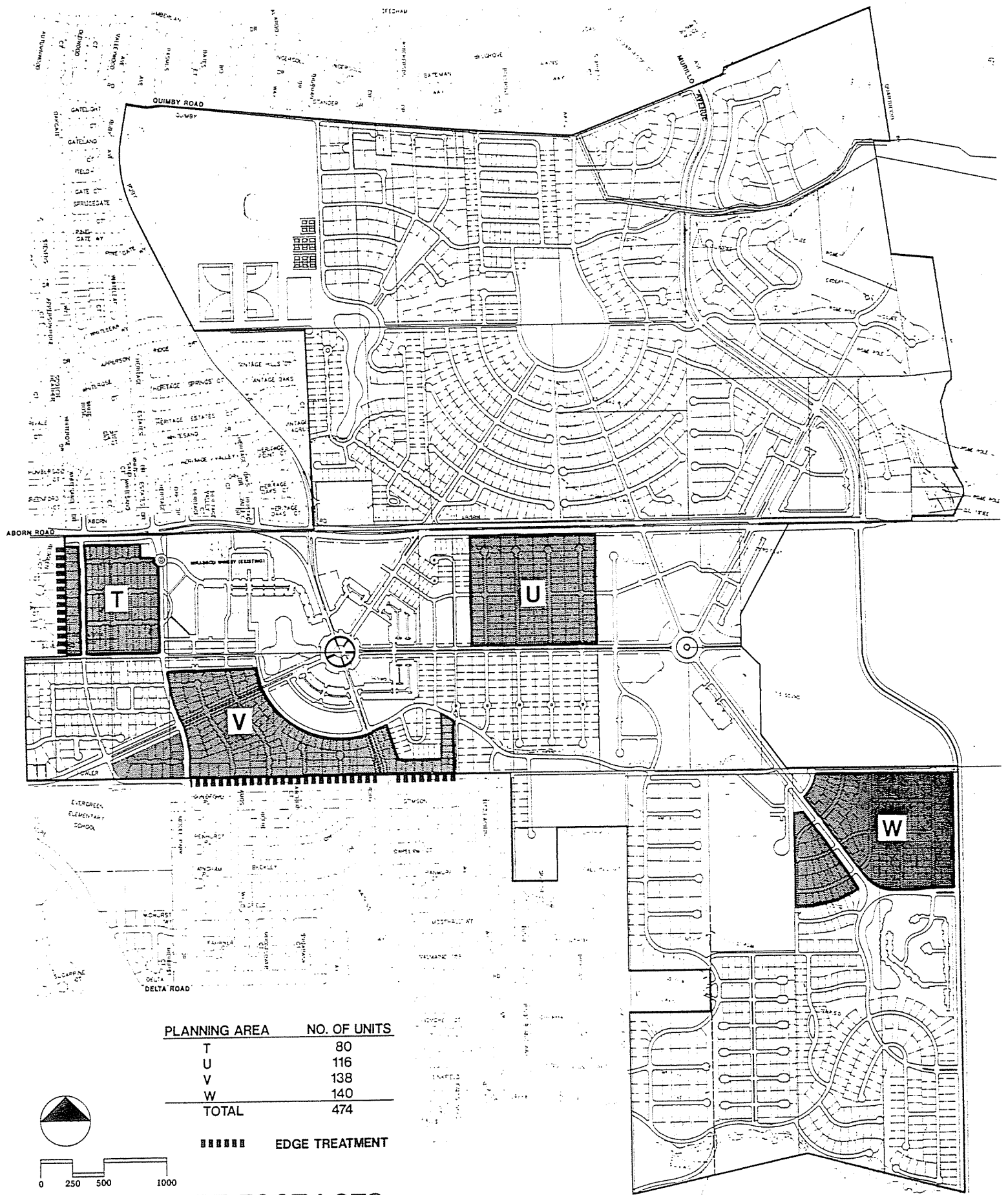
Secondary uses:

- A. Accessory buildings as provided for in Section 20.24.260(6) of Title 20.



4000 SQUARE FOOT LOTS

CITY OF SAN JOSE  
EVERGREEN SPECIFIC PLAN

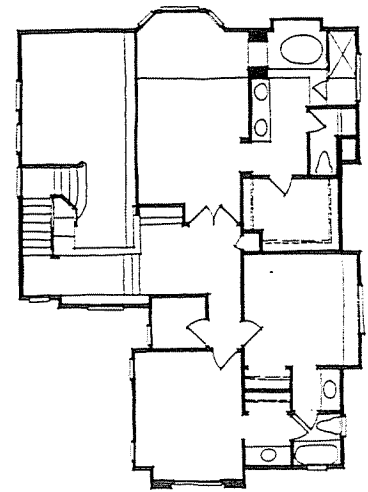


**5000 SQUARE FOOT LOTS**

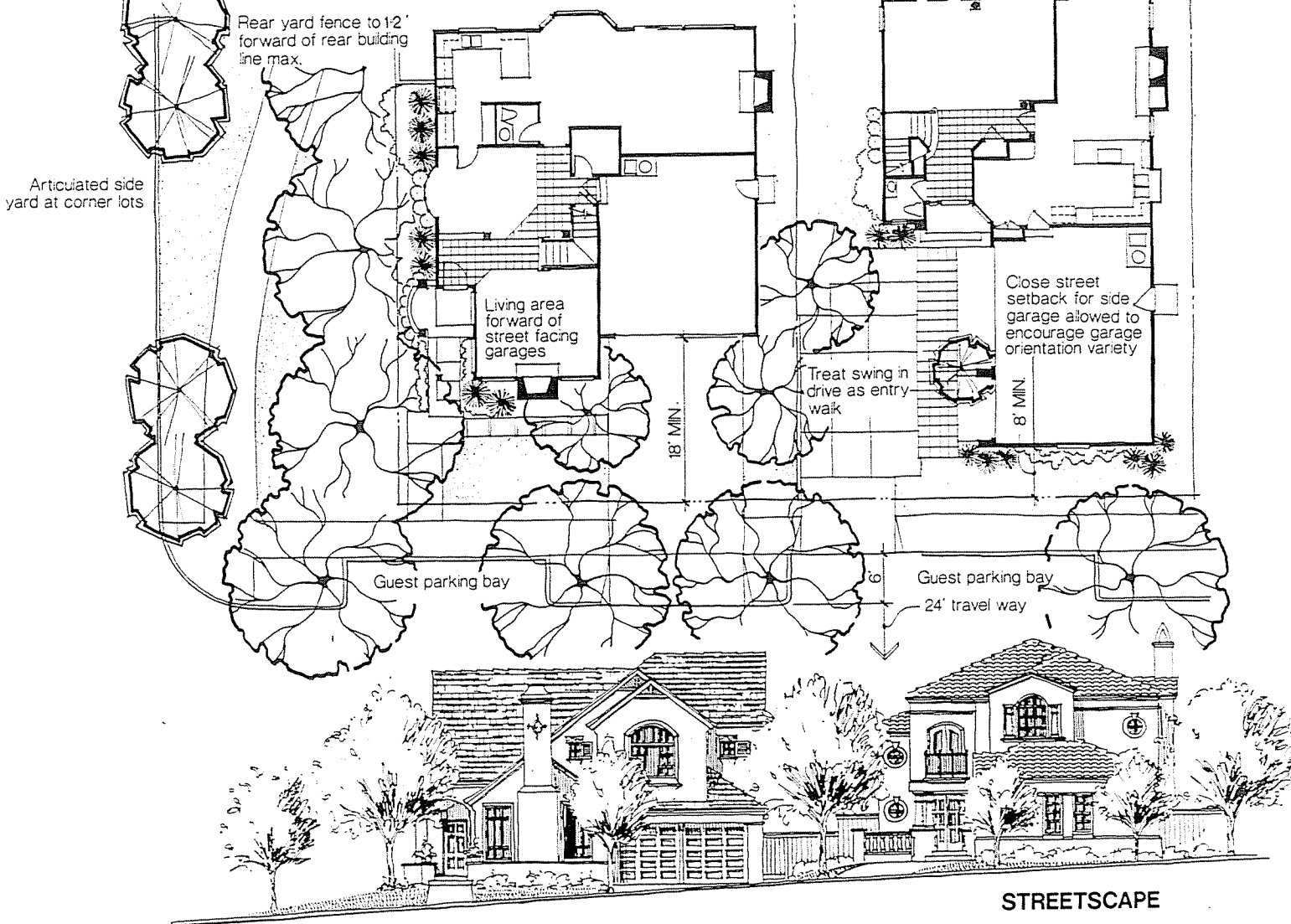
**CITY OF SAN JOSE  
EVERGREEN SPECIFIC PLAN**

## SECOND FLOOR

## SECOND FLOOR



### CORNER SIDE YARD ELEVATION



## 4,000 & 5,000 SQ. FT. LOTS DEVELOPMENT CRITERIA

CITY OF SAN JOSE  
EVERGREEN SPECIFIC PLAN

**Development Standards: 4,000 Square Foot Lots**

Average Lot Size:	At least 4,000 square feet within each planning area
Minimum Lot Size:	3,200 square feet

Building Height Plane: Thirty feet (30').

**Setbacks**

Front Setbacks:	A:3/B:8/C:12/D:18/E:12
Side Setbacks:	J:4/K:10/L:10/M:8/N:30 J:8 at corner lot
Rear Setbacks:	R:10/S:20/T:20/U:20

**Development Standards: 5,000 Square Foot Lots**

Average Lot Size:	At least 5,000 square feet within each planning area.
Minimum Lot Size:	3,600 square feet

Building Height Plane: Thirty feet (30').

**Setbacks**

Front Setbacks:	A:3/B:8/C:15/D:18/E:15
Side Setbacks:	J:4/K:10/L:10/M:8/N:30 J:8 at corner lot
Rear Setbacks:	R:10/S:20/T:20/U:20

Houses within each of these neighborhoods shall either conform to the *Residential Design Guidelines for the City of San Jose*, Sections 17 B.1 and B.2 for garage frontage and placement or incorporate a side-entry garage. Provide on-street parking at a minimum ratio of .75 spaces per home.

**Signs**

Signs are allowed in accordance with Section 20.24.120 of Title 20.

**Off-Street Parking**

The minimum number of off-street parking spaces shall be as set forth in Section 20.12.215 of Title 20.



### **Garage Conversions**

Garage conversions shall not be allowed in any form within the Evergreen Specific Plan area.

### **4,000 and 5,000 Square Foot Lot Landscaping**

Landscaping locations and objectives should conform to the *Residential Design Guidelines for the City of San Jose* and the *Landscape and Irrigation Guidelines for the City of San Jose*. Landscape character should complement the public area as well as the landscape plans included in the "Community Amenities" chapter of the Evergreen Specific Plan.

## 6,000 SQUARE FOOT LOTS

### Concept

These standard lot size, single family neighborhoods are distributed throughout the Evergreen Specific Plan. These lots should generally be sixty feet (60') wide at the building setback line. Key to the success of these neighborhoods is the articulation of building architecture particularly the front facade and side elevation of corner lots. Streetscapes should avoid the "three car garage, front door, living room window" repetition and employ side entry and split garages as well as garages recessed back from the front of the home. Recessed garage doors and planting breaks between garage doors are required. These neighborhoods are sometimes on steeper sloping sites ( $\pm 8\%$ ) and the incorporation of uphill or side-split pads and plans are encouraged.

### Permitted Uses

Primary use:

- A. One-family dwellings.
- B. Churches. (Churches shall abut a major arterial or major collector and shall take access only from a major collector or radial collector.)
- C. Day care facilities.
- D. Private recreation, meeting, and maintenance facilities for the use of surrounding residents.
- E. Public utility facilities primarily to serve Specific Plan area residents.

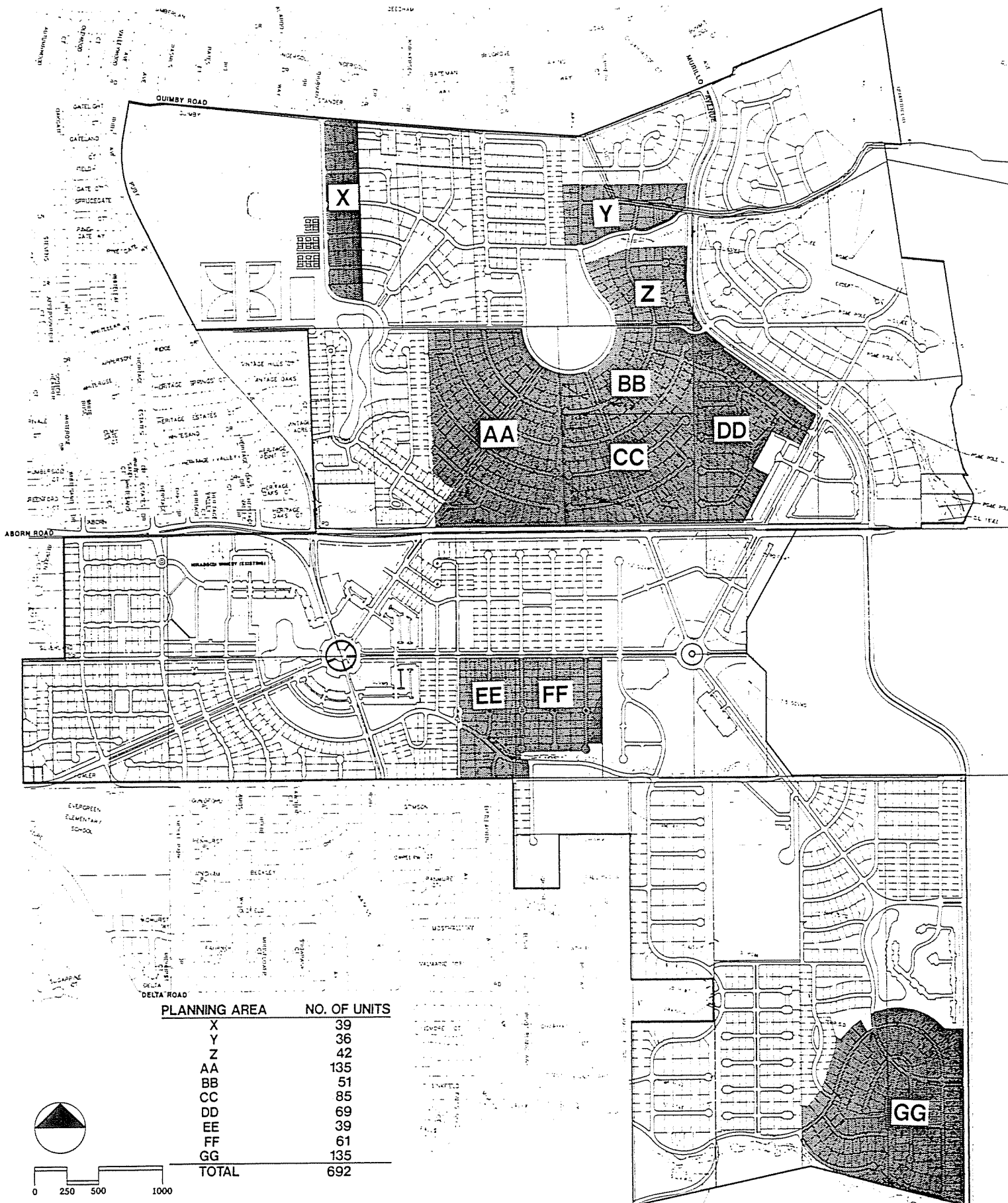
Secondary uses:

- A. Accessory buildings as provided for in Section 20.24.260(6) of Title 20.

### Development Standards

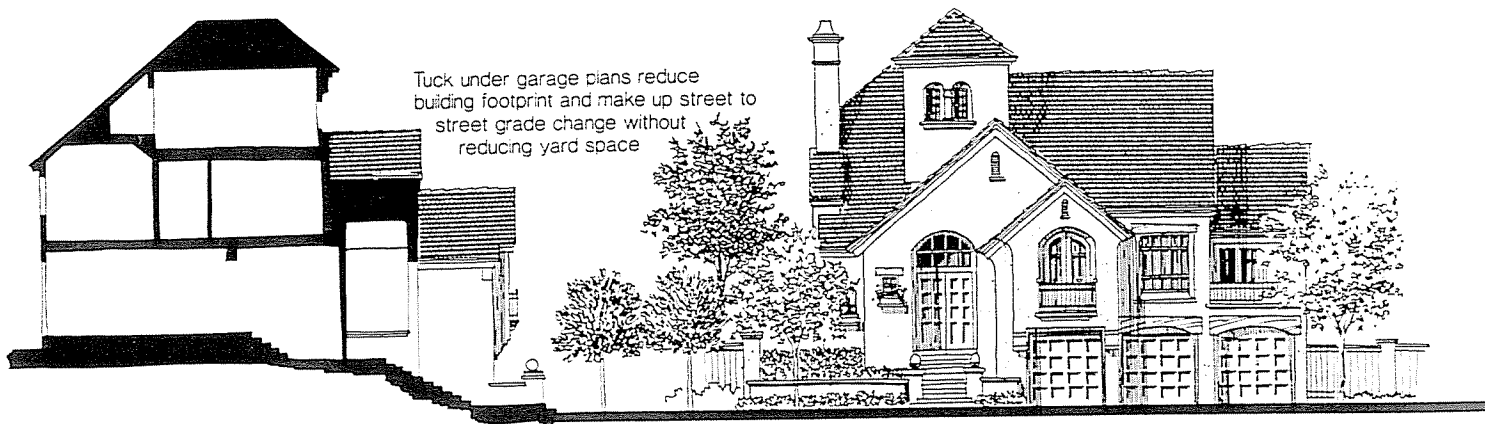
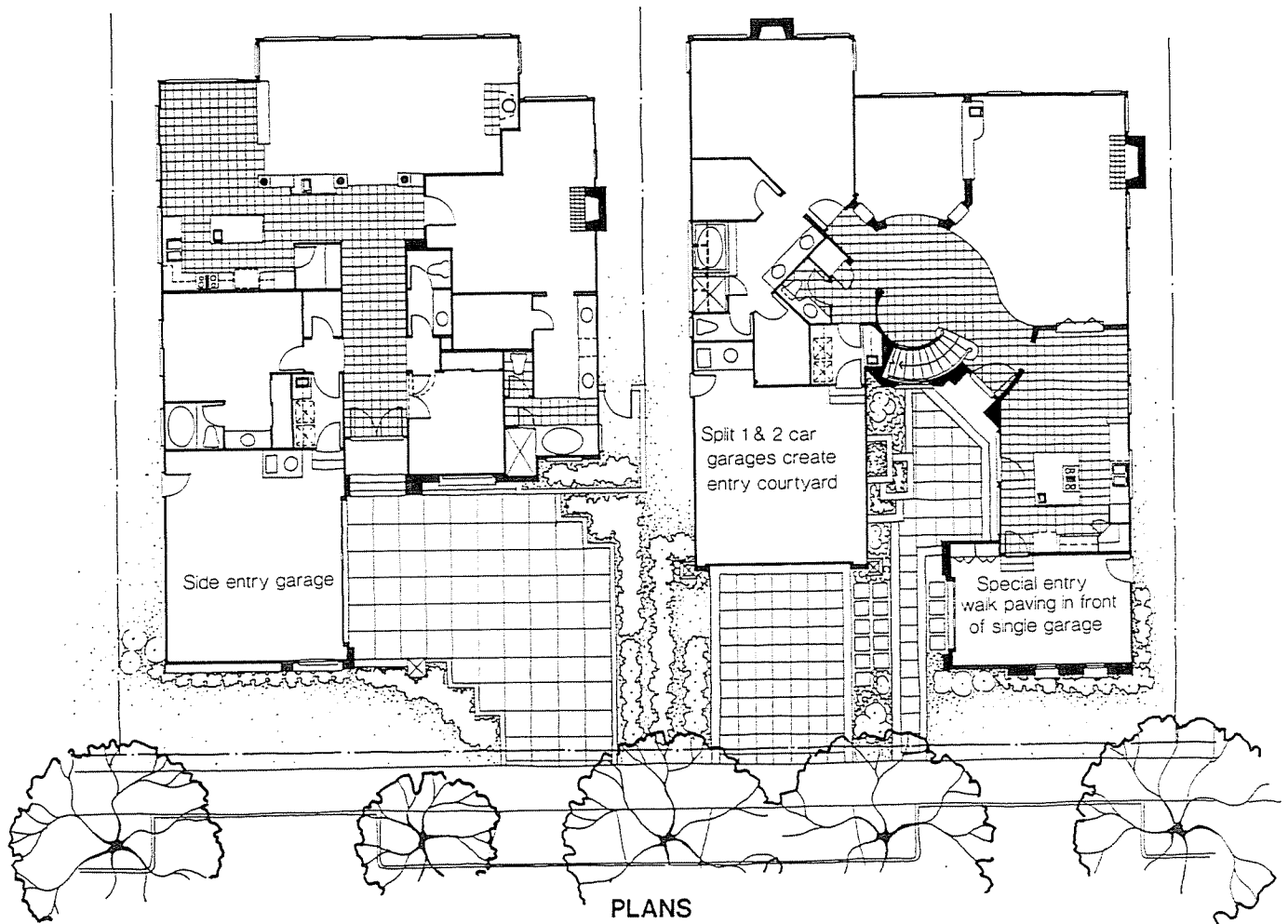
Building Height Plane: Thirty feet (30').

Average Lot Size:	At least 6,000 square feet within each planning area.
Minimum Lot Size:	4,000 square feet.



6000 SQUARE FOOT LOTS

CITY OF SAN JOSE  
EVERGREEN SPECIFIC PLAN



6,000 SQ. FT. LOTS

CITY OF SAN JOSE  
EVERGREEN SPECIFIC PLAN

## **Setbacks**

Front Setbacks:	A:5/B:10/C:18/D:20/E:18
Side Setbacks:	J:5/K:10/L:12/M:8/N:50
	J:10 at corner lot
Rear Setbacks:	R:15/S:25/T:20/U:25

Provide on-street parking at a ratio of one space per home.

## **Signs**

Signs are allowed in accordance with Section 20.12.215 of Title 20.

## **Off-Street Parking**

The minimum number of off-street parking spaces required shall be as set forth in Section 20.24.360 of Title 20.

## **Garage Conversions**

Garage conversions shall not be allowed in any form within the Evergreen Specific Plan area.

## **6,000 Square Foot Lot Landscaping**

Landscaping locations and objectives should conform to the *Landscape and Irrigation Guidelines for the City of San Jose*. Landscape character should complement the public area as well as the landscape plans included in the "Community Amenities" chapter of the Evergreen Specific Plan.

## 7,000 AND 8,000 SQUARE FOOT LOTS

### Concept

These larger lot neighborhoods are distributed throughout the Evergreen Specific Plan. With generally wider frontage they can accommodate larger homes and more "spread out" plans. With eighty feet (80') of frontage, plans can incorporate auto courts and "away" side garages to vary the streetscape and de-emphasize garage doors. Roof forms may vary between cascading to first floor plates at the perimeter to more formal two-story perimeter plates.

### Permitted Uses

Primary use:

- A. One-family dwellings.
- B. Churches. (Churches shall abut a major arterial or major collector and shall take access only from a major collector or radial collector.)
- C. Day care facilities.
- D. Private recreation, meeting, and maintenance facilities for the use of surrounding residents.
- E. Public utility facilities primarily to serve Specific Plan area residents.

Secondary uses:

- A. Accessory buildings as provided for in Section 20.24.260(6) of Title 20.

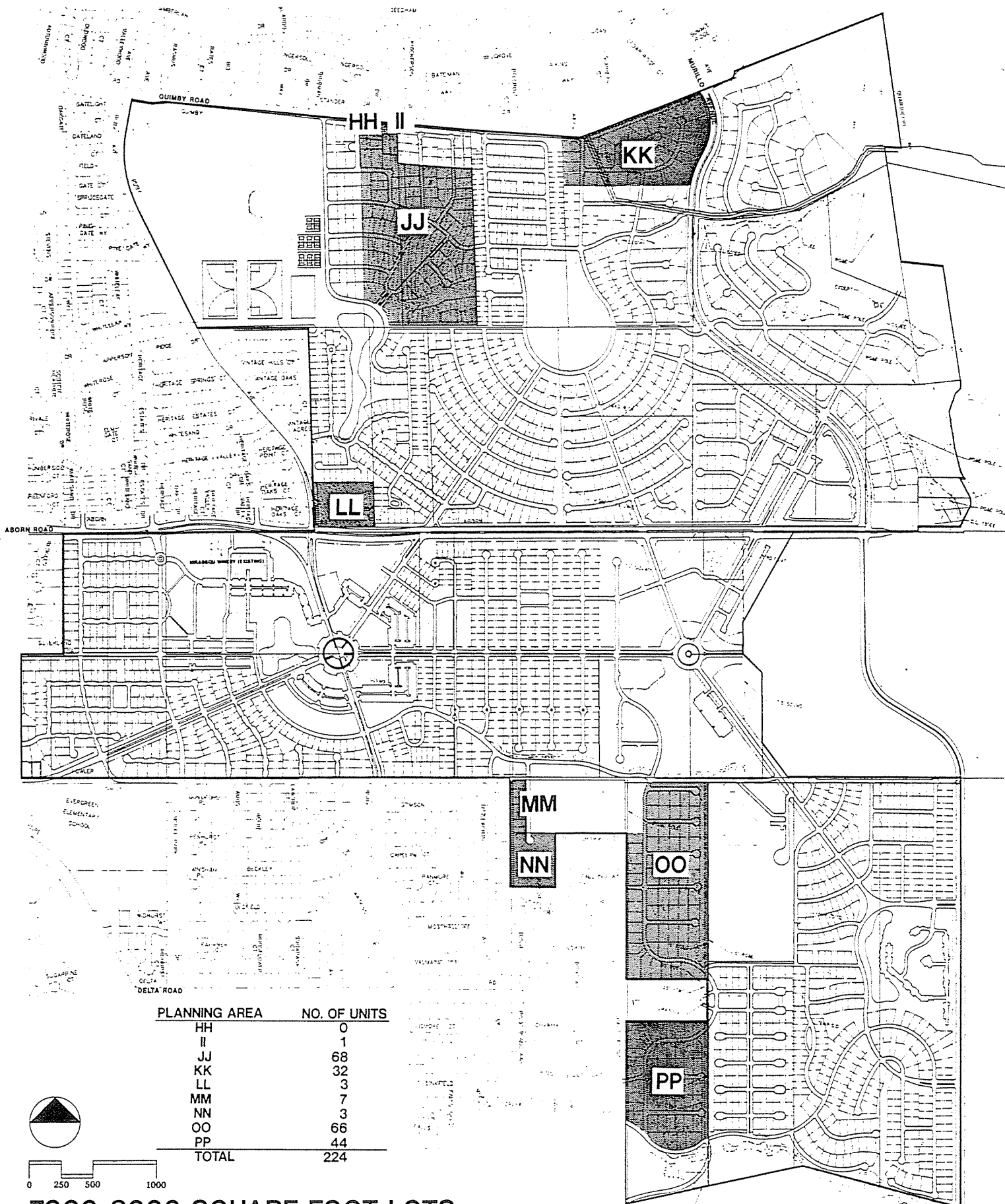
### Development Standards

Average Lot Size:	At least 7,000 Square Feet within each planning area.
Minimum Lot Size:	5,500 Square Feet

Building Height Plane: Thirty feet (30').

### Setbacks

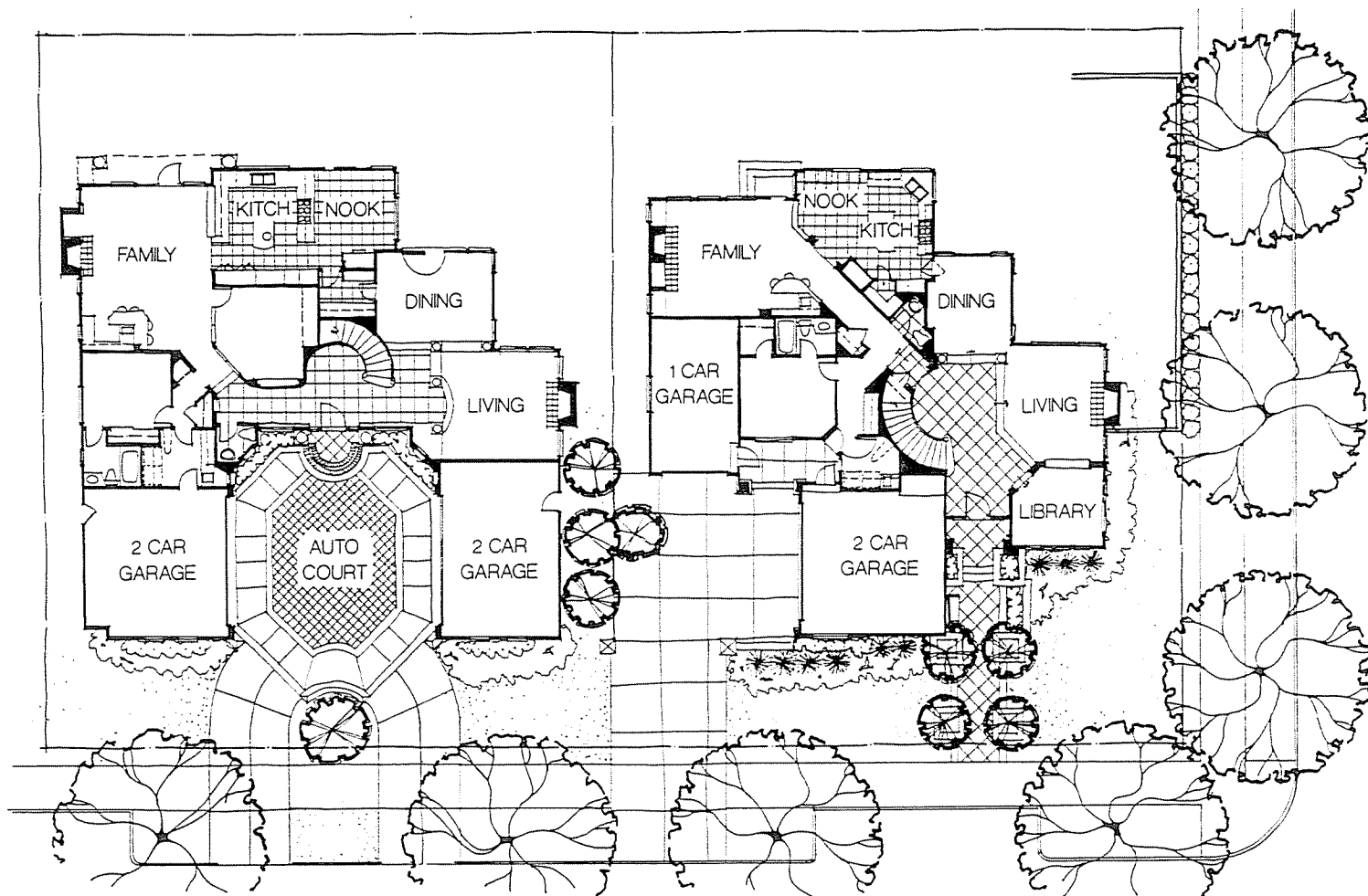
Front Setbacks:	A:5/B:15/C:18/D:25/E:25
Side Setbacks:	J:6/K:13/L:15/M:10/N:50
	J:12 at corner lot
Rear Setbacks:	R:15/S:25/T:20/U:25



7000-8000 SQUARE FOOT LOTS

CITY OF SAN JOSE

EVERGREEN SPECIFIC PLAN



STREET SCAPE

Wider lots accommodate more variation in garage treatment and plan configuration including opposing garage auto courts and side entry plans.



ARTICULATED SIDE ELEVATION AT CORNER LOT

## 7000 AND 8000 SQ. FT. LOTS DEVELOPMENT CRITERIA

CITY OF SAN JOSE  
EVERGREEN SPECIFIC PLAN



On-street guest parking shall be provided at a ratio of one space per home.

### **Signs**

Allowed in accordance with Section 20.24.120 of Title 20.

### **Off-Street Parking**

The minimum number of off-street parking spaces shall be as set forth in Section 20.12.215 of Title 20.

### **Garage Conversions**

Garage conversions shall not be allowed in any form within the Evergreen Specific Plan area.

### **7,000 to 8,000 Square Foot Lot Landscaping**

Landscaping locations and objectives should conform to the *Landscape and Irrigation Guidelines for the City of San Jose*. Landscape character should complement the public area as well as the landscape plans included in the "Community Amenities" chapter of the Evergreen Specific Plan.

## HILLSIDE LOTS

### Concept

These neighborhoods are all located in the hill area east of Murillo in the northeast corner of the Evergreen Specific Plan. They serve as a transition to open space and rural densities further east. While these homes will enjoy spectacular views across San Jose to the west, they are also the most visible homes. Street-facade architecture is as important here as other neighborhoods and the same articulation and de-emphasis of garage doors through split and side garages applies. Upslope split lots are encouraged to lessen grading impact. Particular attention shall be paid to lots on the downhill side of the street. These homes will present very visible rear elevations.

Visible rear elevations shall present a well-articulated architectural form. Generally on flat pad sites, second story areas should be limited to sixty percent (60%) of the length of the rear elevation. Masses should be broken up and roof lines should cascade down to first floor plates at the sides. On downhill split pads where main living areas are on the second floor, decks over first floor areas or first floor roof bands should be used to create a terracing mass stepping down the hill. Screen planting at rear yard should help to soften the view of the home from below while framing views from the house. Such planting should cascade down the hill and in some instances transition into open space.

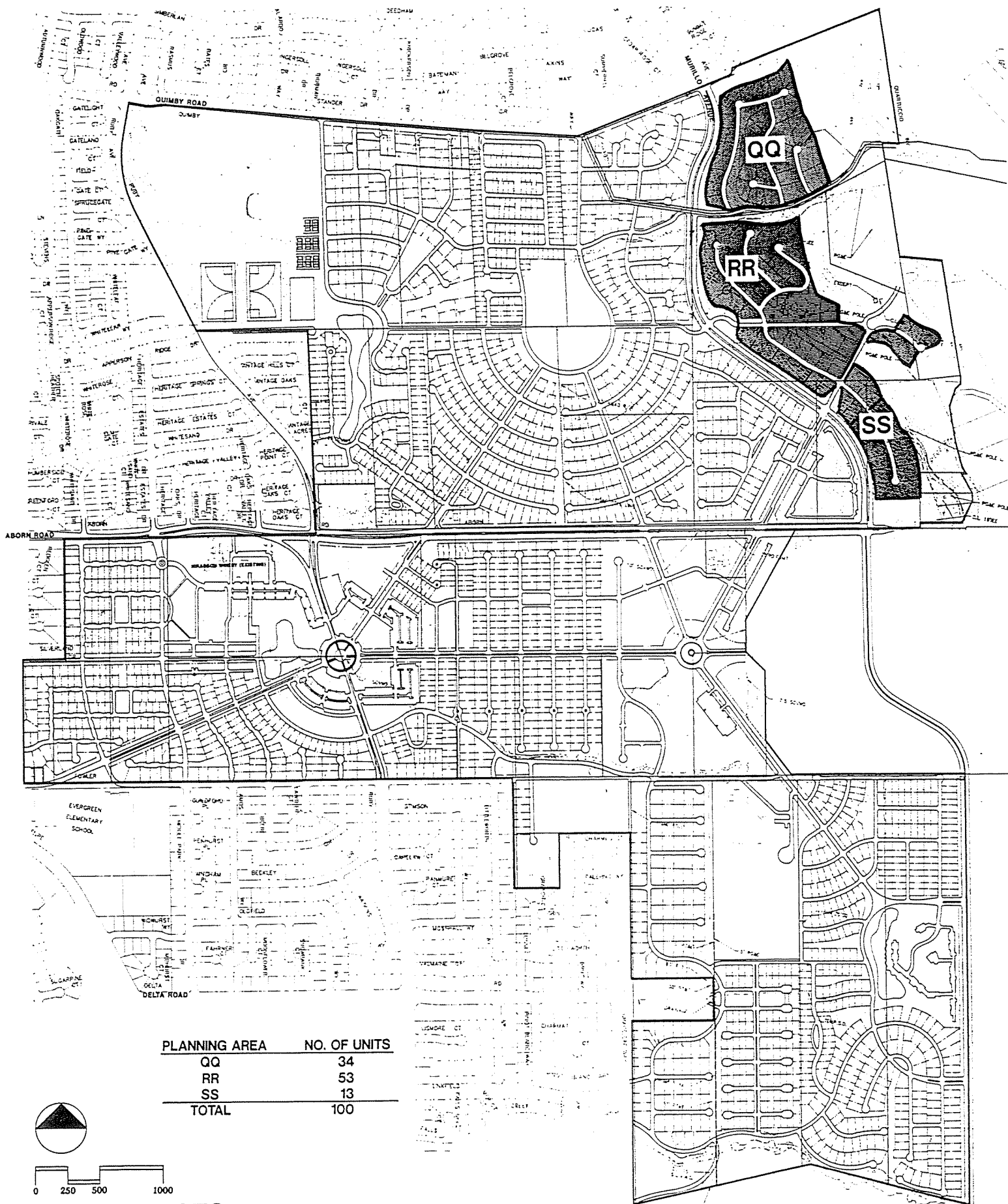
### Permitted Uses

Primary use:

- A. One-family dwellings.
- B. Churches. (Churches shall abut a major arterial or major collector and shall take access only from a major collector or radial collector.)
- C. Day care facilities.
- D. Private recreation, meeting, and maintenance facilities for the use of surrounding residents.
- E. Public utility facilities primarily to serve Specific Plan area residents.

Secondary uses:

- A. Accessory buildings as provided for in Section 20.24.260(6) of Title 20.

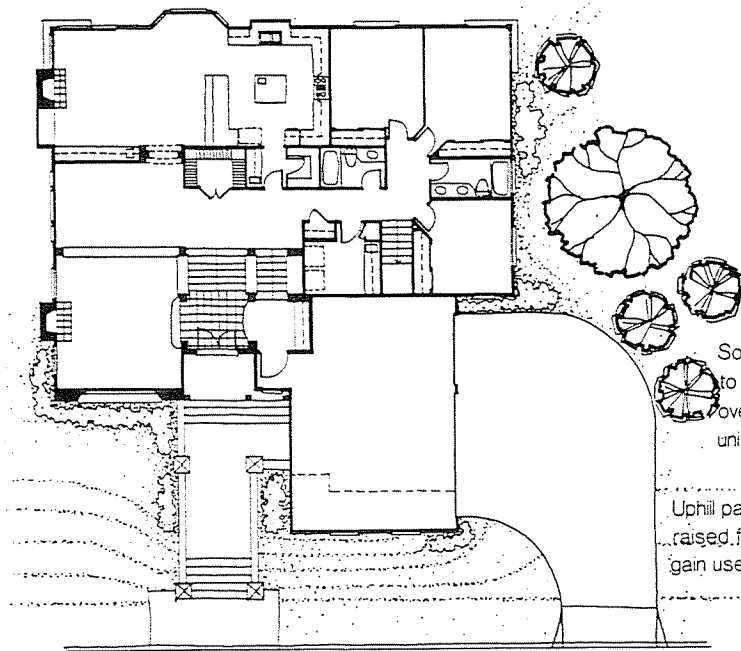


## HILLSIDE LOTS

CITY OF SAN JOSE  
EVERGREEN SPECIFIC PLAN

EXHIBIT

63



UPHILL

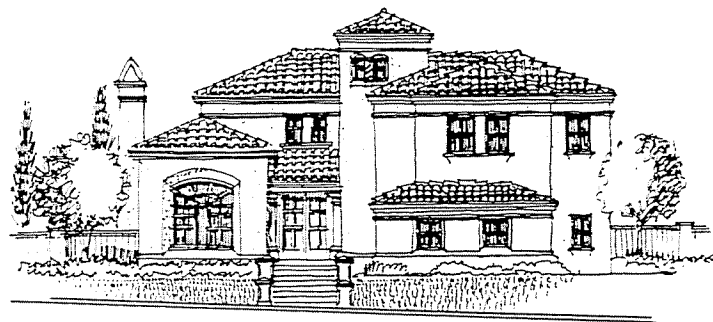


DOWNHILL

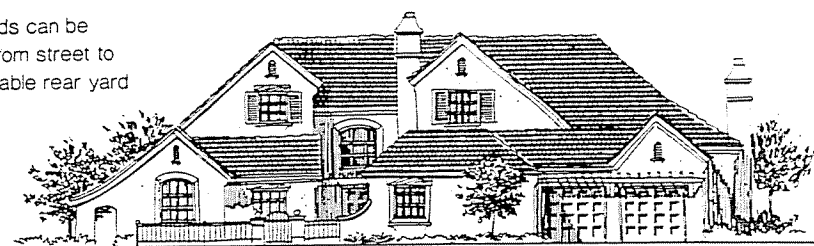
28' wide streets without sidewalks  
add a rural feel and reduce grading  
in the low density hillside area

Some key rooms sited  
to front capture views  
over or between downhill  
units

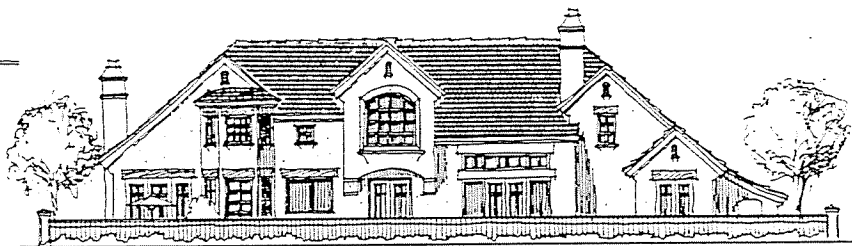
Uphill pads can be  
raised from street to  
gain useable rear yard



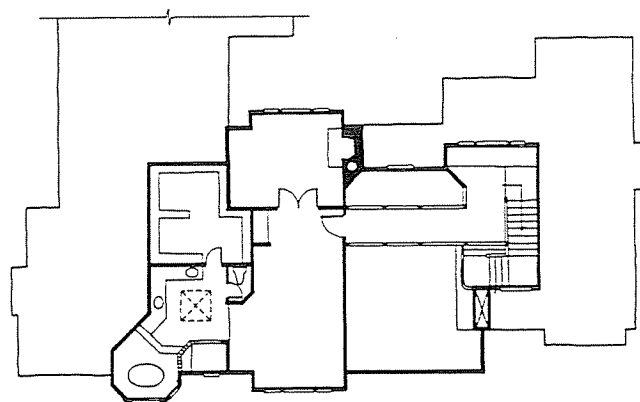
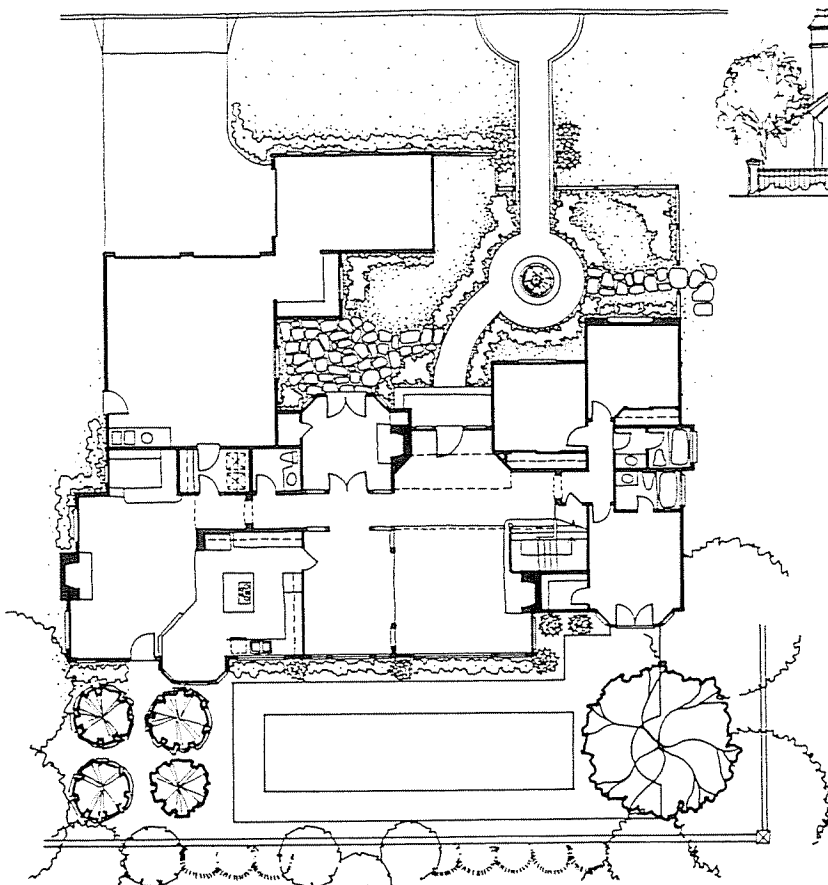
UPHILL HOME FRONT ELEVATION



DOWNHILL HOME FRONT ELEVATION



DOWNHILL HOME REAR ELEVATION



DOWNHILL HOME SECOND FLOOR PLAN

Screen planting on pad  
and downslope to soften  
home impact from below

Consolidate upper floor area  
to minimize second story  
rear elevation impact

## HILLSIDE LOTS DEVELOPMENT CRITERIA

## **Development Standards**

Average Lot Size:	At least 10,000 Square Feet within each planning area.
Minimum Lot Size:	6,000 Square Feet
Building Height Plane:	
Downslope:	24'
Upslope:	35'
Downslope Setbacks:	
Front Setbacks:	A:5/B:15/C:20/D:30/E:25
Side Setbacks:	J:8/K:20/L:30/M:15/N:100
	J:16K at corner lot
Rear Setbacks:	R:20/S:30/T:20/U:40
Upslope Setbacks:	
Front Setbacks:	A:5/B:15/C:20/D:30/E:25
Side Setbacks:	J:8/K:20/L:30/M:15/N:100
	J:16 at corner lot
Rear Setbacks:	R:10/S:20/T:10/U:20

On-street parking shall be provided at a ratio of one space per home.

## **Signs**

Allowed in accordance with Section 20.24.120 of Title 20.

## **Off-Street Parking**

The minimum number of off-street parking spaces shall be as set forth in Section 20.12.215 of Title 20.

## **Garage Conversions**

Garage conversions shall not be allowed in any form within the Evergreen Specific Plan area.

## **Hillside Lot Landscaping**

Landscaping locations and objectives should conform to the *Landscape and Irrigation Guidelines for the City of San Jose*. Landscape character should complement the public area as well as the landscape plans included in the "Community Amenities" chapter of the Evergreen Specific Plan. Slopes between residential lots should be in common open space and maintained by a lighting and landscape district.

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**Infrastructure**



## Chapter 9 - Infrastructure

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A critical element of the Evergreen Specific Plan is the development of the necessary infrastructure to support the project. This chapter outlines the various elements that make up the infrastructure of the Evergreen Specific Plan.

### CIRCULATION SYSTEM

The Evergreen Specific Plan provides a definite hierarchy of streets, walkways, and trails throughout the project area. Area residents will be protected from major through-traffic corridors by providing ample setbacks from the streets, sound walls, and landscape buffers. At the same time, circulation within the project area has been emphasized by the radial street pattern, which provides axial vistas to the civic and village commercial areas.

#### Through-Community Streets

In viewing the surrounding region as a whole, it becomes apparent that the Evergreen Specific Plan area lies between the existing transportation facilities to the west, such as U.S. 101 and Interstate 680, and a future employment center to the east. Local access to the site is provided primarily by Aborn and Quimby Roads. These roads run in an east-west direction and intercept with the Capitol Expressway. The project area can also be accessed via Murillo Avenue from the north and Yerba Buena Avenue from the south.

##### Aborn Road:

Aborn Road, a major arterial (Exhibits 65 and 66), will be the primary route into the project site and to the campus industrial employment center further to the east. Aborn Road is currently designated as a major arterial by the City of San Jose's *Horizon 2000 General Plan*.

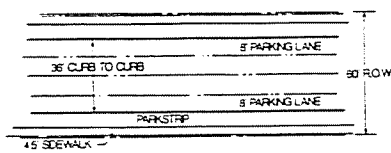
In an effort to allow the majority of campus industrial traffic to pass through the Evergreen Specific Plan area, Aborn Road will remain a major arterial and will be constructed as a six-lane arterial within a one hundred forty-five foot (145') right-of-way. This allows for a generously landscaped parkway of twenty-three and one-half feet (23.5') from the edge of pavement to the acoustical sound wall. Contained within this landscaped parkway is a meandering sidewalk running the length of Aborn Road to Murillo Avenue.

##### Quimby Road:

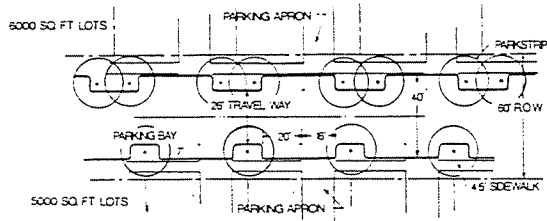
The second of the through-community streets is Quimby Road (Exhibits 65 and 66). As stated previously, Quimby Road is an east-west street running from the Capitol Expressway to the project site. Quimby Road is currently designated as minor arterial from Capitol Expressway to Ruby Avenue, then is downgraded to a major collector from Ruby Avenue to Murillo Avenue.



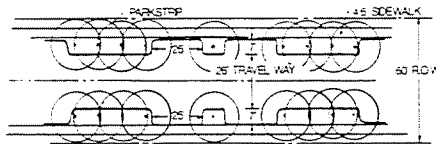
### NEIGHBORHOOD COLLECTOR



### NEIGHBORHOOD COLLECTOR WITH FRONT-ON DWELLING UNITS

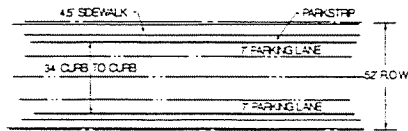


### NEIGHBORHOOD COLLECTOR WITH NO FRONT-ON DWELLING UNITS

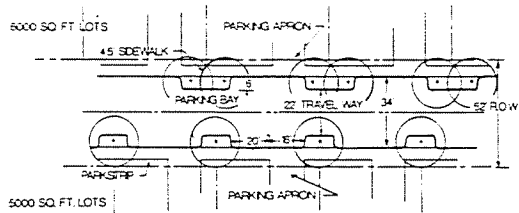


### NEIGHBORHOOD COLLECTORS

### MINOR STREETS

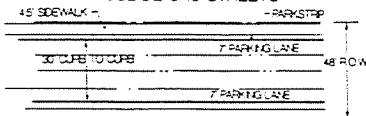


### MINOR STREETS WITH PARKING BAYS

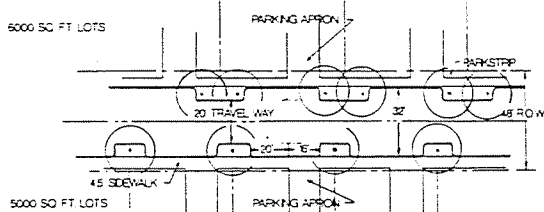


### MINOR STREETS

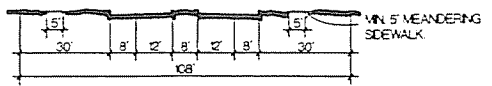
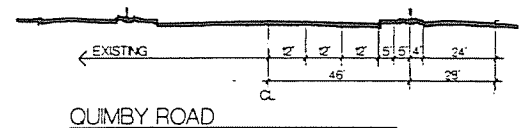
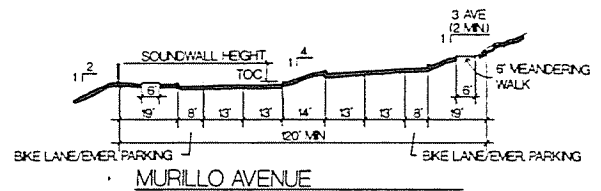
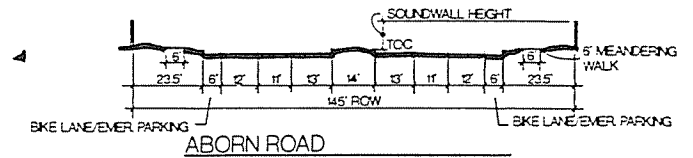
### CUL-DE-SAC STREETS



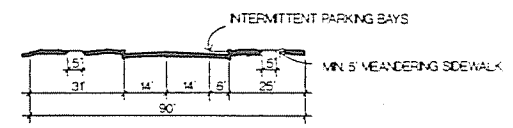
### CUL-DE-SAC STREETS WITH PARKING BAYS



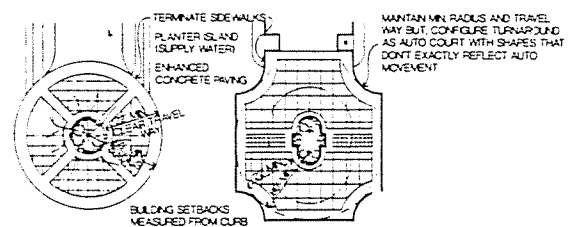
### CUL-DE-SAC STREETS



### RADIAL WITH ISLAND



### RADIAL WITH PARKING POCKET

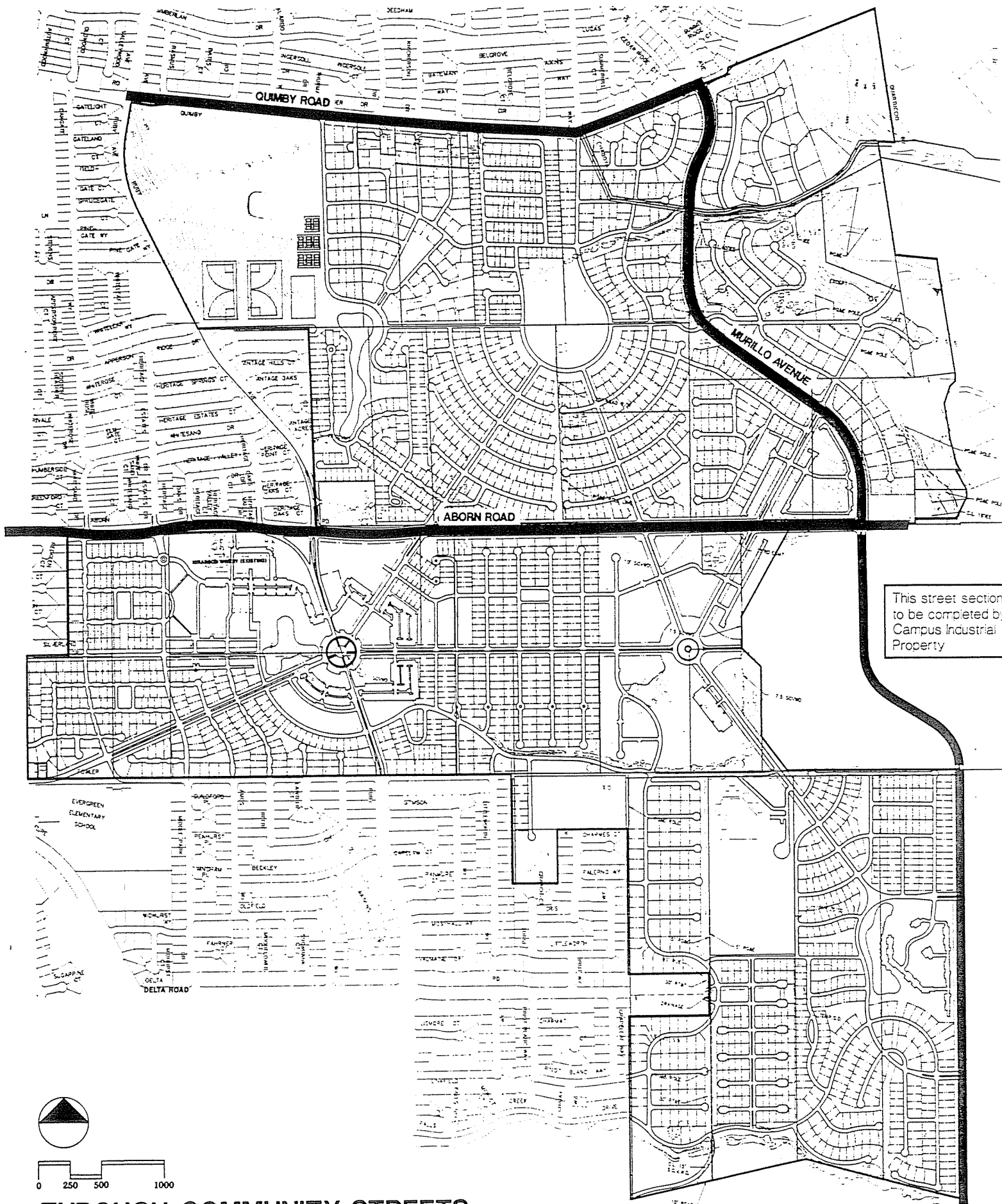


### ALTERNATE CUL-DE-SAC TURNAROUNDS

### CIRCULATION PLAN GENERAL NOTES

1. THE CLASSIFICATION OF NEIGHBORHOOD STREET SECTIONS (NEIGHBORHOOD COLLECTORS, MINOR STREETS, AND CUL-DE-SACS) FOR SPECIFIC STREETS SHALL BE REVIEWED AS PART OF THE PLANNED DEVELOPMENT PERMIT PROCESS FOR INDIVIDUAL PLANNING AREAS.
2. CUL-DE-SACS AND OTHER MINOR STREETS HAVING USE CHARACTERISTICS SIMILAR TO CUL-DE-SACS, I.E. SIMILAR NUMBER OF UNITS SERVED, NO DIRECT CONNECTIONS TO TWO OR MORE NEIGHBORHOOD COLLECTORS AND A HORIZONTAL ALIGNMENT CONSISTENT WITH SAFETY.
3. RESIDENTIAL CURB CUTS ARE LIMITED TO SIXTEEN FEET (16'). "NO PARKING" CURB AREAS IN SINGLE FAMILY NEIGHBORHOODS SHOULD BE SIGNED WITH LOW (3' HIGH) "NO PARKING" SIGNS.
4. AT LEAST ONE (1) ON-STREET PARKING SPACE SHALL BE PROVIDED FOR EACH SINGLE FAMILY DWELLING UNIT OR TWO FAMILY DWELLING UNIT.
5. NEIGHBORHOOD COLLECTORS WITH TREE POCKETS AND PARKING BAYS SHALL HAVE NO MORE THAN TWO (2) CONSECUTIVE PARKING BAY SPACES.
6. NEIGHBORHOOD COLLECTOR RIGHT OF WAY WITH TREE POCKETS AND PARKING BAYS MAY VARY FROM 50' TO 60' DEPENDING ON THE REQUIREMENTS FOR PUBLIC UTILITY EASEMENTS.
7. MINOR STREET RIGHT OF WAY WITH TREE POCKETS AND PARKING BAYS MAY VARY FROM 44' TO 52' DEPENDING ON THE REQUIREMENTS FOR PUBLIC UTILITY EASEMENTS.
8. CUL-DE-SAC STREET RIGHT OF WAY WITH TREE POCKETS AND PARKING BAYS MAY VARY FROM 44' TO 48' DEPENDING ON THE REQUIREMENTS FOR PUBLIC UTILITY EASEMENTS.
9. INTERNAL FRONTAGE ROADS ALONG QUIMBY ROAD ARE INTENDED TO MATCH THE EXISTING CONFIGURATION ON THE NORTH SIDE. THE GOAL IS TO HAVE HOMES FRONTING ON QUIMBY ROAD, BUT WITH NO DIRECT ACCESS ON QUIMBY ROAD.

## STREET SECTIONS



## THROUGH-COMMUNITY STREETS

CITY OF SAN JOSE

EVERGREEN SPECIFIC PLAN

EXHIBIT

66

Quimby Road has been developed in a slightly different manner than Aborn Road. Between White Road and Ruby Avenue, many homes have driveways directly on Quimby Road, which create greater friction on the street. In addition, the existing subdivision on the north side of Quimby Road has developed a unique system of frontage roads and half-walls running the length of Quimby Road to Murillo Avenue. This treatment provides a greater setback for the majority of houses from Quimby Road and the half-wall defines the project's boundary without overwhelming the streetscape, similar to a full acoustical sound wall. The result is a very open streetscape and one of the nicest examples of monumentation in the immediate area.

In response, the Evergreen Specific Plan proposes to maintain the combination of frontage roads and half-walls along the south side of Quimby Road. Quimby Road will remain a major collector, with four travel lanes within a ninety foot (90') right-of-way from Ruby Avenue to Murillo Avenue.

#### Murillo/Yerba Buena Avenue:

The third through-community street and the primary north-south circulation within the Evergreen Specific Plan is Murillo/Yerba Buena Avenue (Exhibits 65 and 66). The City of San Jose's *Horizon 2000 General Plan* designates Murillo/Yerba Buena Avenue as a major collector from Tully Road at the north, to its future intersection with Aborn Road. Continuing south from Aborn Road, Murillo Avenue changes names to Yerba Buena Avenue and continues south to Evergreen Creek, where it turns west and runs into San Felipe Road. Yerba Buena Avenue is then upgraded to a minor arterial status from San Felipe to U.S. 101 and beyond.

The Evergreen Specific Plan's treatment of Murillo/Yerba Buena Avenue is similar to Aborn Road, in that it is anticipated that Murillo/Yerba Buena Avenue will ultimately carry a major amount of future campus industrial traffic.

Murillo/Yerba Buena Avenue will remain as a major collector, with four travel lanes within a one hundred twenty foot (120') right-of-way.

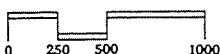
### Major Collectors

The City of San Jose's Circulation Element of the *Horizon 2000 General Plan* has defined both Ruby Avenue and Delta Road as collectors (Exhibit 67). The extension of Ruby Avenue across the project site will be constructed in accordance with the radial collector standard discussed below.

## INTERNAL CIRCULATION

### Radial Collectors

Because of the strong east-west dominance of Aborn Road, the Evergreen Specific Plan has established diagonal streets radiating from the Village Center and Fowler



Creek Park rotaries. The radial streets (Exhibits 65 and 68) pull together the areas north and south of Aborn Road and create a sense of linkage throughout the plan area. These radials provide dramatic axial vistas to important civic structures such as the elementary school site, link individual neighborhoods, and, in conjunction with trails and creek corridors, tie together major community amenities.

Two types of radials are used within the specific plan; radials with and without a median.

**Radial Street (with median):**

This radial is used to connect the Village Center and Fowler Creek rotaries, as well as acting as the primary entrance to the project area from Fowler Road. This section is a two-lane street with an eight foot (8') landscaped median within a one hundred and eight foot (108') right-of-way. This allows a thirty foot (30') setback from end of curb to right-of-way and allows for a meandering sidewalk on both sides.

**Radial Street (no median):**

This radial is used to tie the northern elementary school site to both the Village Center and Fowler Creek Park rotaries. This is also a two-lane street but has no landscaped median and lies within a ninety foot (90') right-of-way. Two types of radial streets are used: with and without parking pockets.

The radials without parking pockets allow a thirty foot (30') landscaped setback with meandering sidewalks on each side of the street.

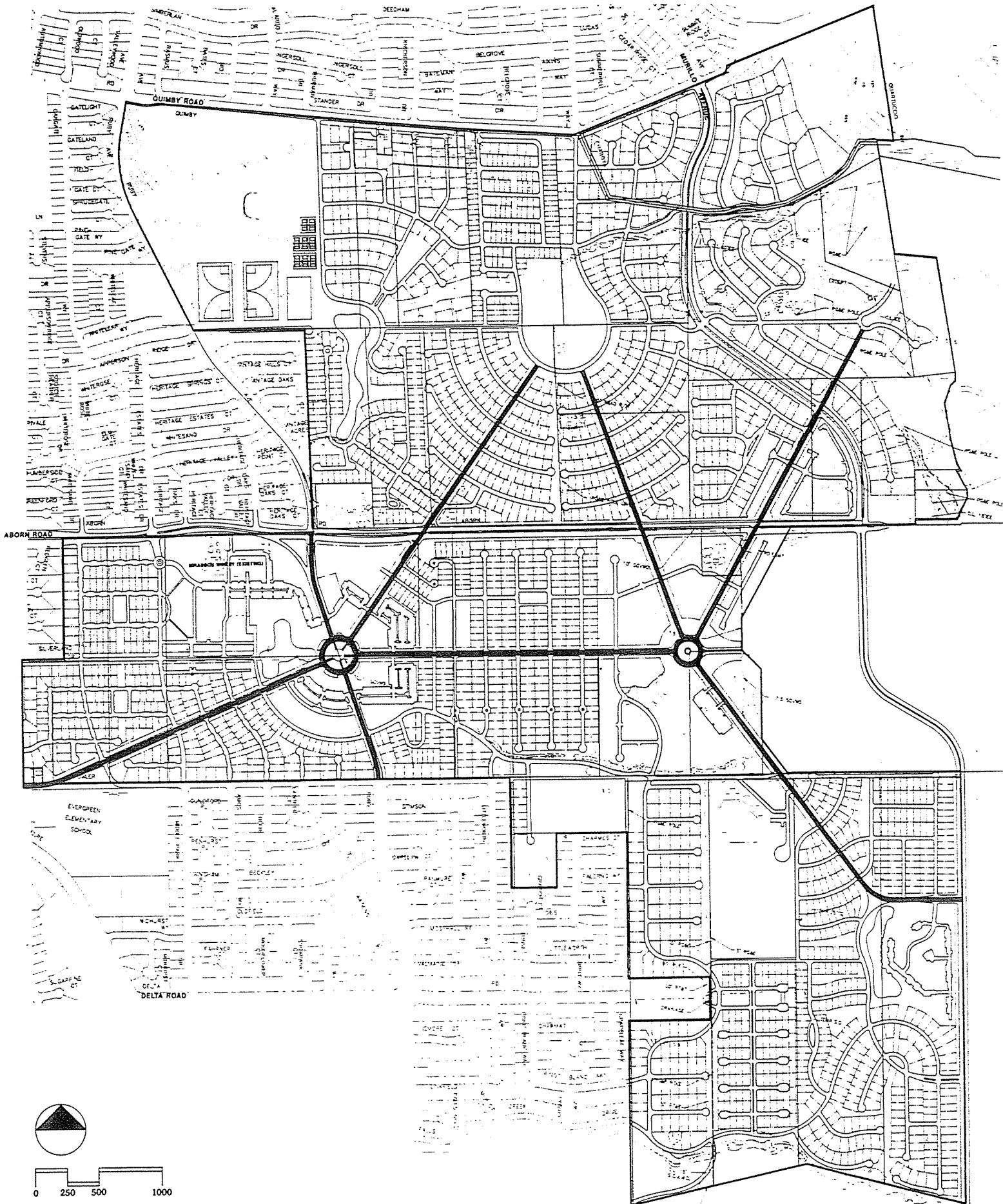
The radials with parking pockets is also within a ninety foot (90') right-of-way, but adds a six foot (6') parking pocket on one side of the street. By slightly reducing the curb-to-curb dimension of the paving, the landscaped setback at the parking bay is only reduced to twenty-five feet (25'), again with a meandering sidewalk.

**Neighborhood Collectors**

In addition to the radial streets, a system of neighborhood collector streets (Exhibits 65 and 69) are used throughout the Specific Plan area to connect the various individual developments and allow better inter-community circulation.

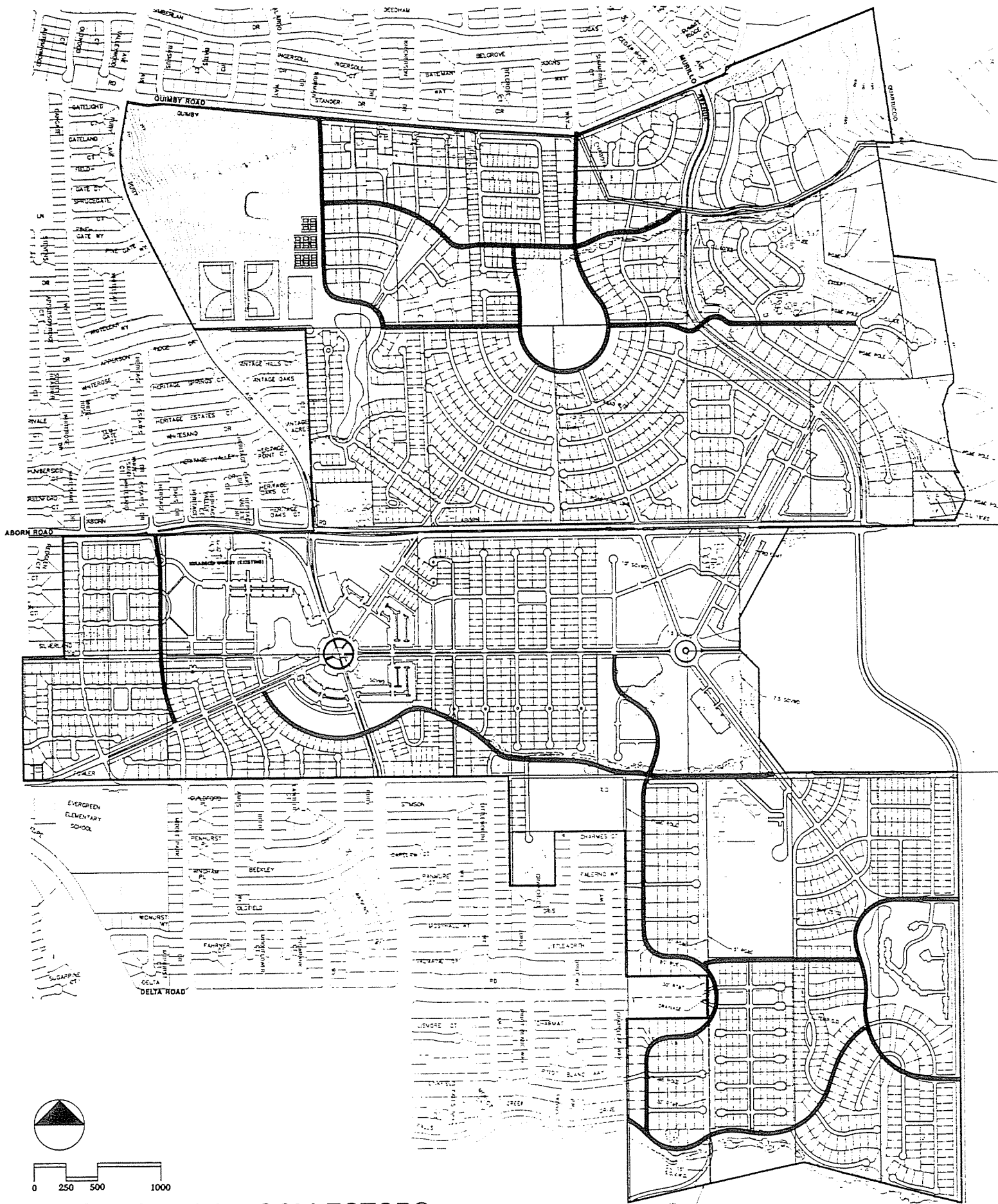
**Local Residential Streets**

Neighborhood streets must accommodate the minimum on-street parking spaces within reasonably close proximity to units served and adequate safety and utility vehicle access with the minimum amount of street paving area to develop a tree canopy over the street.



## RADIAL COLLECTORS

CITY OF SAN JOSE  
EVERGREEN SPECIFIC PLAN



## NEIGHBORHOOD COLLECTORS

CITY OF SAN JOSE  
EVERGREEN SPECIFIC PLAN



Local residential street patterns may vary, except for connections to adjacent properties, as long as the general character of the street pattern, e.g. semi-circular, grid, etc., is maintained.

Curb cuts are limited to sixteen feet (16') wide per home. Designated parking spaces, when not in bays, will require painted space markers. "No Parking" curb areas in single family neighborhoods should be signed at block ends with low (3' high) "No Parking This Side" signs. "No Parking" curbs in multi-family and commercial areas will require red painted curbs.

Minimum six foot (6') wide sidewalks are required on both sides of major arterials, major collectors, and collectors; forty-eight inches (48") minimum on radial collectors and neighborhood streets. No sidewalks are required on neighborhood streets in the Hillside lots area.

Special cul-de-sac turnarounds including island planters and special paving are encouraged. Where such treatments are employed, sidewalks are not required and building front yard setbacks may be measured from face of curb rather than property line.

#### **Intersection Signalization**

Based on the expected levels of service anticipated for the streets in and around the Specific Plan area, future signalization of major intersections may be necessary. Exhibit 70 identifies these twelve signalized intersections.

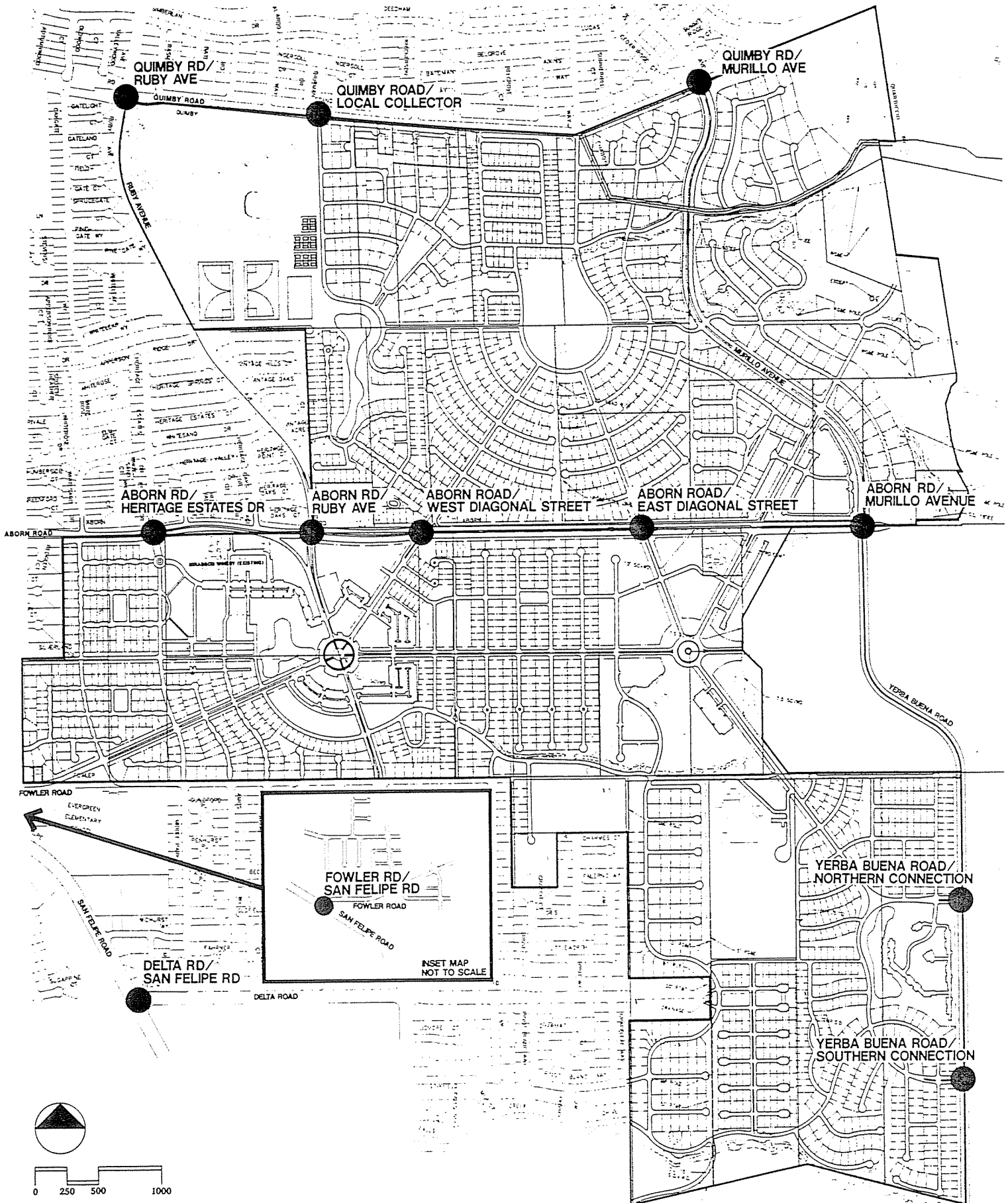
### **VILLAGE CENTER AND FOWLER CREEK PARK ROTARIES**

Although new to San Jose, traffic rotaries are not new to the roadway and street system. Older traffic circles were not conceived as a traffic control device, but rather as a focal point. Beyond the use of rotaries as focal points, rotaries also permit multiple traffic streams from all approaches to pass through an intersection simultaneously. Rotaries allow traffic on minor intersecting roads or streets to enter safely into the major traffic stream. Also, rotaries result in less delay than traffic signals serving the same traffic flows, therefore providing a better level of service.

#### **How a Rotary Works**

A traffic rotary is simply an intersection of four or more streets. The traffic rotary is a large landscaped "traffic island" with traffic moving around the island in a counterclockwise fashion. As a vehicle approaches the rotary, each of the legs to the rotary are controlled by a stop sign. After the vehicle comes to a complete stop, it is free to enter the traffic pattern by turning right and entering the rotary. The width of the rotary, although not striped as such, is the equivalent of two lanes of traffic. Once a vehicle enters the rotary, the vehicle is unrestricted and free to move through the rotary. The inside of the rotary is used to move around the circle until the vehicle reaches the desired intersection they wish to exit. By moving to the outside of the





rotary, the vehicle exits the rotary. Because all traffic is merging into the same directional flow of traffic, head-on and right angle accidents are avoided. The City of San Jose has suggested the placement of raised traffic channel islands at each intersection to force through traffic to move to the inside of the rotary and allow entering traffic the protection of the island as they enter the intersection.

## **WATER DISTRIBUTION**

The following water facilities will be required to serve the Evergreen Specific Plan area.

1. Dedication of land and construction of Aborn Zone 1-2 Reservoir (1 million gallon reservoir) at pad elevation 470'.
2. Dedication of land and construction of Aborn Zone 1-2 pump station at or near the intersection of Ruby Avenue and Aborn Road. (construction of a 1,500 G.P.M. pump station consisting of underground vault with three submersible pumps, fully soundproofed.)
3. Purchase or dedication of land and construction of Fowler zone 3-4 Reservoir (4 million gallon reservoir) near Fowler Road at pad elevation 740'. This reservoir will also serve the adjacent campus industrial properties, who should share in the cost of said improvements.

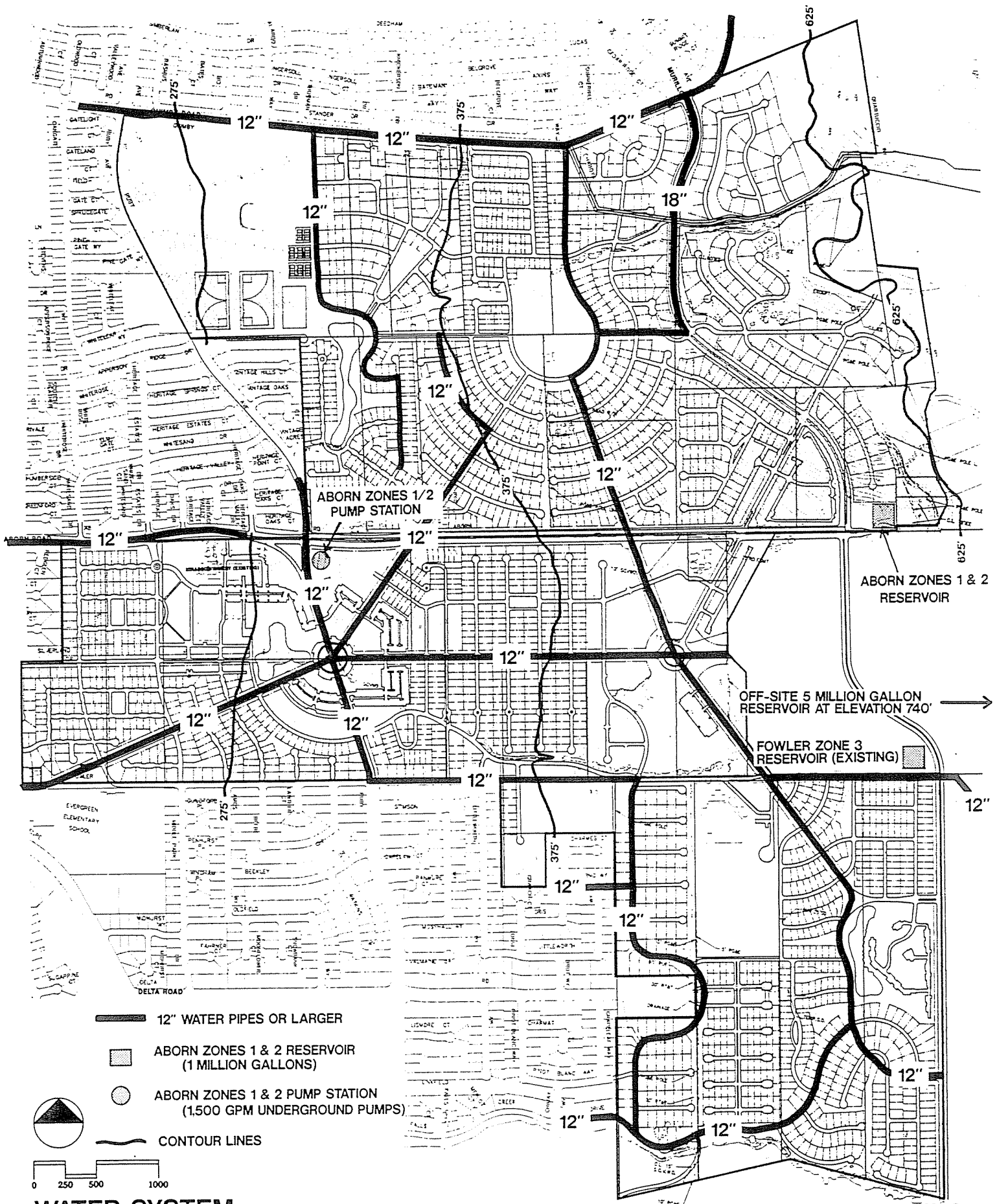
There are no major offsite water distribution improvements necessary for the development of the Evergreen Specific Plan.

Exhibit 71 (Water Distribution) outlines the major framework of the required water distribution system.

## **SANITARY SEWER SYSTEM**

Exhibit 72 (Sanitary Sewer System) outlines the major framework of the required sanitary sewer system for the Evergreen Specific Plan.

At present the Evergreen Interceptor at Chopin Avenue has adequate capacity to serve the Specific Plan area, but there is inadequate capacity available from the project site to Chopin Avenue. The planned extension of the Evergreen Interceptor is under way, and Phase III of the extension has been funded by the City of San Jose's Five Year Capital Improvement Program (CIP), and is expected to be construction in 1992. Phase IV of the Evergreen Interceptor, which is the last phase of the extension and necessary to serve the project site, is currently not funded in the City's CIP. This extension, along Aborn Road from the project boundary at Whitesand Drive to Ruby Avenue then south to Delta Road, is primarily within the project site boundary. This segment of the extension can be constructed as a part of the improvements of the Specific Plan, and reimbursement options will be explored as part of the project's financing plan.



## WATER SYSTEM

### CITY OF SAN JOSE EVERGREEN SPECIFIC PLAN



## **STORM DRAINAGE SYSTEM**

The Evergreen Specific Plan lies within three watersheds: Quimby Creek, Fowler Creek, and Evergreen Creek. All of these water courses are tributary to Thompson Creek. Thompson Creek is, in turn, a tributary to Silver Creek, which is tributary to Coyote Creek. Coyote Creek in turn, discharges into San Francisco Bay.

Currently, the three watercourses, Evergreen, Fowler, and Quimby Creeks, are in a transitional status. Evergreen Creek leaves the foothills as a natural, fairly well incised creek channel. At a point approximately one mile from Thompson Creek, Evergreen Creek has been placed in an engineered channel.

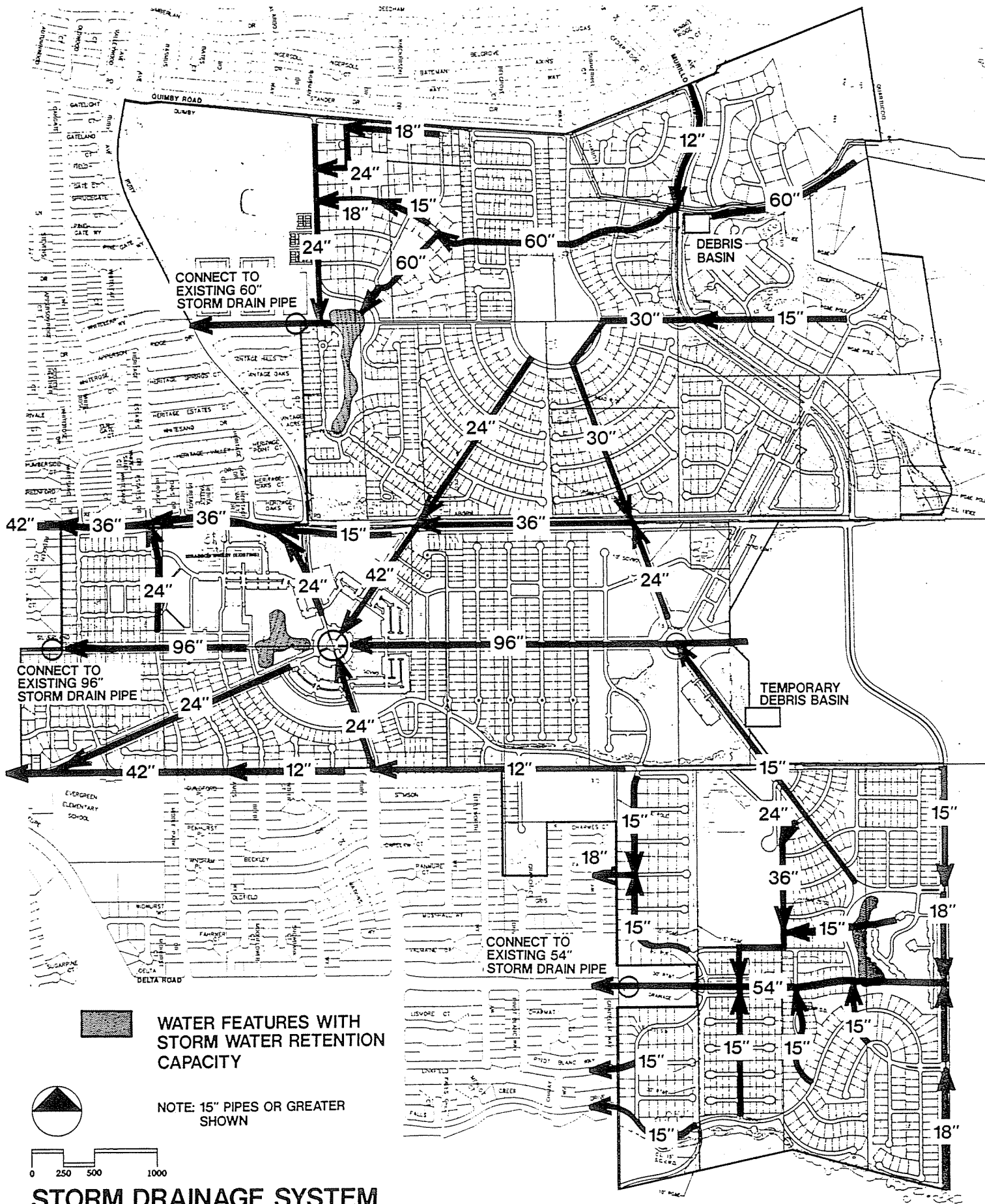
The lower end of Fowler Creek is a flood control facility consisting of a closed conduit at the end of Silverland Drive. At the upstream limit of the pipe there is a temporary debris basin. In the upper end of Fowler Creek, the creek channel is well defined as it discharges from the foothills onto the alluvial plain. The channel disappears midway down the alluvial plain so there is no trace of the creek near the existing, temporary debris basin.

Quimby Creek is in a similar state as Fowler Creek. In the lower portions of the watershed, the flood control facility consists of an open channel. This channel transitions into a closed conduit as the slope steepens. The upper limit of the closed conduit also has a temporary debris basin.

The flood control and drainage aspects of development in the entire Evergreen area of the City are tied to the existence of Lake Cunningham. This lake, besides being a water-oriented park, is also a Santa Clara Valley Water District flood control facility. As flows come down Silver Creek, the lower flows continue down Silver Creek toward Coyote Creek, but the higher flows spill over a weir into Lake Cunningham where they are temporarily stored and released back into Silver Creek after the peak of the flooding is past. This off-channel facility functions to provide some protection to the downstream properties and serve as a mitigation measure for development in the Evergreen area. Thus, development in the Evergreen area should be able to develop and to utilize Lake Cunningham to mitigate the increases in peak discharge which such development could generate.

A hydrologic model was prepared and analyzed to review the proposed development within the Evergreen Specific Plan area and the resulting changes to the discharges to the hydrologic system. The basic model used the Santa Clara Valley Water District's 1976 hydrologic model for the impact assessment was a change in the capacity of the storm drain system. Recently, the City of San Jose has changed its storm drainage criteria. The new criteria, in effect, requires larger storm drains with more capacity to deliver urban runoff to flood control facilities.

Based on the District's 1976 model and the City's new criteria, the hydrologic model used to assess impacts showed a doubling of storm drainage capacity in the Evergreen, Fowler,





and Quimby Creek's watersheds. The changes in peak discharge for the 100-year and 10-year floods are rather small, being on the order of 0.6 percent for the 100-year flood and seven percent increase for a 10-year flood.

Although the increase in peak discharge at Lake Cunningham is small relative to its capacity, it was felt that any decrease in the flood protection provided by this facility should be mitigated. In addition, portions of critical wildlife habitat within Quimby and Fowler Creeks would be removed and the creeks enlarged to create the necessary capacity to carry the 100-year flood discharges. Slopes in the area are steep and the creek channels would require some type of erosion protection. Such protection would also negatively affect the existing wildlife and vegetation habitat.

The storm drainage plan for the Evergreen Specific Plan has made an effort to provide both the flood control facilities but also to leave the existing natural flows in the existing creek channels. Low flows would be diverted from the flood control facilities to the natural creek channels to maintain the natural wildlife and vegetation habitat. The low flows would not create the severe erosion potential of the higher flows. As the natural channels disappear, creek extensions would be created to further define the creek channels and eliminate the sheet flow of higher runoff. The flows coming down the "natural" channels would discharge into the flood control facilities, which would be underneath the water features. In this way, the flows coming down the "natural" creek channels would not adversely affect the water quality of the water features.

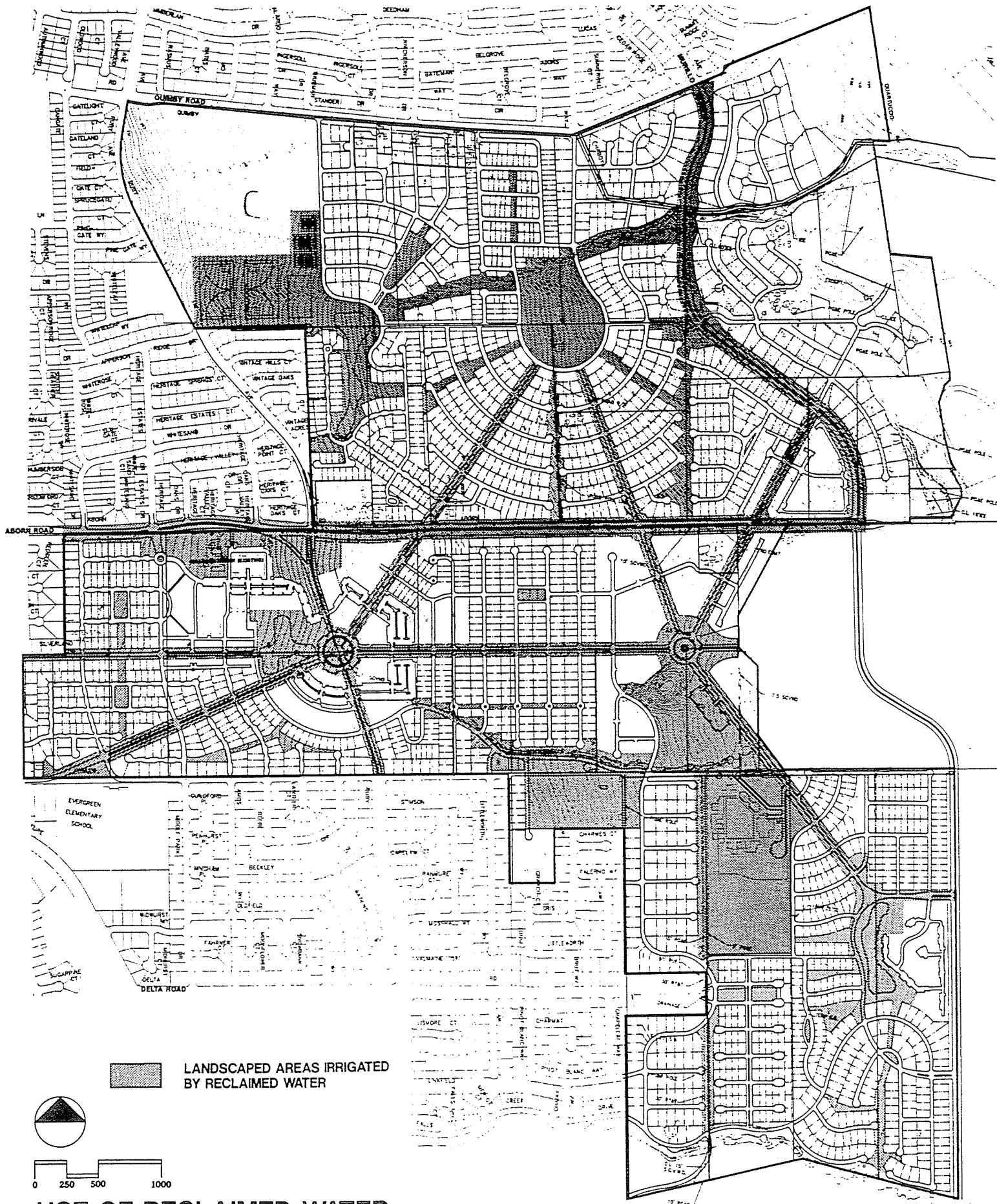
The water features, while serving as year-round project amenities, would also serve as storm water detention basins. These basins would only be used whenever the runoff exceeded the storm drain capacity. The detention storage needed for the Quimby Creek watershed is five (5) acre feet, requiring two feet (2') of freeboard in Quimby Creek lake. The detention storage needed for Fowler Creek watershed is four (4) acre feet, requiring two feet (2') of freeboard in the Fowler Creek lake.

Exhibit 73 outlines the storm drain system for the Evergreen Specific Plan area. The proposed system, although similar to the Santa Clara Valley Water District's 1972 preliminary plans, also calls for the preservation of the Quimby and Fowler Creek corridors, which was not proposed at that time.

## **USE OF RECLAIMED WATER**

As California struggles through an extended drought, and dwindling water supplies force voluntary and mandatory cut backs by water agencies, water for new residential uses has come under greater scrutiny. Although residential uses only use nine percent of all water used in the State of California, all sectors of water users in California must rethink how they use water and determine how to use water resources more efficiently.

The City of San Jose and the Santa Clara Valley Water District have a variety of requirements, regulations, policies, and guidelines specifically developed to use water more efficiently. Water-saving fixtures, such as low-flow shower heads, flow restrictors, and low



## USE OF RECLAIMED WATER

CITY OF SAN JOSE

EVERGREEN SPECIFIC PLAN

EXHIBIT

74



volume flush toilets are now incorporated into the construction of new homes and businesses. In addition, the use of drought resistant and native landscaping as well as more efficient lawn sprinkler and slow-drip systems can greatly reduce the use of water in today's newer homes and subdivisions. In an effort to take water conservation one step further, the Evergreen Specific Plan has examined the use of water within the specific plan to determine what additional measures this plan can implement.

One concept is to find a way to lower the amount of water that would normally be used to water the landscaped areas within the specific plan. The Evergreen Specific Plan will provide the community with approximately 165 acres of common and/or public landscaped areas (Exhibit 74). These areas break down as follows:

Chaboya Middle School	10 Acres
Two Elementary Schools	12 Acres
High School Site	25 Acres
Fowler Creek Park	13 Acres
Heritage Vineyard	6 Acres
Creeks, Trails, Parks	58 Acres
Street Landscaping	41 Acres
<b>Total Landscaping</b>	<b>165 Acres</b>

One way to lower the amount used for the landscaping within the Evergreen Specific Plan is to treat a portion of the wastewater generated by this project and use the treated water on the specific plan area's landscaping. In order to implement this concept, the specific plan recommends a tertiary wastewater treatment plant along a major sewer trunk line and will require the installation of dual water lines along the major core streets within the specific plan. The tertiary treatment plant would "skim" off wastewater, treat it, and the treated water would be used to water the common area landscaping throughout the specific plan area. During the winter months when the landscaping doesn't require as much water, the treatment plant would treat less water. The treated water would be safe for use on the park's and school's playfields but is not a potable source of water for use within homes or businesses.

Based on 165 acres of landscaping, the Specific Plan area will need to treat approximately 441,965 gallons of wastewater per day to generate a sufficient amount of reclaimed water for landscaping purposes. Based on the estimate of water use for landscaping at 2 acre-feet per year per acre, and landscaping acreage calculated at 165 acres, using reclaimed wastewater will save an estimated 330 acre-feet of water per year.

The location of the proposed wastewater reclamation plant will be along the proposed Evergreen Interceptor line (Phase IV) along Ruby Avenue. The plant will require approximately 10,000 square feet and will be contained entirely within a structure which will be architecturally treated in a style similar to the Village Center.

It is important to note that the City of San Jose and the Santa Clara Valley Water District are both exploring this concept and its feasibility for implementation on a larger scale.

There are several benefits in implementing this concept on a larger scale for both agencies. A larger tertiary wastewater plant could provide water for much larger landscaped areas such as existing or future golf courses while, at the same time, not creating an additional demand on water resources. In addition, the more wastewater treated and reused within the City translates to less wastewater that the City of San Jose would treat at its main wastewater treatment plant and lessen the amount of treated fresh water that would be released into San Francisco Bay. If the appropriate public agencies make the determination that this concept is to be implemented on a larger scale, the Evergreen Specific Plan should re-examine the need for an on-site treatment plant in lieu of participating in a larger system out of the Specific Plan area.

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## Implementation



## **Chapter 10 - Implementation**

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**T**his section schematically describes the procedural requirements for the implementation of this plan and determines how the plan will be interpreted.

### **OWNERSHIP WITHIN THE SPECIFIC PLAN**

The Evergreen Specific Plan area consists of forty-seven parcels under the ownership of thirty-one different owners, including Shapell Industries of Northern California, the William Lyon Company, Citation Homes, East Side Union High School District, Roman Catholic Archdiocese of San Jose, and the Mirassou Winery among others. Exhibits 75 and 76 of the Specific Plan identify the location of these parcels and present owners of these properties. (Ownership information as contained within the Evergreen Specific Plan is based on title reports and Santa Clara County assessor's parcel information. This information, although current as of the date of this specific plan, is subject to change.)

The Evergreen Specific Plan applies to all lands within the boundaries as shown in Exhibit 75. All subsequent development proposals and subdivision applications within the Specific Plan boundaries must be consistent with the General Plan and Planned Development Zoning.

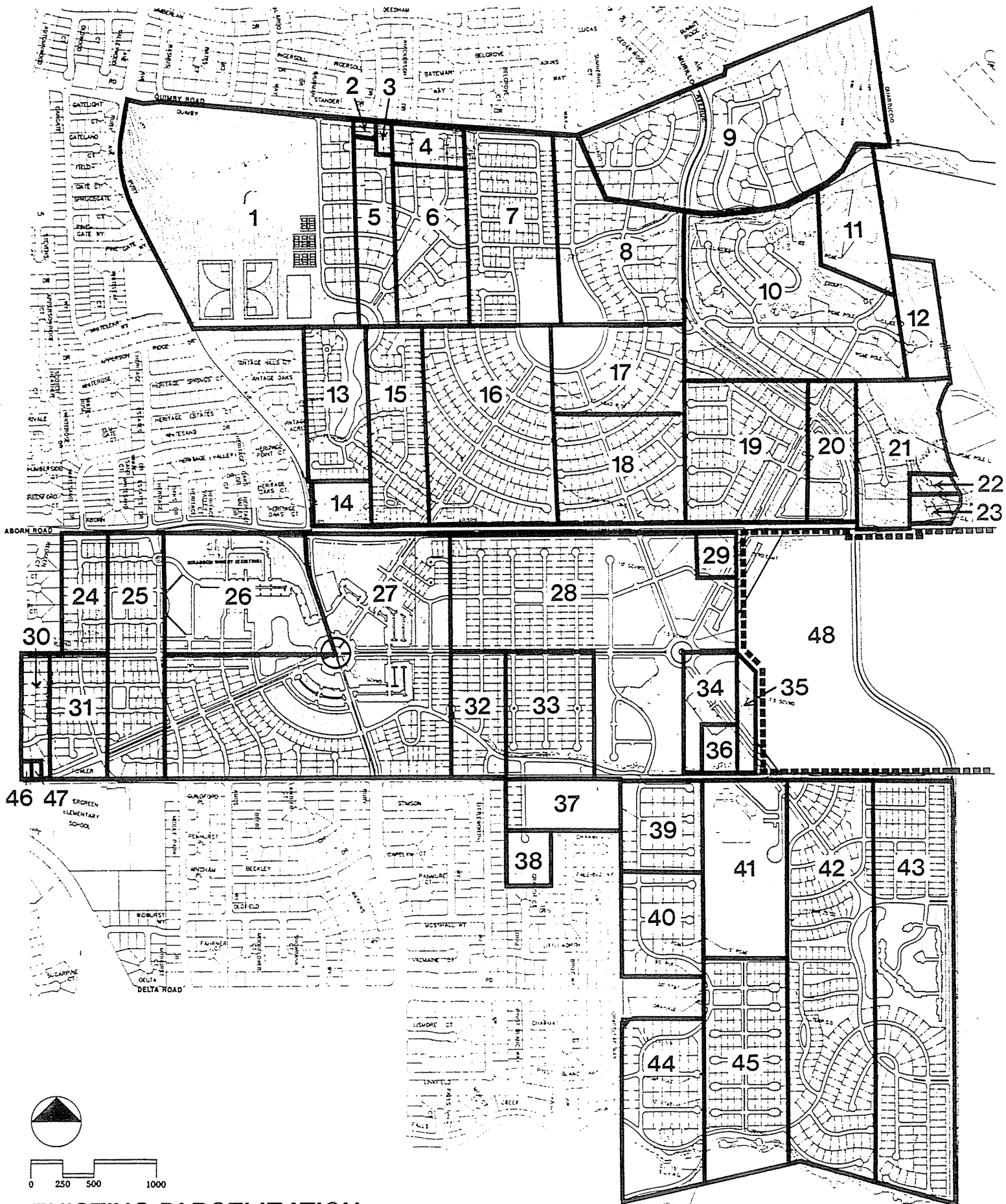
### **URBAN SERVICE AREA BOUNDARY**

In that existing and proposed City of San Jose services and facilities will be in place to serve the Evergreen Specific Plan area, it is the intent of this Specific Plan that the current Urban Service Area boundary be amended to include all properties within the Specific Plan area.

### **ANNEXATIONS**

It is City of San Jose, County of Santa Clara, and LAFCO policy that existing and future urban development should be located within cities. This policy should be implemented through the City's existing agreement with the County which requires that unincorporated properties within the Urban Service Area annex to the City.

Properties not currently within the City limits of the City of San Jose can not develop until said properties are annexed into the City of San Jose. It is the underlining intent of the City of San Jose and Santa Clara County that all urban development within the Specific Plan boundaries occur within the City of San Jose.



## EXISTING PARCELIZATION

CITY OF SAN JOSE

EVERGREEN SPECIFIC PLAN

EXHIBIT

## OWNERSHIP CHART

NO.	A.P.N.	CURRENT OWNER	ACREAGE	PLANNING AREAS
1	659-21-006	East Side Union High School District	65.00 AC	X, High School Site
2	659-21-007	Castro, Eileen M.	0.54 AC	HH
3	659-21-009	Raitano, Dominic and Rosabel	0.66 AC	II
4	659-21-010 659-21-011	Schnutenhaus, Edward et. al.	3.97 AC	J
5	659-21-008	Shapell Industries of Northern California	10.32 AC	JJ
6	659-21-012	Shapell Industries of Northern California	16.78 AC	JJ
7	659-22-010	Davidson, Charles W. and Anita	24.77 AC	K, School Site (Portion)
8	659-22-002	Valley Title	24.77 AC	Y, Z, School Site (Portion)
9	659-22-008	Borello, Louis P.	60.31 AC	KK, QQ
10	659-22-003	Valley Title	44.51 AC	RR
11	659-22-005	Jamison, Joan Lee	9.72 AC	Non-Urban Hillside
12	659-08-017	Valley Title	6.75 AC	Non-Urban Hillside
13	659-10-010	J. Lohr Properties, Inc.	13.28 AC	P



NO.	A.P.N.	CURRENT OWNER	ACREAGE	PLANNING AREAS
14	659-10-009	Mirassou, Edmund A.	3.65 AC	LL
15	659-10-004	Pappani, John, Trustee	16.62 AC	P
16	659-10-003	Sorci, John J.	36.35 AC	AA, School Site (Portion)
17	659-10-002	Sorci, John J.	16.60 AC	BB, School Site (Portion)
18	659-10-001	Sorci, Joseph and Eldiva	20.35 AC	CC
19	659-09-005	Shapell Industries of North- ern California	24.66 AC	D, DD
20	659-09-004	Percy, Phillis A.	9.98 AC	E
21	659-09-003	Costa, Maria et. al.	15.69 AC	SS
22	659-09-001	Alvarez, Herman R.	1.38 AC	Existing Home Site
23	659-09-002	Alvarez, Salvador H.	2.38 AC	Existing Home Site
24	659-06-009	Shapell Industries of North- ern California	7.45 AC	T
25	659-06-001	Shapell Industries of North- ern California	20.01 AC	Q, T, V
26	659-03-008	Mirassou, James L. et. al.	26.49 AC	A, G
27	659-03-009	Mirassou Lands, Ltd.	73.25 AC	B, F, H, L, N, V
28	659-03-010	Mirassou Lands, Ltd.	62.09 AC	I, K, M, O, U

NO.	A.P.N.	CURRENT OWNER	ACREAGE	PLANNING AREAS
29	659-03-007	Mirassou, James L.	2.66 AC	I
30	659-06-003	Shapell Industries of North- ern California	4.42 AC	Q
31	659-06-002	Shapell Industries of North- ern California	9.76 AC	Q
32	659-03-003	Boyd, Charles S.	10.00 AC	EE
33	659-03-002	Loukate, Inc. et. al.	15.00 AC	FF
34	659-03-012	City of San Jose	12.53 AC	Fowler Creek Park
35	659-02-010	City of San Jose		
36	659-03-011	City of San Jose		
37	660-31-003	Sorci, John	8.40 AC	School Site
38	660-31-002	Mirassou, Norbert C.	3.30 AC	NN
39	660-32-004	Roman Catholic Archbishop of San Jose	10.00 AC	MM, OO
40	660-32-003	Roman Catholic Archbishop of San Jose	11.83 AC	OO
41	660-32-010	Evergreen School District	21.90 AC	Chaboya Middle School
42	660-32-007	Lyon, William	47.60 AC	S, W, GG
43	660-32-008	Lyon, William	47.57 AC	C, W, GG

NO.	A.P.N.	CURRENT OWNER	ACREAGE	PLANNING AREAS
44	660-32-001	Lyon, William	20.00 AC	PP
45	660-32-011	Citation Homes	25.80 AC	R
46	659-06-005	Sorci, Marie A. et. al.	0.19 AC	Existing Home Site
47	659-06-004	Sorci, Gaeton	0.19 AC	Existing Home Site
		<b>Property Adjacent to Specific Plan Area:</b>		
48	659-02-007	The Anden Group	90.56 AC	Campus Industrial

## LAND EXCHANGES WITHIN THE SPECIFIC PLAN AREA

The Evergreen Specific Plan has proposed a distinctive diagonal circulation pattern which makes a conscious effort to "break" from the rectangular grid of the existing parcel boundaries. In order to implement this circulation pattern, a series of land exchanges within the Specific Plan are proposed. These exchanges involve trading approximate acreages from one property owner to another in order to facilitate the diagonal streets and Murillo Avenue alignment. (These exchanges are identified in Exhibits 77 and 78.)

One example of the proposed land exchanges within the Specific Plan area would be between the Pappani and Sorci properties (Parcels 15 and 16). The Evergreen Specific Plan has limited points of access on Aborn Road, Quimby Road, and Murillo/Yerba Buena Avenue, which would mean that the majority of future projects will rely on the establishment of the project's collector streets for access. Because of their diagonal nature, the radial collector streets do not always fall along property lines. In this example, a land exchange is proposed between parcels 15 and 16 so that the Pappani property is reconfigured in order to have frontage on the radial collector street. In exchange for the property gained by parcel 15, an equal amount of property is given to parcel 16 from parcel 15. The parcels neither gain nor lose property, and the integrity of the plan remains intact.

### Primary Exchanges and Secondary Exchanges:

There are two types of property exchanges within the Specific Plan area; primary exchanges affecting key elements of the overall Specific Plan and secondary exchanges which only affect adjoining parcels.

### Primary Exchanges:

The following exchanges are deemed "primary" exchanges, and would be implemented by the Specific Plan:

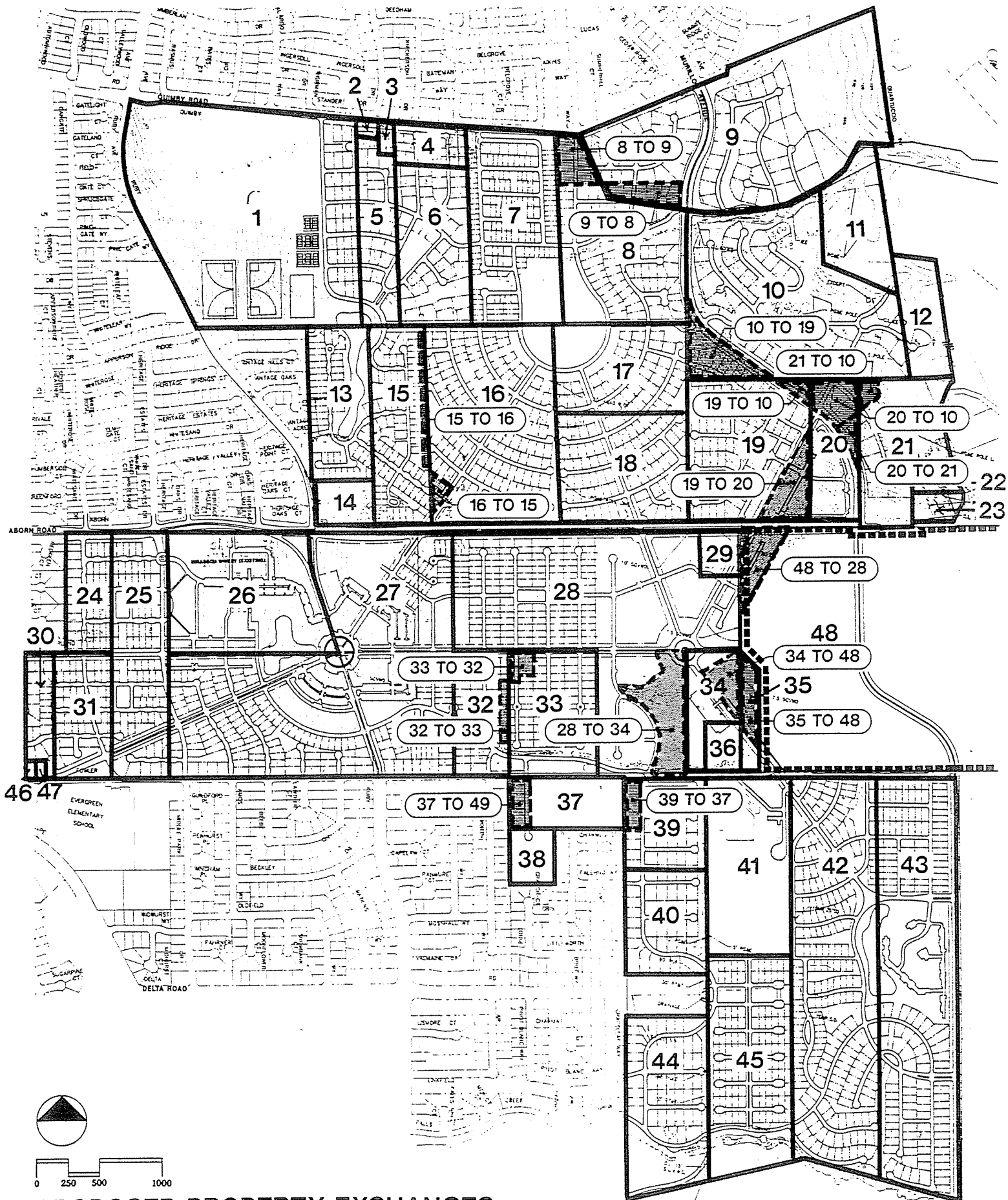
1. Exchanges affecting the alignment of Murillo Avenue. This exchange affects parcels 10, 19, 20, and 21.
2. Exchanges affecting the reconfiguration of Fowler Creek Park and the campus industrial properties. This exchange affects parcels 28, 34, 35, 36 and the adjoining campus industrial parcel 48.

Primary exchanges should occur at the same time that street right-of-ways, school sites, and common open space elements are dedicated.

### Secondary Exchanges:

The following exchanges are deemed "secondary" exchanges, and are not critical to the implementation of key elements of the Specific Plan. These exchanges would be implemented by individual parcels prior to development:

1. Exchanges between parcels 8 and 9. This exchange removes the previously existing segment of Chaboya Road, which currently lies along the boundary

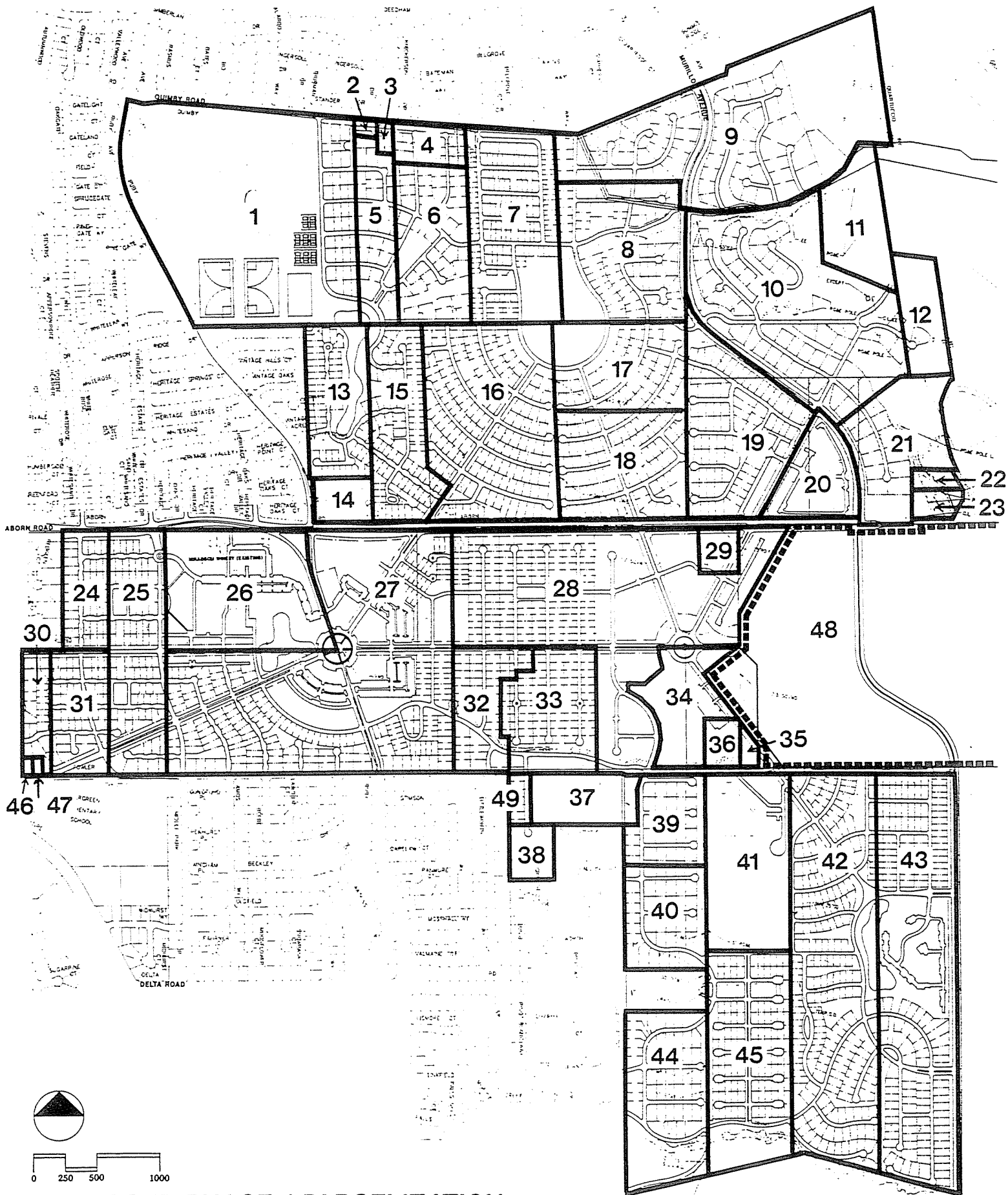


## PROPOSED PROPERTY EXCHANGES

CITY OF SAN JOSE  
EVERGREEN SPECIFIC PLAN

EXHIBIT

77



## PROPOSED PHASE 1 PARCELIZATION

CITY OF SAN JOSE  
EVERGREEN SPECIFIC PLAN

EXHIBIT

78

between parcels 8 and 9. Through the establishment and construction of Murillo Avenue between Quimby and Aborn Roads, this segment of Chaboya Road can be eliminated and the parcels reconfigured into more conventional parcels.

2. Exchanges between parcels 15 and 16. As cited above, this exchange provides parcel 15 with access to the radial collector street.
3. Exchanges between parcels 32 and 33. The east-west dimensions of the two parcels do not easily facilitate development as shown. The land exchange allows for the implementation of the plan while maintaining the existing acreage for both parcels.
4. Exchanges between parcels 37 and 39. This exchange creates a buffer between existing homes and the new elementary school by providing a row of residential units backing against the existing homes. The row of new lots also provides access to the existing residence on parcel 38. Parcel 38, already surrounded on three sides by existing development, would not be "sealed off" by the development of an elementary school site on parcel 37.

## **DEVELOPMENT ALLOCATIONS WITHIN THE SPECIFIC PLAN**

A maximum of 2,996 dwelling units has been established for the Evergreen Specific Plan area. This total has been established primarily through the Evergreen Specific Plan Task Force and its examination of the traffic capacity of the Evergreen Development Policy area and the project area's ability to support additional development through improvements to the area's circulation system. Although individual property owners may feel that they can develop lotting patterns that would create more dwelling units than have been allocated by the Specific Plan, it is important to note that the overall unit maximum must be observed and units allocated by planning area can not be exceeded.

### **Planning Areas and Unit Allocations**

In recognition of the fact that the ownership of properties can change, and also recognizing that the Specific Plan has, in some cases, identified different product types within an individual parcel, the Specific Plan has identified individual planning areas for each distinct neighborhood. These planning areas are designated in consecutive order, starting with the High Density Residential (Planning Areas A - C) through the Hillside lots (Planning Areas QQ - SS). These forty-five planning areas and their corresponding unit counts serve to identify individual allocations of dwelling units. No planning area can build less than eighty-five percent (85%) of the maximum number of units designated for said planning area.

When a Planned Development permit for any planning area approves a number of units less than the maximum number of units allowed in said planning area, the extra

# UNIT TYPE AND BREAKDOWN BY CURRENT OWNERSHIP

	Hillside Large Lots	7000 to 8000 SF	6000 SF	5000 SF	4000 SF	Carriage Homes	Duplex/ 4-Plex	Townhomes	Multi-family 12-25 DU/AC	Total
William Lyon	53	44	135	140	77				177	626
Citation					118			72		190
Shapell		68	69	334	99	202 + 116		154	54	1,096
Blackwell			78							78
J. Lohr					131					131
Borello	34	32								66
Davidson							157			157
Sorci			136							136
Lou-Kate/ Boyd			100							100
Mirassou		6						53	68	127
Catholic Church		73								73
High School			39							39
Schnuten- haus							28			28
Costa	13									13
Borelo/ Sorci/Hyatt			135							135
Misc.		1								1
Total	100	224	692	474	425	202 + 116	185	279	299	2,996



units may be transferred to one or more of the remaining planning areas of similar type as long as the combined total number of units is not exceeded.

### **Dwelling Unit Breakdown**

Exhibit 79 provides a dwelling unit breakdown by product type, planning area, and by property ownership. This exhibit has attempted to group parcels under common ownership into a single category, thus outlining totals for each of the product types and a summary of the different product types for each owner. Because of the possibility that ownership within the Specific Plan can change over time, the unit allocations as shown by the individual planning areas take precedent over the dwelling unit breakdown as depicted in Exhibit 79.

## **OFF-SITE TRAFFIC MITIGATION**

Development within the Evergreen area has been constricted by a traffic circulation system that has been unable to expand to meet additional traffic demands. Development within the Evergreen Specific Plan area is contingent upon the provision of additional traffic capacity (Exhibit 80). Through its analysis of the area's circulation system and projections of expected traffic impacts for the Evergreen Specific Plan area and other expected development within the general Evergreen area, a series of circulation improvements have been identified as necessary to provide traffic capacity. The improvements can be grouped into three categories based on the jurisdiction who would ultimately be responsible for circulation elements. The 140 dwelling units which received an allocation (File No. C 87-10-75, Shapell) in the 1989 Annual Evergreen Monitoring Report as approved by City Council on March 27, 1990 are not required to participate in the specific traffic capacity improvements identified in the Evergreen Development Policy, revised June 25, 1991.

### **City of San Jose Jurisdiction (Exhibit 81)**

#### **Aborn Road:**

As the main arterial connecting Capitol Expressway to the project site, Aborn Road will require six lanes over this length. To achieve this, the only required improvement will be restriping (the existing roadway width, as constructed, is already sufficient for six lanes).

#### **Quimby Road:**

With Quimby Road serving as the principal northern access route to the Evergreen Planned Residential Community, minor roadway upgrades will be needed in order to provide additional traffic-carrying capacity. With the exception of a 1,500-foot segment between Capitol Expressway and White Road, Quimby Road is currently either four lanes or of sufficient width to accommodate four lanes. A one-quarter mile section of Quimby Road will need to be widened to four lanes from the current three. (This widening is also a





## ROAD IMPROVEMENTS - CITY OF SAN JOSE JURISDICTION

CITY OF SAN JOSE  
EVERGREEN SPECIFIC PLAN

condition of approval for an approved project located immediately adjacent to the proposed widening.)

**White Road/Aborn Road:**

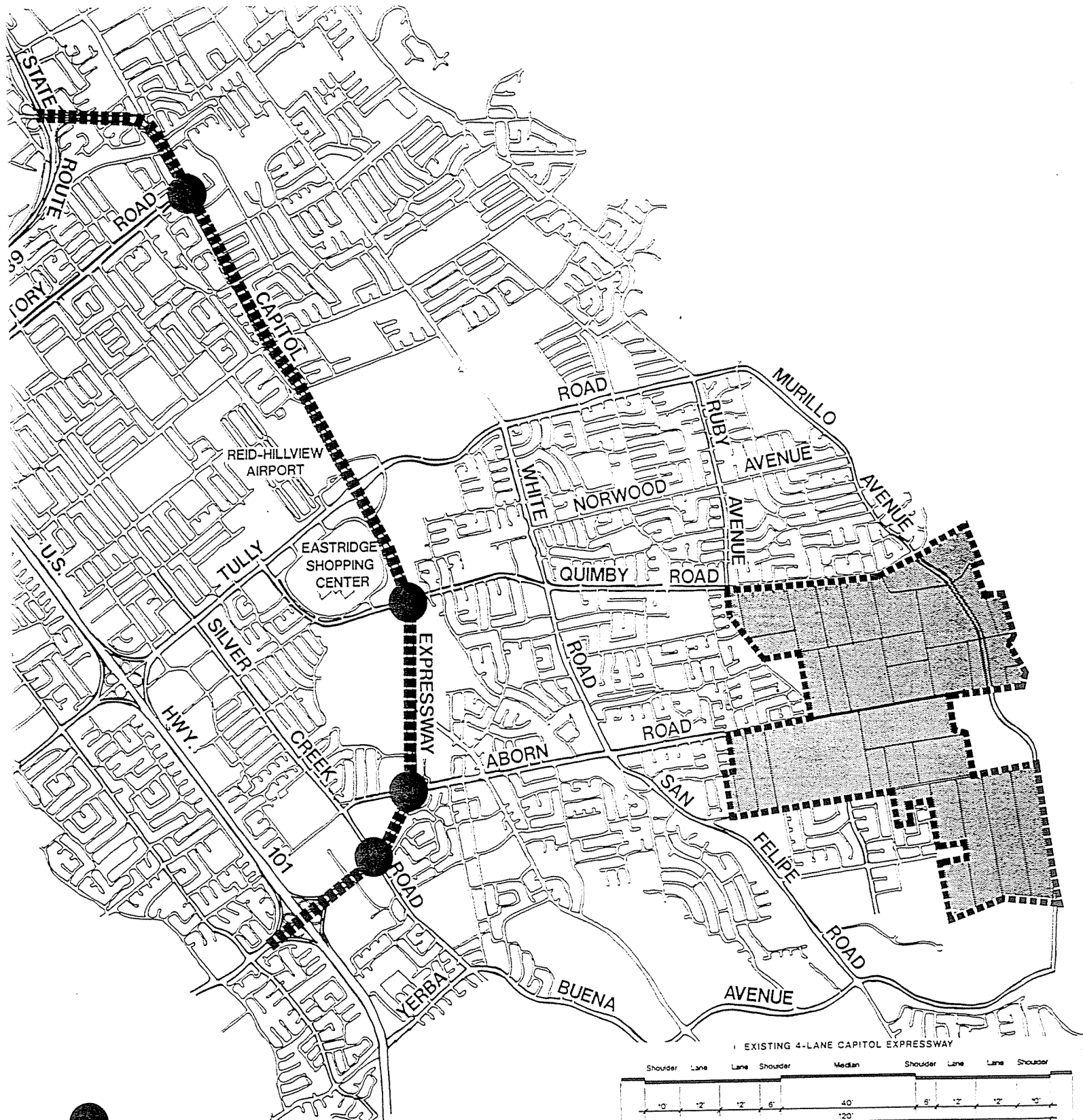
An additional left-turn lane will be needed at the north approach, and an additional through-lane will be needed at the east approach. The exclusive right-turn lane at the west leg needs to be converted to a shared through/right-turn lane in order to be consistent with the plan for a six-lane Aborn Road. Under cumulative conditions there will be a substantial increase in traffic volumes on Aborn Road due to the trips generated by the campus industrial facility. The increased demand at this intersection can best be accommodated with the addition of second left-turn lane at the west approach. This lane will fit in the available right-of-way.

Upgrades to those portions of Quimby and Aborn Roads lying between Ruby Avenue and Murillo Road will constitute on-site roadway improvements. This will require that rights-of-way be dedicated and the roadways be widened in accordance with the City of San Jose's circulation element, which calls for four lanes on Quimby Road and six lanes on Aborn Road. Murillo Road, Ruby Avenue, and other local streets will be designed in conformance with City standards and will be subject to approval by City staff prior to construction.

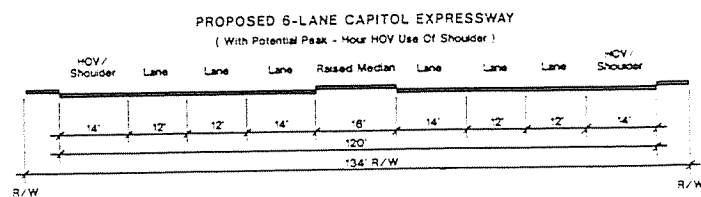
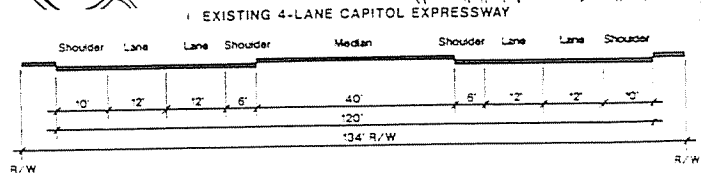
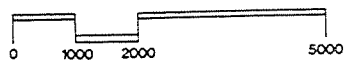
**Santa Clara County Jurisdiction (Exhibit 82)**

The most significant improvement measure will involve the upgrade of Capitol Expressway from the current four lanes (six lanes south of Aborn Road and north of Story Road) to six lanes plus high-occupancy vehicle (HOV) lanes between I-680 and U.S. 101, as shown on Exhibit 82. This recommendation is consistent with that given in the *Transportation 2000 County-wide Expressway Study* prepared for Santa Clara County by CH2 MHILL in November, 1986. Constructed as such, shoulder use will be permitted for high-occupancy vehicles during the peak hours of operation. With sufficient levels of HOV use, Capitol Expressway will effectively function as an eight-lane facility during the hours of peak demand. This roadway scheme is currently being utilized on other expressways in Santa Clara County and has proven to be conceptually sound and operationally effective. Furthermore, the design can be accommodated within the existing right-of-way. Serving as the mainline transportation facility in Evergreen, Capitol Expressway is expected to carry between 50,000 and 60,000 vehicles daily. The upgrade of Capitol Expressway will also contribute significantly to intersection improvement efforts in addition to providing the needed through-capacity.

In order to ensure that all study intersections operate satisfactorily under project conditions, it will be necessary to provide the intersection improvements described below. These improvements are consistent with the aforementioned roadway upgrades.



 INTERSECTIONS REQUIRING IMPROVEMENTS  
 CAPITOL EXPRESSWAY WIDENING



# ROAD IMPROVEMENTS - SANTA CLARA COUNTY JURISDICTION

CITY OF SAN JOSE  
 EVERGREEN SPECIFIC PLAN

**Capitol Expressway/Story Road:**

By converting the exclusive right-turn lane at the north approach into a shared through/right-turn lane, the high volume of southbound traffic into Evergreen during the p.m. peak hour can be accommodated. The addition of a through-lane to the south approach will similarly serve to alleviate the congestion in the northbound direction during the a.m. peak hour.

**Capitol Expressway/Quimby Road:**

In keeping with the recommended upgrade of Capitol Expressway, it will be necessary to add a through-lane and convert the exclusive right-turn lane into a shared through/right-turn lane at each of the north and south approaches.

**Capitol Expressway/Aborn Road:**

The extremely high left-turn demand from westbound Aborn Road will necessitate that a third left-turn lane be added to the east approach. Because of the relatively low through volumes from this approach, this left-turn lane can be taken out of one of the existing westbound through-lanes. At both the north and south approaches an additional through-lane is needed. The exclusive right-turn lane at the north approach will also need to be converted to a shared through/right-turn lane.

**Capitol Expressway/Quimby Road:**

Due to the higher volumes on Capitol Expressway at buildout, the south approach will require an additional left-turn lane. Sufficient roadway width currently exists to accommodate this lane.

**Capitol Expressway/Silver Creek Road:**

The addition of one through-lane to each of the east and west approaches on Capitol Expressway will significantly improve the intersection operation. Slight modifications to the signal timing will also be needed.

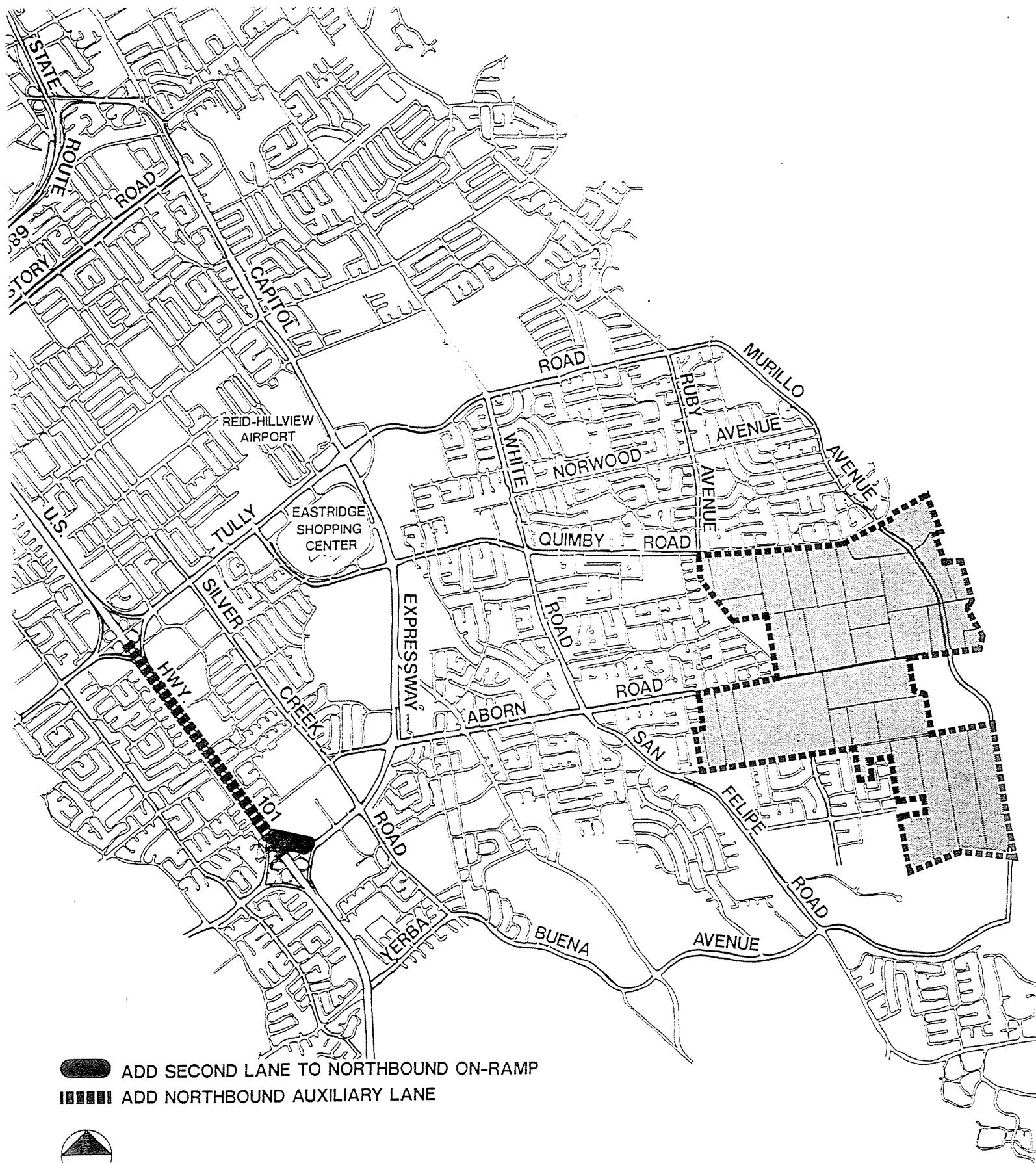
**CalTrans Jurisdiction (Exhibit 83)**



The following improvements are required within the jurisdiction of CalTrans.

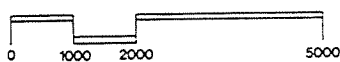
**U.S. 101/Capitol Expressway/Yerba Buena Road Interchange:**

The northbound on-ramp is expected to operate at LOS F during the a.m. peak hour. Mitigation will involve the addition of an auxiliary lane from the northbound on-ramp from Capitol Expressway through the Tully Road interchange and to I-280. A two-lane on-ramp could then be constructed at Capitol, which will accommodate twice the number of vehicles as the existing ramp. With ramp capacity thus doubled, and assuming no metering, ramp operation will improve to LOS C under project conditions. The level of service





 ADD SECOND LANE TO NORTHBOUND ON-RAMP  
 ADD NORTHBOUND AUXILIARY LANE



## ROAD IMPROVEMENTS - CALTRANS JURISDICTION

CITY OF SAN JOSE  
 EVERGREEN SPECIFIC PLAN

EXHIBIT

83

with ramp metering will depend on the freeway flow characteristics, which cannot be determined at this time.

As a part of the approval process of the Evergreen Specific Plan, the Evergreen Development Policy will be revised to reflect these identified improvements. (For a more detailed examination of the circulation issues relating to the Evergreen Specific Plan and Evergreen Development Policy, please refer to the environmental impact report and technical appendix).

## **ENFORCEMENT OF DESIGN GUIDELINES AND GDP STANDARDS**

Although information contained within this Specific Plan is fairly detailed, there will inevitably be questions about exactly what certain provisions mean. Also, when PD permits and subdivision applications are proposed within the Evergreen Specific Plan area, it is possible that some changes to the plan will be requested. The purpose of this section is to describe how questions about the plan and proposed changes will be resolved.

## **RESPONSIBILITY FOR INTERPRETATION OF THE SPECIFIC PLAN**

The Director of Planning for the City of San Jose is responsible for interpreting the provisions of this plan. Any determination that the Director of Planning makes may be appealed to the Planning Commission of the City of San Jose in accordance with the appeal process set forth by the City of San Jose. The enforcement of design guidelines as set forth in this document will be the responsibility of the Department of City Planning of the City of San Jose.

## **PROPOSED AMENDMENTS TO THE SPECIFIC PLAN**

Any Specific Plan area property owner, Planning Commission, or City Council may request an amendment to the Specific Plan. The application for amendment shall be in the form specified by the Director of Planning and shall explain the proposal and the reason for the change and should be accompanied by any necessary supporting documents, plans, etc. The proposal will then be reviewed through public hearings before the Planning Commission and City Council, although only the City Council has the authority to amend the Specific Plan. If the Specific Plan changes require a General Plan amendment, said General Plan amendment will be considered only during the General Plan Annual review process. Applications for amendments will be subject to filing fees to be determined by the City Council.

## **DEVELOPMENT REVIEW PROCESS**

Plans for PD permits will be submitted by the property owners within the Specific Plan and will be reviewed to ensure that they meet the guidelines and standards as set forth within this document and the accompanying General Development Plan. Any submittal for PD



permit must include any additional environmental studies or information as required by the Evergreen Specific Plan Environmental Impact Report or the City of San Jose. Submittals for PD permits may be reviewed by the Evergreen Lighting and Landscaping District Board of Directors or their appointed representatives (see below) prior to submittal to the City of San Jose.

## **LANDSCAPE AND LIGHTING DISTRICT**

A landscape and lighting district may be formed to address the construction and maintenance of a variety of items within the Evergreen Specific Plan as outlined in the following section "Financing Strategy". The landscape and lighting district will be governed by a board of directors representing the residents of the Evergreen Specific Plan area. The board will be responsible for overseeing the maintenance and repair of common areas and related concerns within the Specific Plan area.

## **ARCHITECTURAL REVIEW COMMITTEE**

An Architectural Review Committee (A.R.C.) may be established to review and make recommendations to the City on development plans and in order to ensure that they comply with the goals of the overall project as stated in this document as well as the fixed and flexible criteria established in the General Development Plan. All development plans must also comply with setbacks and other standards established for the Evergreen Planned Residential Community by the City of San Jose, as well as all applicable building codes. If deemed appropriate, the A.R.C. can be in association with, or appointed by, the Evergreen Landscaping and Lighting District.

## **FINANCING PLAN**

The following set of policies shall govern the funding of facilities and public services for the Evergreen Specific Plan. The principals underlying these policies are that all development in the Evergreen Specific Plan area should:

1. Pay the full cost of the infrastructure needed to serve the area;
2. Fund the costs of mitigating adverse impacts on the City's existing infrastructure (e.g. offsite transportation improvements);
3. Provide for reimbursements from other development areas for costs that Evergreen Specific Plan is required to advance;
4. Provide for a fair allocation of costs among land uses; and,

5. Develop a method to reimburse landowners for the equable distribution of specific plan requirements such as open space areas and street rights-of-ways, who were required to dedicate an excessive amount of property, or provide up-front financing of improvements benefitting property that develops later.

The following policies govern the financing of infrastructure and public services for the Evergreen Specific Plan in accordance with the above stated principals.

- Policy 1:** Fund the full costs of the on-site and off-site public infrastructure and public services required to support development in the Evergreen Specific Plan from revenues generated by development within the Evergreen Specific Plan, except where specific existing City, County, District, or State sources are available.
- Policy 2:** Allocate the core infrastructure costs to property within the Evergreen Specific Plan based on the general principles of benefit received, with consideration of the financial feasibility of the proposed land use.
- Policy 3:** Adopt, through execution of a development agreement, an "Evergreen Fee" that establishes a fair share cost allocation for public improvements required to serve the development of the Evergreen Specific Plan.
- Policy 4:** Adopt an "Area of Benefit" ordinance and form an "Area of Benefit" for the entire Evergreen Development Policy area that establishes a fair share cost allocation for required off-site transportation improvements as required to serve the development of the Evergreen Specific Plan.
- Policy 5:** Use pay-as-you-go financing to the extent possible. Use debt financing only when essential to provide facilities necessary to permit development or to maintain service standards.
- Policy 6:** Require development projects in the Specific Plan to fund the oversizing of facilities if required by the City, subject to reimbursement from future developments benefiting from the oversizing.
- Policy 7:** Require developers who proceed ahead of the infrastructure sequencing plan to pay the costs of extending the core infrastructure to their project subject to future reimbursement.
- Policy 8:** Require dedication of land for road improvements and construction of road improvements consistent with city-wide policies.

**Policy 9:** Pay for the acquisition of dedicated property required for arterial roads that exceeds 25% of the gross acreage of any existing parcel. Acquire parcels that are left without an economic use as a result of the implementation of the specific plan.

## **FUNDING STRATEGY**

The following strategy has been developed for the funding of the following elements determined to be critical to implementing the Evergreen Specific Plan.

### **Off-Site Circulation Improvements**

Construction of the various off-site circulation improvements identified previously in this chapter will be funded by a combination of City, County, State, developer financing, and an "Area of Benefit" fee that covers the entire Evergreen Development Policy area. Assessment districts or Mello-Roos Community Facilities Districts may be considered in place of, or in combination with, the set fee.

### **Major Arterials, Collectors And Underlying Infrastructure**

The Specific Plan area is bordered on the north by Quimby Road and bisected by Aborn Road and Murillo/Yerba Buena Avenue. Improvements to these roadways will be paid for by a combination of developer financing for abutting roadway frontages and the "Evergreen Fee" that covers the entire Specific Plan area. Assessment districts or Mello-Roos Community Facilities Districts may be considered in place of, or in combination with, the "Evergreen Fee". Landscape maintenance within public right-of-way may be funded by a landscaping and lighting district.

### **Local Collectors, Core Roads And Underlying Infrastructure**

Improvements to these roadways will be paid for by a combination of developer financing for abutting roadway frontages and an "Evergreen Fee" that covers the entire Specific Plan area. Assessment districts or Mello-Roos Community Facilities Districts may be considered in place of, or in combination with, the "Evergreen Fee". Landscape maintenance within public right-of-way may be funded by a landscaping and lighting district. Developers may proceed ahead of the infrastructure sequencing plan if they pay the costs of extending the core infrastructure to their project subject to future reimbursement.

### **Fire Station**

The City of San Jose Fire Department has identified the need for a new fire station within the Evergreen Specific Plan area. Based on the service area of the new station in relationship to the size of the Specific Plan area, the City has determined that the Evergreen Specific Plan will be responsible for 25% of the cost of the new facility.

This portion of the cost of the fire facility will be funded through Mello-Roos Community Facilities Districts financing or the "Evergreen Fee".

### **Schools**

Development within the Evergreen Specific Plan will be required to mitigate the impacts of the plan on the elementary and secondary schools. As a result, the Evergreen School District has identified the need for two new elementary schools within the Evergreen Specific Plan area. The East Side Union High School District has recently committed to the construction of a new high school on the property within the Evergreen Specific Plan area.

#### **A. Evergreen School District:**

The Evergreen Specific Plan will dedicate the property for two elementary school sites as designated in the Specific Plan. Construction of the two elementary schools will be funded through a combination of AB 2926 development fees, State School Building Program, a Mello-Roos Community Facilities Districts, and the "Evergreen Fee".

#### **B. East Side Union High School:**

The Evergreen Specific Plan will fund its proportional share of the new high school based on the expected number of high school students generated by the Specific Plan area. This portion of the cost of the new high school will be funded through a combination of AB 2926 development fees, Mello-Roos Community Financing Districts, the "Evergreen Fee", and bond financing.

### **Fowler Creek Park**

The Evergreen Specific Plan has identified its proportional share of impact on the facilities that will be provided in the Fowler Creek Park. (The City of San Jose currently owns the site, which will be reconfigured through land exchanges). The Evergreen Specific Plan will contribute approximately \$5.3 million toward the construction of the Fowler Creek Park and provide other parks and open space amenities in lieu of the payment of the City of San Jose Parkland Dedication fees.

### **Creeks, Internal Trails, Water Features, And Open Space**

All pedestrian trails, creek corridors, water features, and other open space features will be dedicated to the City of San Jose and become publicly owned. The Evergreen Specific Plan's financing plan will develop a system to fairly distribute the cost of the various trails, water features, and open space elements among the Specific Plan area so that properties with more than their fair share of open space features will be compensated by those properties who have less than their fair share. Actual construction of these elements will be funded through an assessment district or Mello-Roos Community Facilities Districts and/or the "Evergreen Fee". Landscape maintenance may be funded by a property owner's association or a landscaping and

lighting district. In the hillside areas within the Evergreen Specific Plan where either the placement of lots or roadways creates common lots consisting of landscaped banks, these lots will be maintained by a property owner's association or a landscaping and lighting district.

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